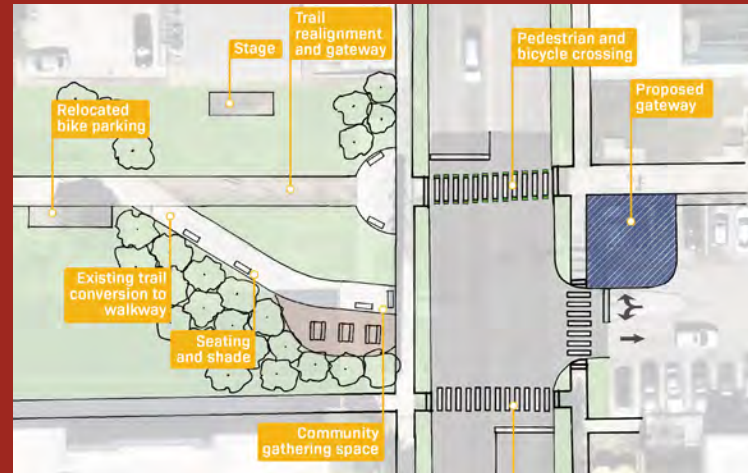


Greater Heights Strategic Connections Plan



MEMORIAL HEIGHTS
REDEVELOPMENT AUTHORITY

tei Planning
+ Design

TOOLE
DESIGN

May 2026



Memorial Heights Redevelopment Authority

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This planning effort was funded by the Memorial Heights Redevelopment Authority (MHRA; TIRZ 5) and the Texas Department of Transportation via a Transportation Alternatives Set-Aside planning grant awarded in 2023.

Consulting Team



Community & Agency Partners

Thank you to the organizations and agencies that provided input and insight to improve the final outcome of the Greater Heights Strategic Connections Plan. This includes the many community organizations who helped spread the word about community engagement efforts. These include:

- | | |
|--|---|
| City of Houston Council District C | Houston Parks Board |
| City of Houston Council District H | Norhill Neighborhood Association |
| City of Houston Public Works | Parent-Teacher Organizations/Associations at multiple elementary and middle schools |
| Clark Pines Civic Association | Shady Acres Civic Club |
| First Ward Civic Council | Super Neighborhood 14 |
| Harris County Precinct 1 | Super Neighborhood 15 |
| Harris County Precinct 2 | Super Neighborhood 22 |
| Harris County Precinct 4 | A Tale of Two Bridges |
| LINK Houston | TIRZ 12 |
| METRO | TIRZ 13 |
| Greater Heights Super Neighborhood Council | Texas Department of Transportation |
| Greater Northside Management District | West End Civic Club |
| Houston-Galveston Area Council | Woodland Heights Civic Club |
| Houston Heights Association | |

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Greater Heights Strategic Connections Plan

Executive Summary



About the Plan

What's in the Plan?

The Greater Heights Strategic Connections Plan is a forward-looking mobility planning document that identifies more than 54 miles of investments across 29 projects to improve safety and connectivity for the people who live, work, study, and worship in the neighborhoods of the Greater Heights.

This document represents a bold vision for mobility investments in the Greater Heights. Together, designing, constructing, and maintaining these projects will require the coordinated effort and funding of many agencies over several years.

The Executive Summary

This Executive Summary offers a review of the analysis, community feedback, in-person field reviews, and stakeholder dialogue across the ten-month planning process from Summer 2025 to Spring 2026. The Executive Summary provides an overview of:

- The Study Area
- Key Existing Conditions Insights
- The Plan's Vision & Goals
- Corridor Needs Analysis
- Recommended Projects
- Implementation Strategy

For additional details on any of these plan elements, please see the relevant chapter or appendix.

Building on Prior Work

The Greater Heights Strategic Connections Plan pulls from and complements the important planning efforts that preceded this document, including the City of Houston Bike Plan, City of Houston and Harris County Vision Zero efforts, and the planning efforts of MHRA and other local agencies.

Funding Source

This plan was created for the Memorial Heights Redevelopment Authority (MHRA) and funded with MHRA's local funds in partnership with a 2023 Transportation Alternatives Set-Aside grant from the Texas Department of Transportation (TxDOT). These statewide grant funds are intended to help local communities identify and define mobility projects that are focused on improving infrastructure for people walking, biking, pushing a stroller, using a wheelchair, and the many other ways that people get around outside of a private vehicle.

About the Study Area

An Active Community

The Study Area for this plan includes several neighborhoods in the Greater Heights as shown in Figure 1. These neighborhoods are defined by their cherished active streets and trails like Nicholson Street, the Shepherd/Durham Corridor, and the White Oak Bayou Greenway where people feel safe walking and biking.



Figure 1. Destinations & Commercial Corridors (Source: Harris County Appraisal District Land Use, 2024; Team Analysis, 2025).

Schools

More than a dozen schools in the Study Area serve families, often near high-speed roadways and unsafe crossings.

Parks

The Study Area boasts neighborhood parks like Love Park and Cottage Grove Park, as well as regional parks like Stude Park and the White Oak Bayou Greenway.

Major Commercial Corridors

Greater Heights businesses attract visitors from across the region to commercial corridors like Shepherd and Durham Drive and 20th Street, as well as developments like the MKT.

Community Centers Several other community resources are located in the Study Area, including the Heights Hospital.

Libraries

Hospitals

About the Study Area

TIRZ Boundaries

The Memorial Heights Redevelopment Authority (MHRA), also known as Tax Increment Reinvestment Zone 5 (TIRZ 5), is the sponsor of this planning effort and a jurisdiction created by the City of Houston. **TIRZs invest in infrastructure projects with the goal to attract additional investment and economic development within and near its boundaries.**

The revenue generated within a TIRZ comes from existing tax revenue. The City sets the property tax base year at the same time the TIRZ is created, and then any increases of that initial revenue each year is called “increment.” The TIRZ Board identifies infrastructure projects via a Capital Improvements Program that is approved by City Council and then implemented by the TIRZ.

MHRA has the ability to leverage debt and utilize federal grant funding to deliver projects. MHRA has experience as a direct recipient of U.S. Department of Transportation funding and is a Federal Transit Administration grantee. **To date, MHRA has leveraged over \$85 million in federal funding.**

As shown in Figure 2, the Study Area boundaries and the MHRA boundaries are not identical. In fact, parts of the Study Area are within both TIRZ 12 and TIRZ 13. MHRA worked closely with the City to ensure that the Study Area could comprehensively address mobility concerns across neighborhoods and along corridors. **This plan is intentionally designed to identify a set of generational investments that stretch beyond the boundaries of the TIRZ and can have a positive impact on connectivity and safety for everyone in the study area.**

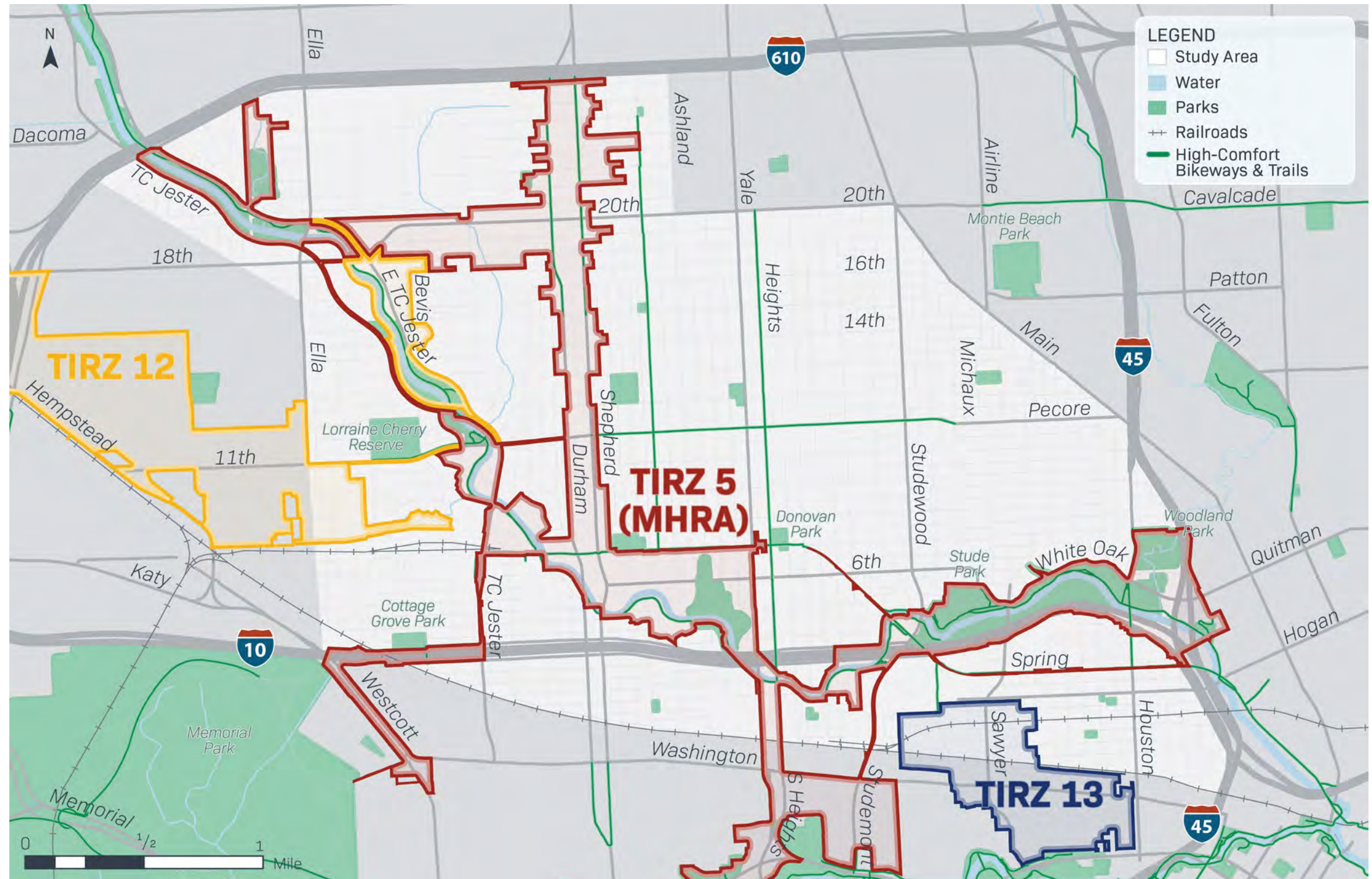


Figure 2. Greater Heights Strategic Connections Plan Study Area Boundary with TIRZ 5, TIRZ 12, and TIRZ 13 Boundaries (Source: City of Houston, 2025)

About the Study Area

Houston City Council Districts

Figure 3 displays the two Houston City Council Districts that share boundaries with parts of the Study Area - District C and District H. District C is represented by Abbie Kamin and District H is represented by Mario Castillo Jr.

The boundary splitting the two council districts follows Main Street from I-610, and then Studewood Street to Washington Avenue.

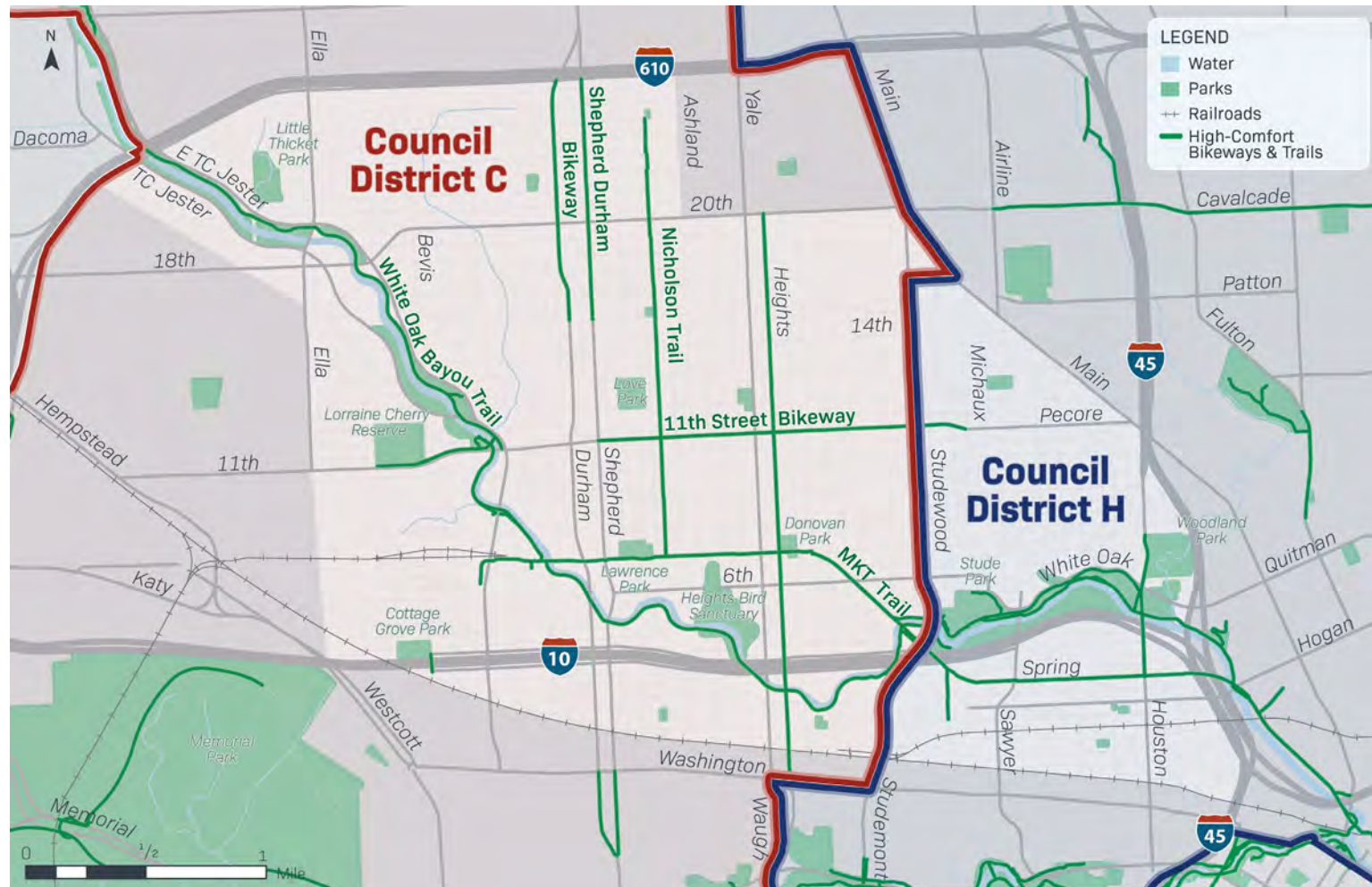


Figure 3. Houston City Council District Boundaries (Source: City of Houston, 2025)

Harris County Precincts

Figure 4 displays the three Harris County Commissioner Precincts with jurisdiction in the Study Area - Precinct 1 (Commissioner Rodney Ellis), Precinct 2 (Commissioner Adrian Garcia), and Precinct 4 (Commissioner Leslie Briones).

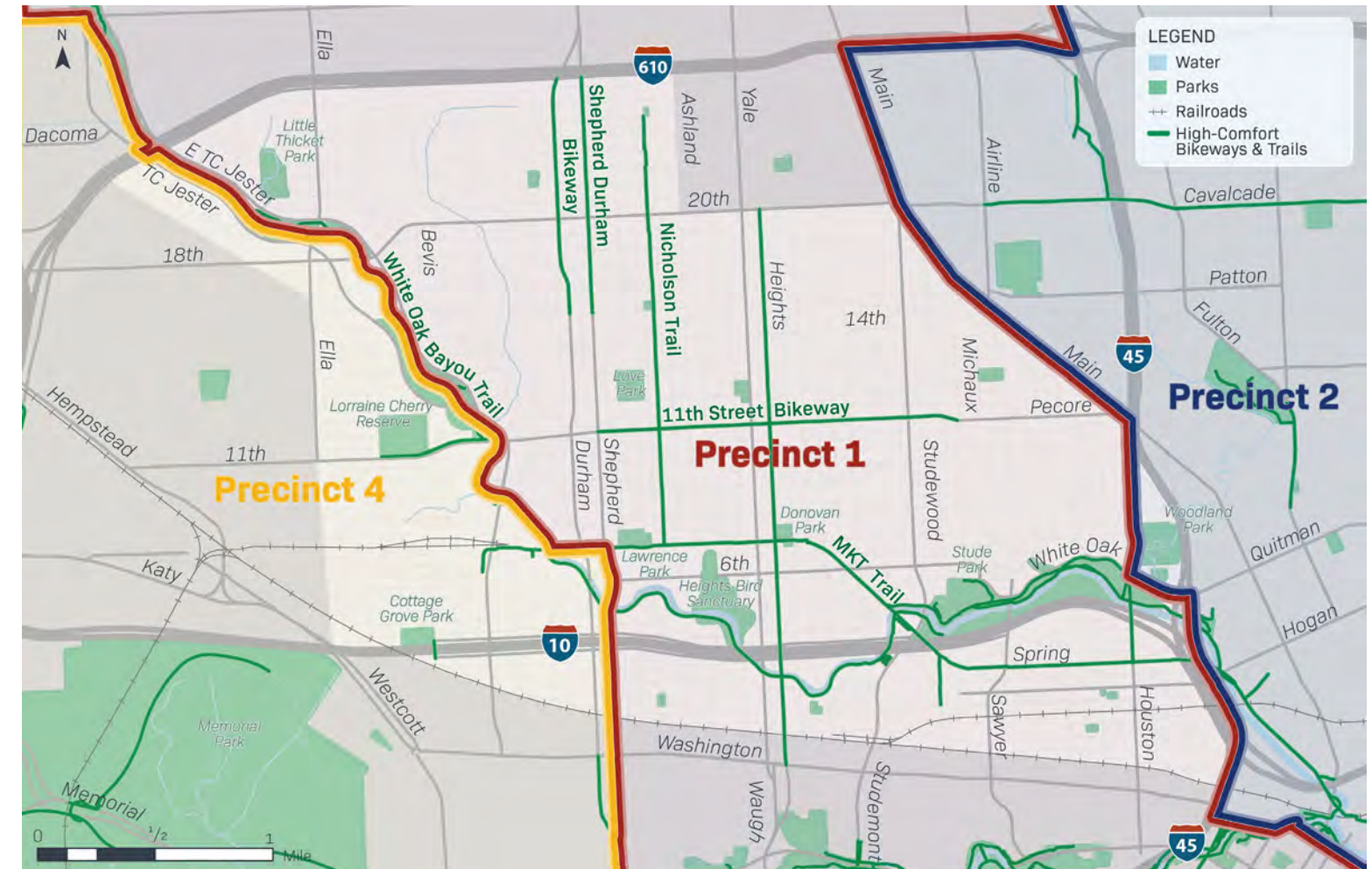


Figure 4. Harris County Precinct Boundaries (Source: Harris County, 2025)

Seven Key Insights from Analyzing Existing Data

Mobility & Demographic Takeaways

This plan includes an in-depth Existing Conditions chapter that showcases key analysis from publicly available data. This analysis reveals a more comprehensive understanding of mobility in Greater Heights, highlighting both challenges and opportunities within the Study Area from the layout and design of the street grid to the location of destinations and the purposes for daily trips.

The seven statements on this page represent the Key Insights, or major takeaways from the Existing Conditions chapter. They offer a baseline to help identify the right projects in the right locations to best serve the needs of the people who travel in Greater Heights every day.

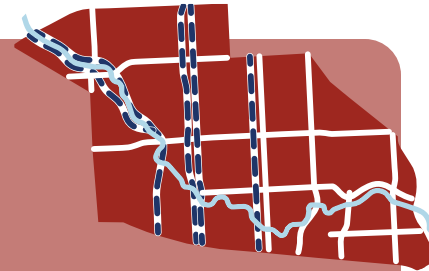
The Greater Heights benefits from **local destinations that provide access to daily essentials** without the need to cross a major highway.



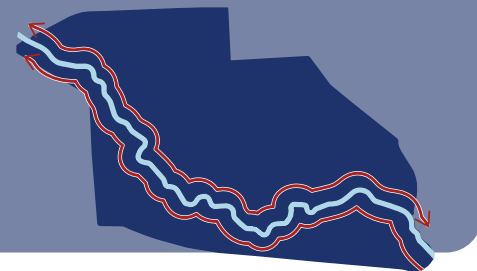
Neighborhoods in Greater Heights are **popular for families with children and older adults** who want to age in place.



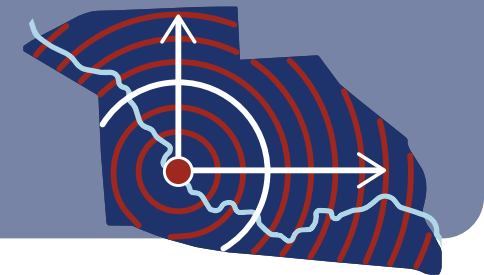
Several **major corridors act as barriers for local travel of all modes** with compounding safety challenges — high vehicle speeds, few crossings and wide roadways. While MHRA is proactively addressing safety and access along Shepherd and Durham, other corridors such as TC Jester Boulevard, Yale Street, N Main Street, and others still have safety and access challenges.



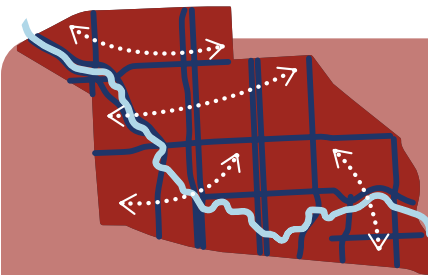
Although it is a great community asset, **White Oak Bayou is a major barrier** with infrequent and unsafe crossings for all modes of travel.



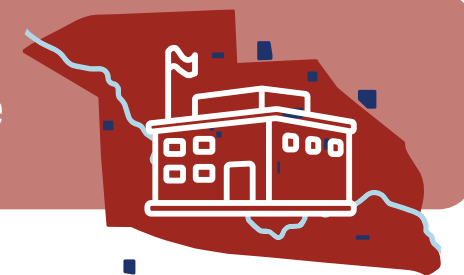
Many daily trips in Greater Heights are less than three miles, but most travelers are still choosing to drive. Safe, well-connected sidewalks, trails, and bikeways would give residents greater mobility choices for those small trips and could help reduce congestion on local streets.



Recent investments have improved overall network connectivity, but **safe east-west connections in northern neighborhoods and north-south connections across I-10 and White Oak Bayou remain a challenge.**



Many **neighborhood schools are located along or near major corridors with high crash rates**, high vehicle speeds, and few safe crossings, making it difficult for students to get to class safely.



A Vision & Goals for Greater Heights Mobility

The Vision & Goals of this plan were developed using feedback from the community and the MHRA Board of Directors during the first stages of the plan. These data-driven goals helped to shape the location and types of investments included in each recommendation.

Vision The Greater Heights is an active community built on an historic street grid where safe, easy mobility choices cultivate a healthy, connected, and dynamic place for people to grow up and grow old.

Goals



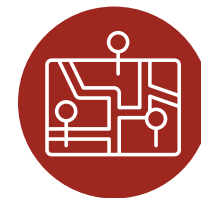
Safe Crossings Goal

Improve crossing conditions on key corridors by **reducing the average distance between safe crossings** to bring major roadways into compliance with city standards.



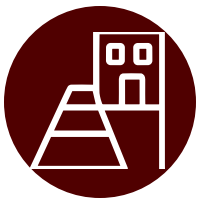
School Streets Goal

Create an environment in which students feel safe walking or biking to school by **implementing safe street infrastructure initially within a 1/4-mile radius of all schools.**



Connected Network Goal

Increase the share of residents who have excellent or direct access (1/8 mile) to active streets and trails from 20 percent to 100 percent by delivering targeted street and intersection projects that connect to everyday destinations and expand the existing bikeway and trail network.



Complete Sidewalks Goal

Close all gaps in the sidewalk network and repair existing sidewalks that provide important connections to local destinations.



Great Spaces Goal

Design streets and trails as destinations, where abundant shade, lighting, and other amenities encourage people to spend time together outside.

Mapping the People-First Network

Identifying Where to Invest

The next step in the planning process utilized the five Goals and the data from the Existing Conditions exercise to develop a Needs Analysis framework. The Needs Analysis follows a multi-step process to identify a *People-First Network*, defined as the network of high-need corridors and intersections where improvements to safety and connectivity would have the greatest impact on people walking, biking, and rolling in the Study Area. Combined with existing active streets and trails, this *People-First Network* highlights a grid of streets where investments are most needed.

Mapping the *People-First Network* in 5 Steps

- 1. What do people need?**

Mapped GIS Data corresponding to the stated goals and other notable destinations and demographic context data
- 2. Where are the biggest barriers?**

Overlaid all mapped features from the first question and processed them into a single prioritization network to identify the areas of highest need for a *People-First Network*
- 3. Which streets rise to the top?**

Visualized the highest-need corridors considering all the combined factors
- 4. What did we confirm in the field?**

Performed in-person fieldwork on the proposed *People-First Network*
- 5. How do we complete a comprehensive *People-First Network*?**

Compared community feedback to the needs analysis and mapped a full grid of streets that was both data-based and community-informed



Figure 5. Needs Analysis Fieldwork Data Points (Source: Team Analysis, 2025)

Recommended Mobility Projects

Investments for Safer Streets

The People-First Network converged into 29 project recommendations for safer and better connected streets along 54 miles of roadways and trail in the Greater Heights. This includes nine Catalyst Projects, shown in gold on the next page. Catalyst Projects are investments to most critical corridors that address specific safety and accessibility concerns on streets in Greater Heights with the highest need.

Beyond Catalyst Projects, other projects focus on ensuring that people-first streets are geographically distributed across the Greater Heights and better move people across barriers. For example, Projects tagged as White Oak Bayou recommendations address access and safety across and along White Oak Bayou. Neighborhood Streets projects address non-thoroughfare corridors that support comfortable walking and biking.

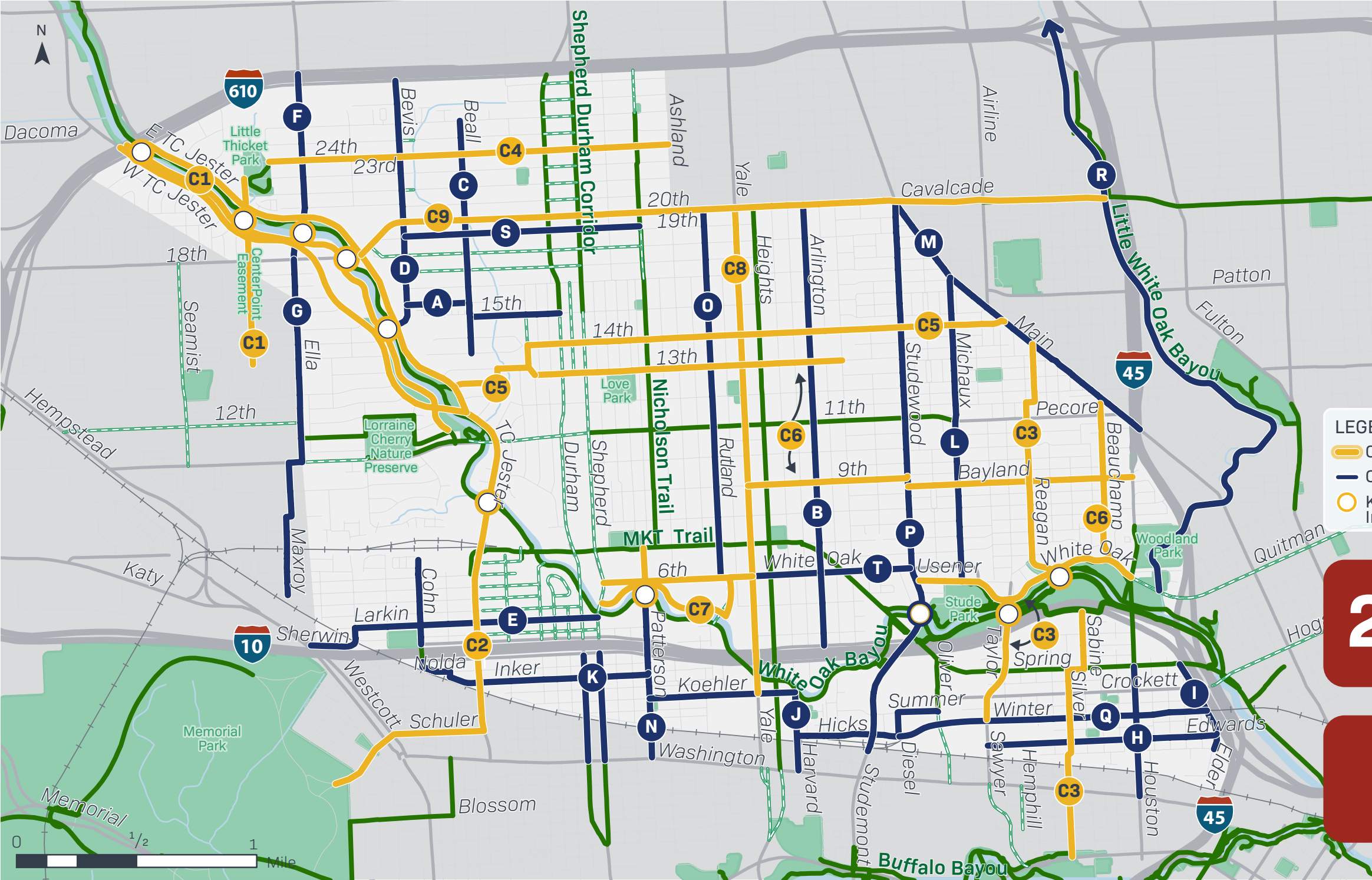
Together, all projects strengthen connections between neighborhoods and nearby schools, parks, destinations, and regional amenities such as White Oak Bayou, Memorial Park, Downtown, and adjacent neighborhoods, supporting the People-First Network.

The Project Recommendations close network gaps, improve crossings of major barriers, and strengthen east-west and north-south connections. Accomplishing this supports a more complete system for walking and biking that expands access to everyday destinations as well as regional trails, parks, and employment centers.



Together, these projects create *Greater Heights Strategic Connections.*

Recommended Mobility Projects



Recommendations

Figure 6 shows the full set of Project Recommendations for the Greater Heights Strategic Connections Plan. Together, these projects help to achieve the Plan’s Vision & Goals by creating a safe, connected grid of *People-First Streets* throughout the Study Area.

These projects build upon the existing network of active streets and trails, and serve as a complement to previous planning efforts like the City of Houston Bike Plan and plans by TIRZ 12 and TIRZ 13.

The table on the following page lists the projects by letter and number corresponding with this map.

29 projects

54+ miles

Figure 6. Greater Heights Strategic Connections Plan Recommended Projects (Source: Team Analysis, 2026)

Recommended Mobility Projects

	ID	Project Name	Pg.	Description	Mi.
Catalyst Projects	C1	White Oak Connectivity Vision	79	New trail on Bayou western bank & CPE easements, retrofit & new bridge improvements, and safer crossings of TC Jester Blvd.	6.6
	C2	White Oak to Memorial Park Connection	82	Retrofit improvements on TC Jester Blvd. & Schuler St. for safe walking/biking link from MKT Trail & WOB to Memorial Park	2.1
	C3	Heights to Buffalo Bayou Corridor	84	Retrofit Reagan, Silver, Taylor, & White Oak Sts., Rebuild Sabine & Usener Sts., and a new bridge for connection over WOB & I-10	3.8
	C4	Shady Acres Greenway	86	Reconstruction of W 24th, 23rd, or 22nd Sts with shared-use path & safety improvements from Little Thicket Park to Nicholson St.	1.7
	C5	14th Street Reconstruction	88	Reconstruction of 14th, Grovewood, and Wynnwood Sts. to formalize an east-west Heights link from WOB to N Main St.	2.5
	C6	Heights School Streets	91	Reconstruction/Retrofit Neighborhood Street improvements to 13th, 9th, Bayland, & Beauchamp Sts. for safe school access	3.5
	C7	MKT Trail/Bird Sanctuary Loop	93	New bayou trails, new bridges, & improved (retrofit) local street connections to MKT Trail & local commercial destinations	1.7
	C8	Yale Street Improvements	95	Phased retrofit safety improvements to include sidewalk & crossing improvements with long-term reconstruction options	2.0
	C9	20th Street Reconstruction	99	Reconstruction as two-lane roadway with center turn lane, median, and wider sidewalks with median where ROW allows	3.2
Projects	A	W 15th/W 15 1/2 Streets	101	Retrofit Neighborhood Street improvements; east-west connections from Bevis St. to the Shepherd-Durham Corridor	0.7
	B	Arlington Street	102	Retrofit Neighborhood Street improvements; north-south connections from I-10 to W 20th, incl. safe school streets	1.9
	C	Beall Street	102	Reconstruction Neighborhood Street improvements north of 20th St. & Shared Use Path from 20th St. to 14th St.	1.0
	D	Bevis Street	103	Reconstruction Neighborhood Street improvements north of 20th St. & Shared Use Path from 20th St. to E TC Jester Blvd.	1.0
	E	Cohn Street/Larkin/Sherwin	103	Reconstruction Neighborhood Street improvements in Cottage Grove for future connection to Memorial Park	1.7
	F	Ella Blvd (N)	104	Reconstruction of Ella Blvd. north of 18th St. with safer intersections and curb changes to accommodate a shared-use path on both sides	0.8
	G	Ella Blvd (S) & Maxroy Street	104	Reconstruct Ella Blvd. south of 18th per TIRZ 12 Mobility Plan recommendations; Redesign Maxroy St. as Neighborhood Street	1.3
	H	Edwards Street & Houston Avenue	105	Redesign Edwards St. and Houston Ave. per TIRZ 13 Mobility Plan; for Edwards St., continue improvement west to Elder St.	1.5
	I	Elder Street Trail	105	Build extension of the Spring St. Trail south to Dart St. as part of TxDOT's NHHIP project for improved north-south connectivity	0.4
	J	Harvard/Hicks/Diesel/Summer Streets	106	Retrofit as Neighborhood Streets for better access between Bayou and commercial destinations; Reconstruction of Oliver St.	1.0
	K	Shepherd/Durham & Inker/Koehler	107	Reconstruction of Shepherd & Durham south of I-10 along with key Neighborhood Streets to provide safer east-west access	2.5
	L	Michaux Street	107	Retrofit Neighborhood Street improvements for safer north-south connections from Bayou to N Main St.	1.3
	M	N Main Street	108	Reconstruction to accommodate vehicle volumes & improve visibility and safety of crossings and turning movements	1.4
	N	Patterson Street	109	Retrofit as a Neighborhood Street for improved north-south connections from Bayou, over I-10, and toward Washington Ave.	0.6
	O	Rutland Street	109	Retrofit as Neighborhood Street for improved north-south connections from Bayou to 20th St.	1.6
	P	Studemont & Studewood Streets	110	Retrofit improvements to add safe crossings & Shared Use Paths behind the curb where feasible	2.4
	Q	Winter Street Promenade	110	Reconstruction to accommodate a wide Shared Use Path as outlined in TIRZ 13 Mobility Plan	1.3
	R	Little White Oak Bayou Extension	111	New trail with underpass crossings of major barriers and connection to existing Little White Oak trail at Cavalcade St.	3.0
	S	W 19th Street	111	Reconstruction to include boulevard between Bevis Street and Nicholson Trail, connecting two commercial corridors	1.1
	T	White Oak Drive	112	Retrofit improvements to enhance crossing safety, visibility, and turning movements from Arlington St. to Studewood St.	0.7
		Total Project Mileage			54+

Figure 7. Recommended Projects Table (Source: Team Analysis, 2026)

Together, these projects add new ways to access White Oak Bayou.



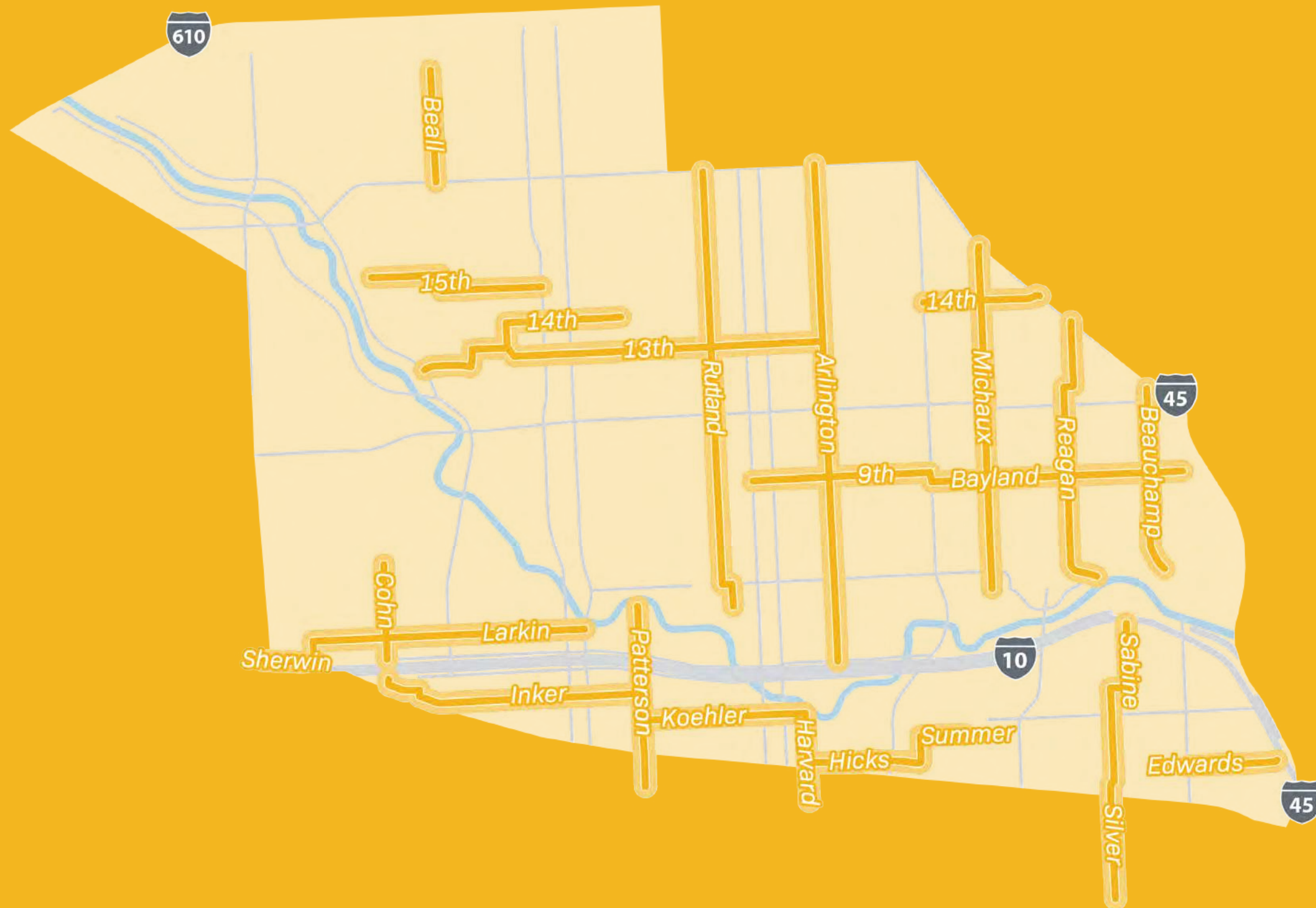
Projects add **more than 36 miles** of stronger neighborhood connections to **White Oak Bayou Greenway**, including 3.2 new miles of Bayou trails.

Together, these projects create new & safer connections across major barriers.



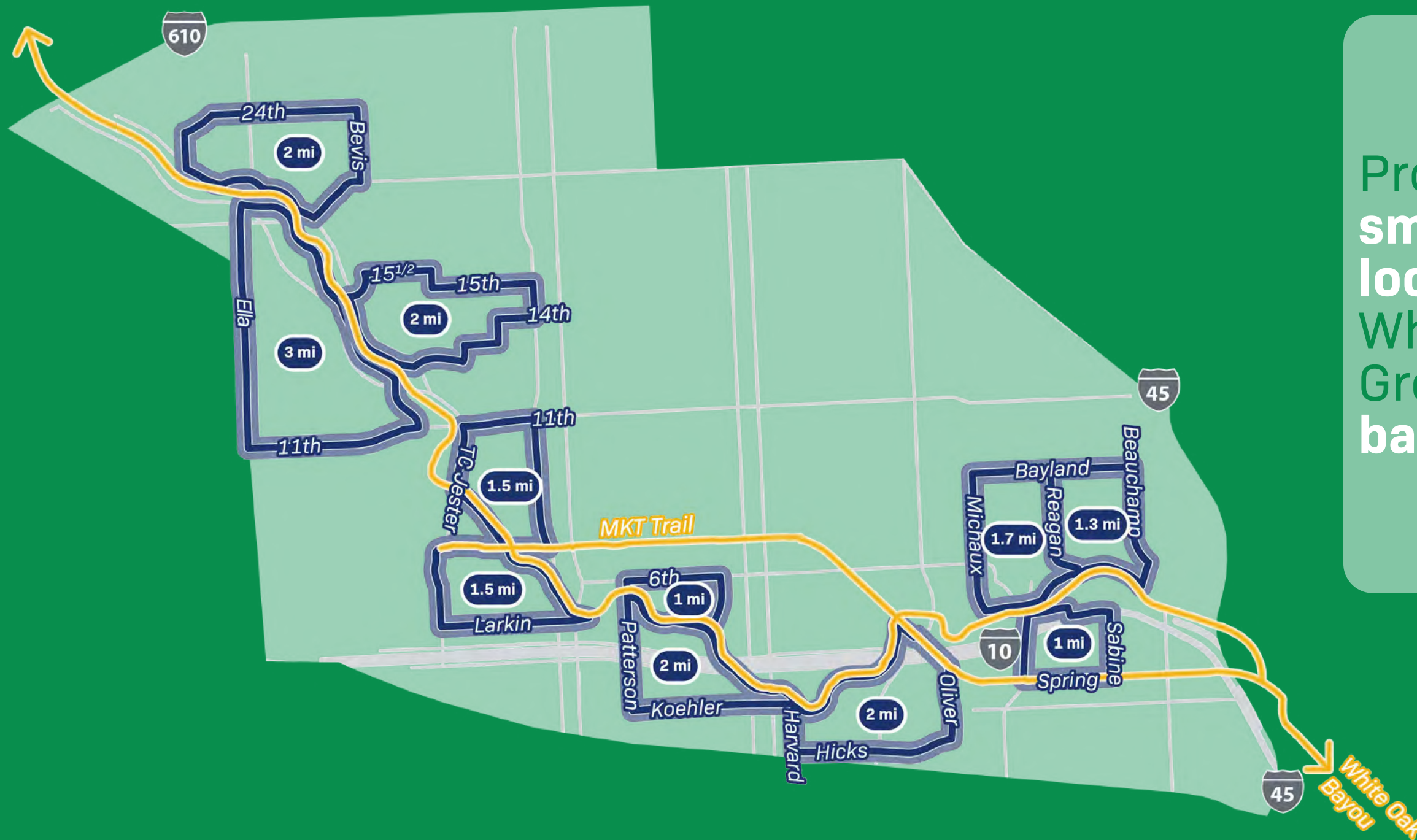
When combined with existing & programmed projects, this plan improves **6 I-10 crossings, 9 Bayou crossings, & adds 5 new Bayou bridges.**

Together, these projects build great local streets to grow up & grow old.



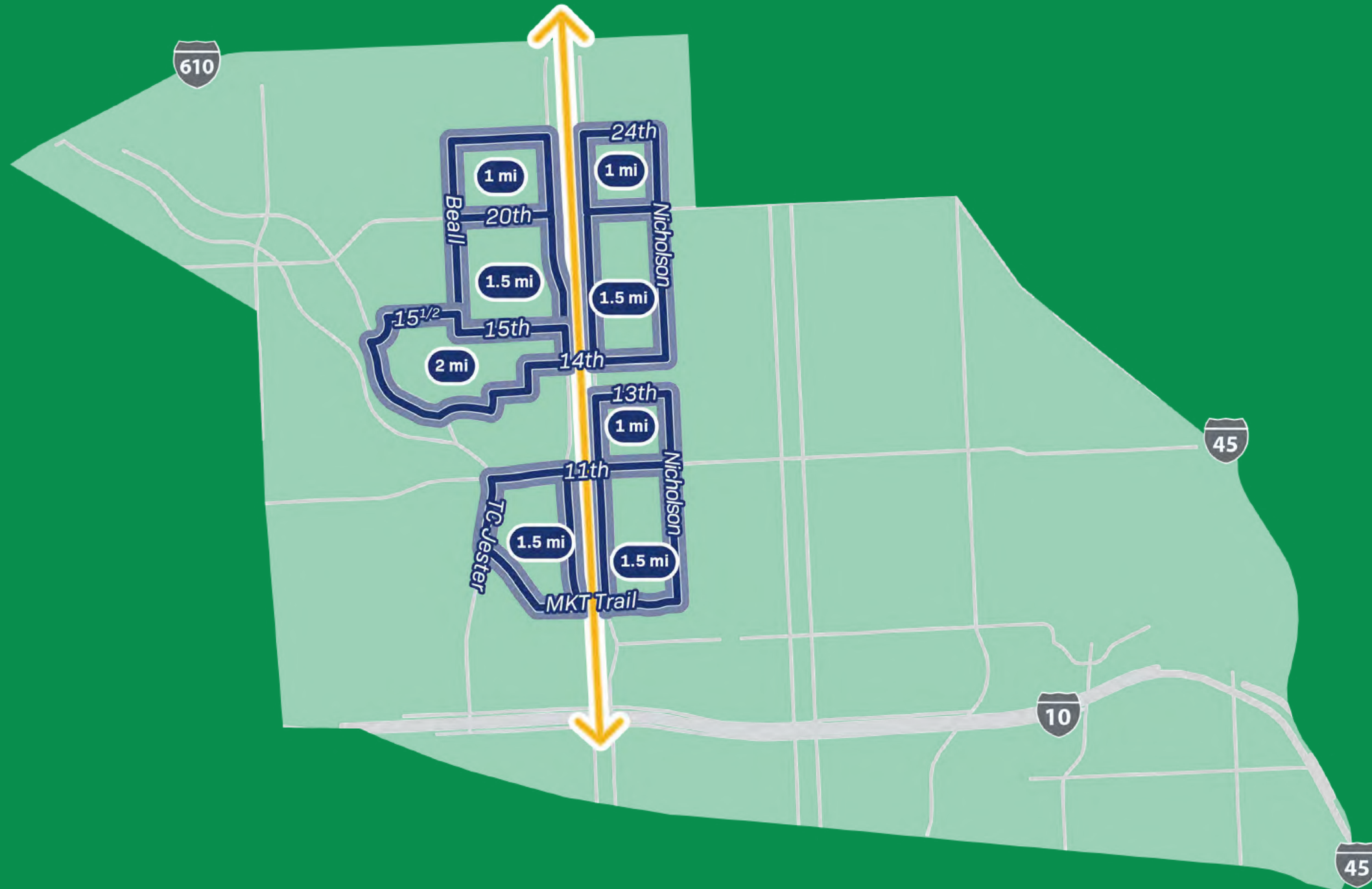
Projects create **more than 18 miles of Neighborhood Streets** where safer local roadways help foster a **sense of place & connection.**

Together, these projects bring the Bayou & local street grid closer together.



Projects establish small neighborhood loops that make White Oak Bayou Greenway feel like a backyard amenity.

Together, these projects unlock new ways to get to the Shepherd-Durham Corridor.



Projects establish **small neighborhood loops** that redefine how people **stroll & shop** along the Shepherd-Durham Corridor.

Implementation of a Generational Investment

Practical Steps for a Visionary Plan

Together, the Project Recommendations represent a generational level of investment for projects that span an area well outside of the existing MHRA (TIRZ 5) boundaries. As a result, **full implementation of these projects will take collaboration among multiple agencies and several years.**

The full plan document includes a detailed table that lists potential implementing agencies, funding sources, phasing, and a planning-level cost estimate for design and construction for each of the 29 recommended projects. Together, these investments total more than \$576 million. This plan will serve as **a playbook for all agencies to identify and advance community-vetted projects that focus on safety and mobility in the Greater Heights.**



29 projects

54 miles

\$576 M
Planning-level cost estimate



Existing Conditions

Chapter One

The Greater Heights Strategic Connections Plan



**Existing Conditions
Key Insights**

**Plan & Project
Review**

About the Study Area

**Who lives in the
Study Area?**

**Today's
Transportation
Network**

Key Safety Indicators

Key Insights from Existing Conditions

Major Takeaways from Mobility & Demographic Data

This Existing Conditions document reviews publicly available data as well as data provided by MHRA and its partners to unearth a more comprehensive understanding of mobility in Greater Heights. This data highlights both challenges and opportunities within the Study Area from the layout and design of the street grid to the location of destinations and the purposes for daily trips.

The seven statements on this page represent the Key Insights, or major takeaways from the Existing Conditions exercise. They offer a baseline for the next steps of this planning effort and ultimately to help identify the right projects in the right locations to best serve the needs of the people who travel in Greater Heights every day.

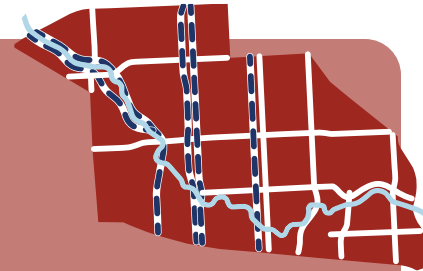
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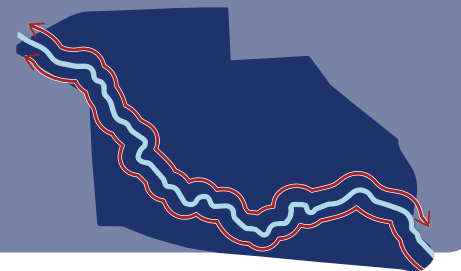
Neighborhoods in Greater Heights are **popular for families with children and older adults** who want to age in place.



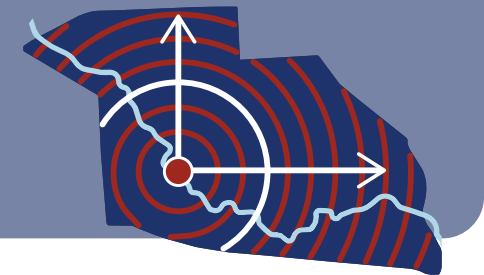
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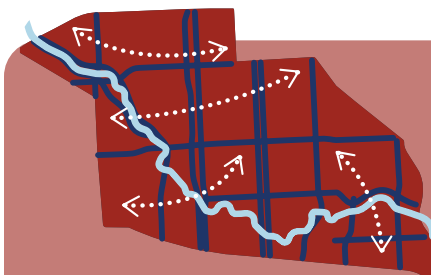
Although it is a great community asset, **White Oak Bayou is a major barrier** with infrequent and unsafe crossings for all modes of travel.



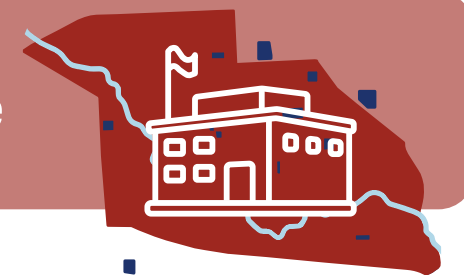
Many daily trips in Greater Heights are less than three miles, but most travelers are still choosing to drive. Safe, well-connected sidewalks, trails, and bikeways would give residents greater mobility choices for those small trips and could help reduce congestion on local streets.



Recent investments have improved overall network connectivity, but **safe east-west connections in northern neighborhoods and north-south connections across I-10 and White Oak Bayou remain a challenge.**



Many **neighborhood schools are located along or near major corridors with high crash rates**, high vehicle speeds, and few safe crossings, making it difficult for students to get to class safely.



Plan & Project Review

Review of Previous Planning Efforts

For more than a decade, MHRA and other stakeholders have developed several plans and projects aimed at improving street safety, connectivity, and comfort in the Greater Heights area and beyond. These plans emphasize things including safety, streetscape enhancements, and accessibility for people with disabilities, and have highlighted the public’s recurring emphasis on improving the roadway and trail network for all road users.

Local Efforts

- TIRZ 5 Three Study Areas Mobility & Drainage Study
- City Park | TIRZ 12 Mobility Plan
- Envision Old 6th Ward TIRZ: A Mobility Plan for the Future
- Old 6th Ward/TIRZ 13 Phase II Mobility Plan
- All in 4 Inner Northwest Community Plan by Harris County Precinct 4
Note: This plan was not publicly released upon completion of this Plan Review.

Regional Efforts

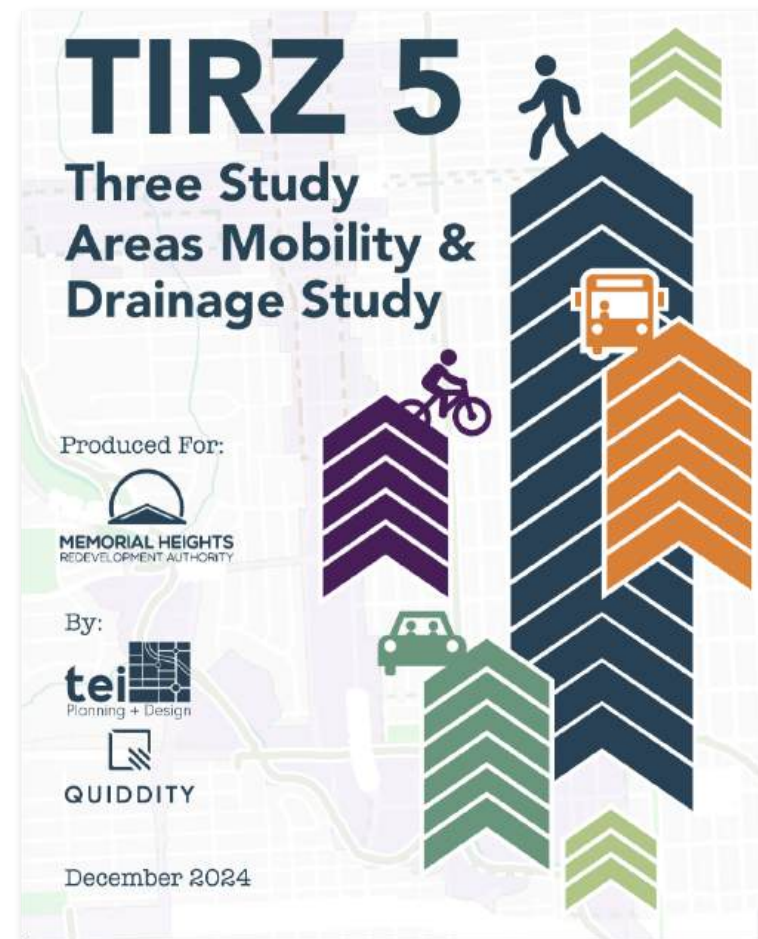
- Beyond the Bayous - Houston Parks Board
- Houston Bike Plan - City of Houston
- Vision Zero Action Plan - City of Houston and Harris County
- Streetscape Resource Guide - Scenic Houston

Taking stock of previous plans and projects is critical to the mobility planning process as these documents highlight ideas that have already been considered and where gaps may exist in existing recommendations. For example, the *Houston Bike Plan* proposes a high-level vision for bikeways across the City of Houston, while the *City Park | TIRZ 12 Mobility Plan* focuses on specific roadway design considerations for the TIRZ itself, while still referencing the *Houston Bike Plan* in its recommendations. This keeps the smaller area plans in alignment with the regional vision to ensure that both neighborhood and city-wide networks and connections can be made incrementally.

In addition to documenting previous recommendations, this analysis offers insight into prior community engagement efforts in the Study Area. Community members and local elected officials played a key role in shaping these planning efforts. Through that engagement and this Plan and Project Review, MHRA can better calibrate the recommendations of this Greater Heights Strategic Connections Plan to address the consistent public input from residents, workers, and businesses.



TIRZ 5 Three Study Areas Mobility & Drainage Study



**Memorial Heights
Redevelopment Authority**
December 2024

The *Three Study Areas Mobility & Drainage Study* was completed by MHRA in December 2024. The plan outlines recommendations aimed at improving mobility and drainage in three distinct study areas near the Shepherd Durham Corridor, the Missouri Kansas Texas (MKT) Trail, and White Oak Bayou. Recommended mobility investments would improve connectivity and safety for all modes and address street parking needs. Drainage improvements would provide additional stormwater detention.

Relevant Projects, Policies, & Programs

- Reconstruct multiple roadway segments between Shepherd Drive and Durham Drive to create better connections and complete the reconstruction of the corridor.
- Reconstruct streets near the MKT development for improved access to the MKT Trail, Lawrence Park, and retail.
- Reconstruct and repave east/west corridors between the Shepherd Durham project and White Oak Bayou, including 19th Street.
- Improve the safety and connectivity of study area trails with improved crossings at neighborhood streets along the MKT Trail and Nicholson Trail.
- Improve drainage: provide additional stormwater detention, support Shepherd-Durham Project.
- Improve area walkability with upgraded sidewalks and ramps.
- Improve on-street parking for residents and area visitors: reconfigure and formalize on-street parking pending the reconstruction of corridors.

For a map of projects included in this plan, see Figure 6.

City Park | TIRZ 12 Mobility Plan

The *City Park | TIRZ 12 Mobility Plan* was completed by the City Park Redevelopment Authority in August 2024. The plan highlights several recommendations which focus on creating and expanding the area’s multimodal transportation network, community spaces, and transportation safety.

Relevant Projects, Policies, & Programs

- Create a multimodal network through the expansion of pedestrian, bicycle, and transit infrastructure that complements existing trails and bikeways.
- Implement projects on Ella Boulevard and 18th Street (reconstruction), Bevis Street and Grovewood Lane (retrofit), and the CenterPoint Corridor and west bank of White Oak Bayou (new trails) to expand facilities for people walking and biking.
- Retrofit corridors such as Bevis Street and Grovewood Lane to include improvements such as protected bike lanes and neighborhood safe street improvements
- Leverage infrastructure improvements by other partnering agencies to capitalize on regional efforts that support economic development.
- Expand access to existing recreational opportunities, green spaces, and destinations such as the Lorraine Cherry Nature Preserve and White Oak Bayou.
- Implement projects that create separate, well-lit and signalized facilities for all modes with a priority for intersections with high incidences of crashes.

For a map of projects included in this plan, see Figure 6.



City Park Development Authority
August 2024

Old 6th Ward/TIRZ 13 Phase II Mobility Plan



The *Old 6th Ward/TIRZ 13 Phase II Mobility Plan* was completed by TIRZ 13 and H-GAC in 2024. The plan outlines several recommendations for improved landscaping and placemaking, better access for people who walk, bike or have a disability, and improved connectivity across mobility barriers like railroads.

Relevant Projects, Policies, & Programs

- A vision for Houston Avenue, including a railroad underpass, landscaping, and sidepaths behind the curb.
- Traffic calming on Crockett Street, especially near Crockett Elementary and an updated signal and intersection at Crockett Street and Houston Avenue.
- New sidewalks and curb ramps for streets like Shearn Street, Silver Street, and Taylor Street.
- New sidewalks, curb ramps, and trail crossing treatments along Spring Street and the MKT Trail.
- Strategic partnership opportunities.
- Sidewalk prioritization program.

For a map of projects included in this plan, see Figure 6.

Old 6th Ward (TIRZ 13) and Houston-Galveston Area Council 2024

Envision Old 6th Ward TIRZ: A Mobility Plan for the Future

Envision Old 6th Ward TIRZ: A Mobility Plan for the Future was completed by the Old 6th Ward TIRZ (TIRZ 13) in July 2021. The plan focuses on creating a more active community and vibrant atmosphere through revitalizing neighborhood blocks and improving active transportation networks throughout the area.

Relevant Projects, Policies, & Programs

- Bridge the gaps between people and great places while building on the historic street grid.
- Encourage an active, walkable community for people of all ages and abilities through a District Sidewalk Program and a Safe Intersection and Street Crossing Program.
- Transit service enhancements and accessibility improvements for the 44 Acres Homes and the 30 Clinton/Ella.
- Reconstruction of streets such as Sawyer Street and Oliver Street to include safe and comfortable facilities for all modes of transportation.

For a map of projects included in this plan, see Figure 6.



Old 6th Ward TIRZ (TIRZ 13) July 2021

Beyond the Bayous



Houston Parks Board November 2018

The *Beyond the Bayous* Plan was completed for the Houston Parks Board in November 2017. The plan discusses several high-level connectivity improvements across Houston's bayous such as converting underutilized right-of-way to trails, adding new parks in neighborhoods that lack open space, and more connections in neighborhoods along the bayous.

Relevant Projects, Policies, & Programs

- Convert underutilized right-of-way to greenways to create regional connections between the bayous. This includes right-of-way in CenterPoint easements like the one immediately west of Little Thicket Park that crosses White Oak Bayou in the Study Area. It also includes a recommendation to extend the MKT Trail west along the existing rail lines and Old Katy Road to Post Oak Road.
- Build new trails along minor bayous such as Little White Oak Bayou in the east of the Study Area.

Houston Bike Plan

The *Houston Bike Plan* was completed in 2017 for the City of Houston and establishes a ten-year vision for Houston's bikeway network. As Houston grows and the trend of cycling for commuting and recreation increases, the *Houston Bike Plan* plays a crucial role in understanding the importance of cycling and why Houston needs more safe and accessible facilities and routes.

Relevant Projects, Policies, & Programs

- Provide a safer bicycle network for people of all ages and abilities through improved facilities, education, and enforcement.
- Multimodal retrofits or full reconstruction of roadways to include high-comfort pedestrian and bicycle infrastructure.
- Provide shared-use paths, on- and off-street bicycle facilities, and neighborhood safe streets that connect people to activity centers like commercial, employment, and recreation areas.

Selection of Recommended Bikeways in the Study Area

- On-Street Bikeways: Houston Avenue, Crockett Street, White Oak Drive, Taylor/Sawyer Street, Ella Boulevard, Pecore Street, TC Jester Boulevard
- Off-Street Bikeways and Shared-Use Paths: Shepherd Drive/Durham Drive, 19th Street
- Neighborhood Safe Streets: 18th Street, 23rd Street, 24th Street, 14th Street, Watson Street, and Cornish Street



City of Houston February 2017

Houston Vision Zero Action Plan



City of Houston
November 2020

The *Houston Vision Zero Action Plan* was completed for the City of Houston in November 2020. The plan highlights the importance of transportation safety for all modes for all people and presents actions to take to achieve zero traffic-related deaths by 2030. The plan defines a **High-Injury Network**, which identifies the six percent of roadways where 60 percent of fatal and serious-injury crashes occur.

Relevant Projects, Policies, & Programs

- Prioritize Vision Zero goals in Houston’s Infrastructure Design Manual and evaluate every roadway reconstruction project and ensure safety for people walking, biking, rolling, and driving.
- Encourage pedestrian right-of-way at intersections and mid-block crossings and construct 50 miles of sidewalks and 25 miles of high-comfort bikeways each year.
- Redesign ten locations on the High-Injury Network every two years.

Study Area Corridors with Segments on the High-Injury Network

Ella Boulevard, Yale Street, Shepherd Drive, Durham Drive, Studemont Street, Inker Street, E 4th Street, W 18th Street, W 24th Street, and W 27th Street.

Note: These Study Area corridors are on the **Harris County High-Injury Network**, but not the City of Houston network: Larkin Street, TC Jester Boulevard, Fowler Street, Sawyer Street, Houston Avenue, Heights Boulevard, W 6th Street, W 11th Street, W 20th Street, and W 26th Street.

Streetscape Resource Guide

The third edition of the *Scenic Houston Streetscape Resource Guide* was completed in 2025 by Scenic Houston, a non-profit organization advocating for improvements to the overall experience of public spaces in the City. The guide shares recommendations from materials and landscaping to signage and public art to create a more appealing and safe streetscape.

The guide highlights both national and local examples of well-designed streetscapes as a resource and inspiration for local Houston stakeholders involved in the planning, design, engineering, and maintenance of local roadways.

Relevant Projects, Policies, & Programs

- Improve the pedestrian realm: add pedestrian connections, more comfortable transit stops, landscape buffers, public art, lighting, and street furniture.
- Improve the travelway realm: provide on-street parking with curb extensions, landscaped medians, placemaking, protected bikeways, and clear signage.
- Improve the shared realm (intersections): design accessible intersections for people who walk, bike, roll, ride transit, or drive using lighting, high-visibility crosswalks, and ramps.



Scenic Houston
2025

Programmed Projects

MHRA and other agencies have programmed mobility, parks, and drainage projects in the Study Area, see Figure 1. **What does “Programmed” mean?** Programmed projects have been allocated funding for implementation and often appear in an agency’s Capital Improvement Program (CIP). The final design of these projects may change due to a shift in priorities and conditions.

The following pages contain a full list of Programmed Projects, and the implementing agencies. Projects include:

- Corridor Projects
- Intersection Projects
- Parks & Other

Note: Map colors have no specific meaning and are only intended to help distinguish project boundaries.

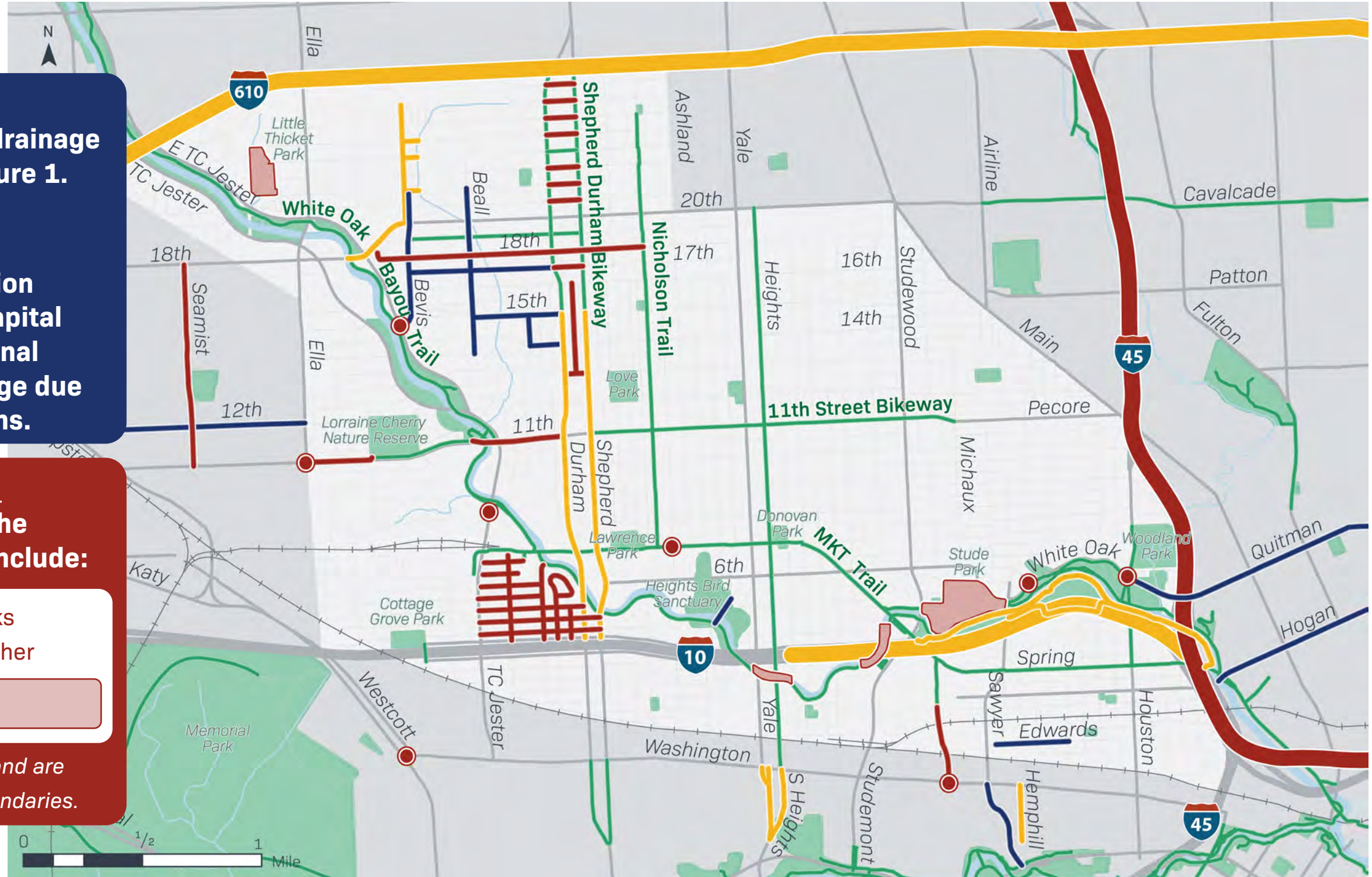


Figure 1. Programmed Projects in the Study Area (Source: MHRA, City of Houston, Harris County, TxDOT, TIRZ 12, TIRZ 13, 2025)

Programmed Corridor Projects

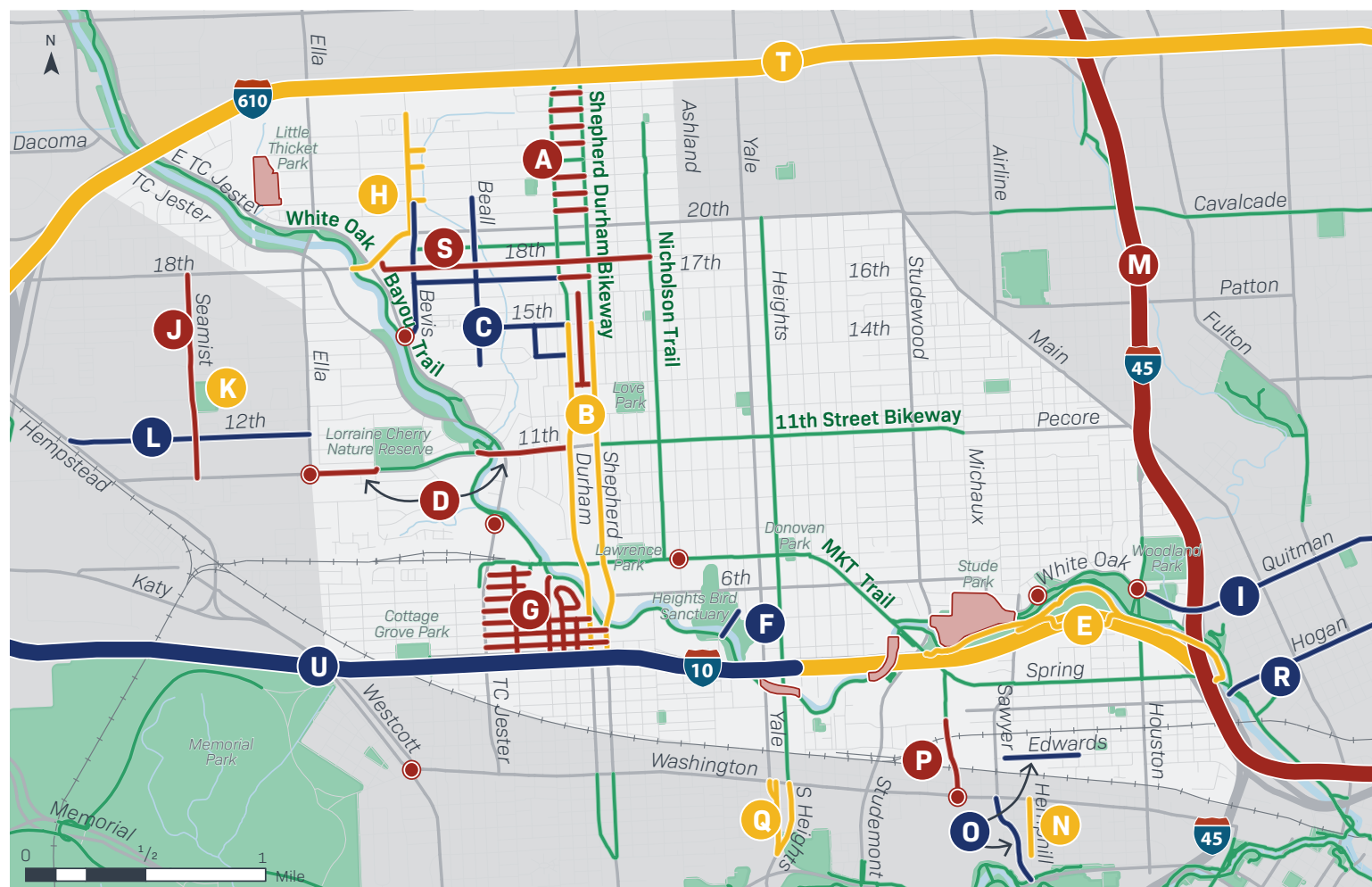


Figure 2. Programmed Projects in the Study Area (Source: MHRA, City of Houston, Harris County, TxDOT, TIRZ 12, TIRZ 13, 2025)

MHRA, the City of Houston (COH), Congressional District 7 (CD7, Representative Lizzie Fletcher), the Texas Department of Transportation (TxDOT), Harris County Precincts 2 and 4, and the Greater Northside Management District (GNMD) all have programmed corridor projects in the Study Area.

These projects, shown in Figure 2 and described in the table (Figure 3), vary from sidewalk construction to large highway reconstruction and elevation.

LEGEND

- Water
- Park/Greenspace
- + Railroad
- High-Comfort Bikeway & Trails

Programmed Projects

- ■ ■ ■ ■

Project (Agency)	Description
A Shepherd Durham Cross Streets (MHRA)	Reconstruction of roadway including pavement and utilities on: W 28th, W 27th, W 26th, W 25th, W 23rd, W 22nd, W 21st, W 17th, W 13th, and Laird streets
B Shepherd Durham Project (Phase 2) (MHRA & COH)	Reconstruction of Shepherd Drive and Durham Drive from I-10 to W 15th Street to include drainage improvements, streetlights, bikeways, sidewalks, and landscaping. This project is an extension of Phase 1 improvements north to I-610.
C CD7 Sidewalk Improvements (MHRA & CD 7)	Assessment, design, and construction of high-priority sidewalk improvements as part of a Community Project Funding request facilitated by U.S. Rep. Lizzie Fletcher of Texas's CD7
D Lorraine Cherry-White Oak Bayou Connectivity (MHRA)	Extension of the existing path from Shirkmere Road west to between Ella Blvd and from White Oak Bayou east to Durham Drive
E I-10 White Oak Bayou Project; CSJ: 0271-07-326 (TxDOT)	Elevation of I-10 to allow additional stormwater detention beneath the highway; to include construction of trail on south bank of White Oak Bayou
F Rutland Bridge; CSJ: 0912-72-581 (TxDOT)	Construction of a bridge over White Oak Bayou to connect the trail on the southern side to Rutland Place
G Cottage Grove East (COH)	Reconstruction of local streets east of TC Jester Boulevard to include curb and gutter drainage and sidewalk infrastructure
H Turkey Gully Project (COH)	Reconstruction of local streets to improve drainage from Turkey Gully; may include mobility improvements and utility upgrades
I Quitman Street Project (Harris Co. Precinct 2 & GNMD)	Reconstruction of Quitman Street from Houston Avenue to Elysian Street, providing new sidewalks, trees, lighting, safe crossing improvements, and bi-directional bike lanes
J Seamist Drive Improvements (TIRZ 12, Harris Co. Precinct 4)	Roadway reconstruction between 11th Street and 18th Street to include safety buffers, new sidewalks, and drainage improvements
K Multimodal Connectivity to Jaycee Park (TIRZ 12)	Construct safe multimodal infrastructure to Jaycee Park for residents in Lazybrook/Timbergrove; roadways for improvement to be determined
L 12th St Impr. (TIRZ 12, Harris Co. Precinct 4)	Roadway reconstruction to include curbed roadways with 6-foot safety buffers and new drainage
M North Houston Highway Impr. Proj.; CSJ: 0500-03-560 & 0500-03-599 (TxDOT)	Reconstruction and widening of I-45, including a re-routing of I-45 to the east side of Downtown; includes reconstruction of frontage roads
N Hemphill Street (TIRZ 13)	Reconstruction, improving pavement conditions, walkability, updating drainage, and constructing new sidewalks
O Sawyer & Edwards Streets (TIRZ 13)	Roadway safety measures to strengthen the connection to Buffalo Bayou, including sidewalk improvements and safe crossings
P Oliver Street (TIRZ 13)	Reconstruction of Oliver Street from Washington Avenue to Summer Street
Q Waugh & S. Heights Improvements (MHRA)	Safety and mobility improvements on Waugh, South Heights, Yale and Waughford between the bridge over Memorial Drive and Washington Avenue
R Reimagine Hogan St (TIRZ 21, Harris Co. Precinct 2)	Engineering study to improve mobility and safety along Hogan Street
S 18th St. Impr. (MHRA)	Street rehabilitation project to improve safety and access along 18th Street
T I-610; MPO IDs 19326, 19327 (TxDOT)	Safety, operational and mobility improvements to I-610 N mainlanes and frontage roads
U Inner Katy, CSJs: 0271-07-342/344/346 (TxDOT)	Reconstruct 10 mainlanes and two 3-lane frontage roads. Construct 4 new non-tolled managed lanes and drainage improvements.

Figure 3. Programmed Projects in the Study Area (Source: MHRA, City of Houston, Harris County, TxDOT, TIRZ 12, TIRZ 13, 2025)

Programmed Intersection, Parks & Other Projects

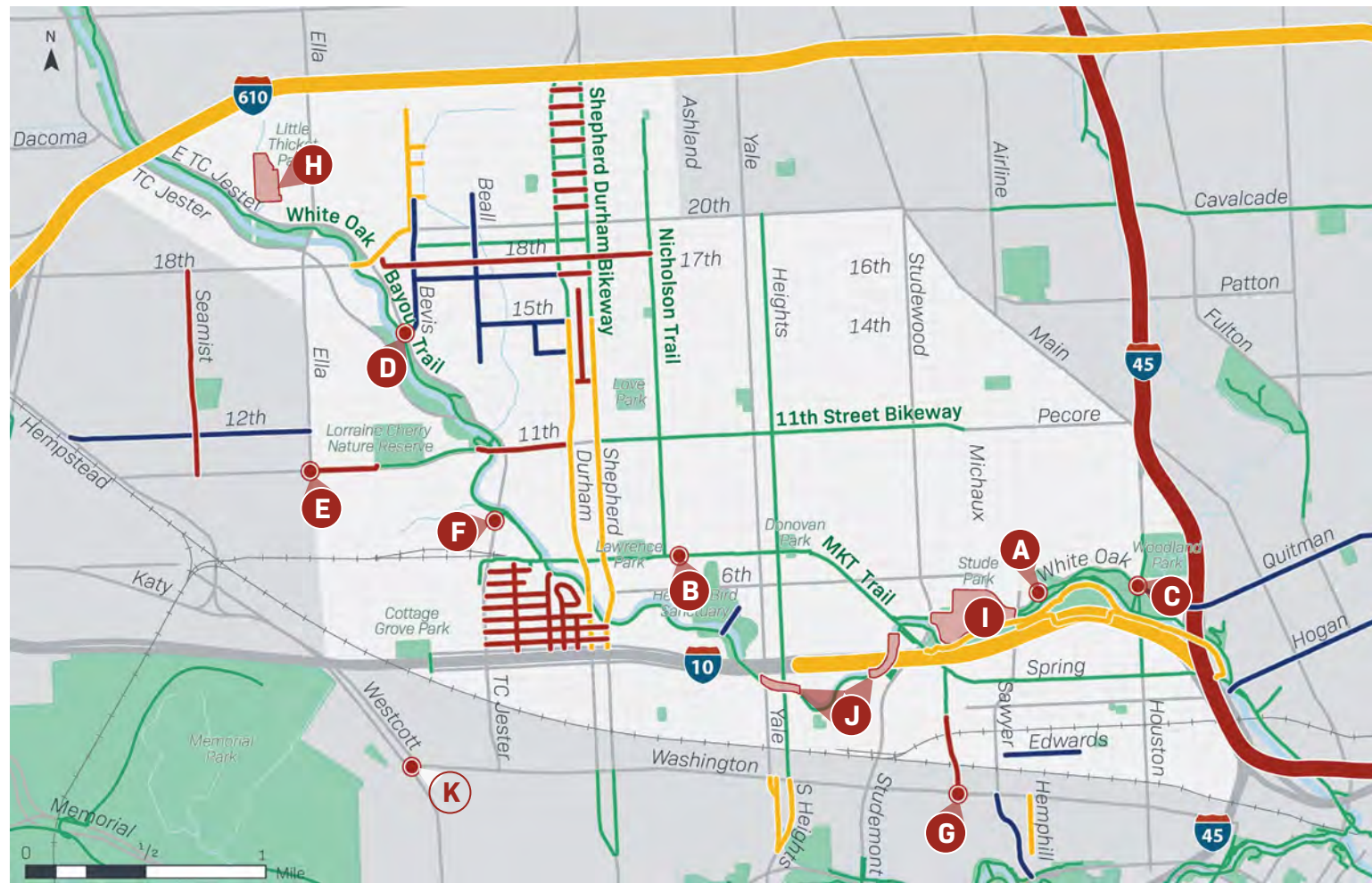


Figure 4. Programmed Projects in the Study Area (Source: MHRA, City of Houston, Harris County, TxDOT, TIRZ 12, TIRZ 13, 2025)

In addition to corridor projects, MHRA, the City of Houston, and the Harris County Flood Control District (HCFCD) have programmed several intersection, parks, and other projects such as the North Canal Project in various stages of planning and construction.

Figure 4 maps these programmed projects, which are described in detail in the table in Figure 5.

LEGEND

- Water
- Park/Greenspace
- +— Railroad
- High-Comfort Bikeway & Trails

Programmed Projects

- ■ ■ ■

Project	Agency	Description
Intersection Projects		
A White Oak Dr & Greenleaf St	MHRA	Improved intersection for more visible and safer crossing between the neighborhood and the White Oak Bayou Trail for people walking and biking
B Waverly St & MKT Trail	MHRA, City of Houston Council District C	Crossing improvements at the Waverly Street and MKT Trail intersection
C Houston Ave & White Oak Dr	MHRA	Rehabilitation of damaged wall and sidewalk and installation of bollards to protect sidewalk and park
D E TC Jester Blvd & Bevis St	TIRZ 12	Crossing improvements of E TC Jester Blvd including curb ramps, pedestrian crossings, & other improvements
E 11th St & Ella Blvd	TIRZ 12	Traffic signal and intersection redesign to include mast arms, curb ramps, markings, & other improvements
F TC Jester Blvd & Stonewood	City of Houston Council District C	Improved crossing of TC Jester Boulevard south of White Oak Bayou near the Stonewood Apartments
G Washington Ave & Sawyer St	City of Houston	Signal and crossing improvements at the intersection of Washington Avenue and Sawyer Street
Park & Drainage Projects		
H Little Thicket Park Improvements	MHRA	Improved park amenities and erosion control/bank stabilization
I Stude Park Improvements	MHRA & City of Houston	Improvement of park facilities, including new playground amenities
J North Canal Project	MHRA, City of Houston, TxDOT, and HCFCD	Expansion of White Oak Bayou beneath the Yale Street and Heights Boulevard bridges, and beneath I-10 west of Studemont Street; part of North Canal Project
Other Projects		
K Westcott Greenspace Improvements	MHRA	Improvements to the greenspace of the Westcott Street roundabout

Figure 5. Programmed Projects in the Study Area (Source: MHRA, City of Houston, Harris County, TxDOT, TIRZ 12, TIRZ 13, 2025)

Planned Projects

In addition to the programmed projects described on previous pages and shown in red in Figure 6, MHRA and others have conducted transportation planning efforts to identify potential future projects.

These planning efforts, shown in dashed lines in Figure 6 will serve as key inputs into the Greater Heights Strategic Connections Plan.

- Programmed Projects (See Figure 1)**
- TIRZ 12 Mobility Plan (2024)**
- TIRZ 5 Three Study Areas Mobility & Drainage Study (2024)**
- Old Sixth Ward/TIRZ 13 Mobility Plan Phases 1 (2021) and 2 (2024)**

City- and Region-Wide Plans

This planning effort will also take into account other planning documents not shown on this map. These plans, in particular the *Houston Bike Plan* and *Houston Vision Zero Action Plan*, will provide an important starting point for potential recommendations.



Figure 6. Planned Projects in the Study Area (Source: MHRA, 2025)

About the Study Area

A History of Greater Heights

Origin Story on High Ground

The Greater Heights Strategic Connection Study Area encompasses a collection of historic communities central to the early days of Houston’s founding. The area includes the Houston Heights, Woodland Heights, Shady Acres, Cottage Grove, First Ward, Rice Military, Olivewood, and Washington Avenue neighborhoods, and portions of Lazybrook and Timbergrove, among others. Figure 7 shows an 1895 map with major neighborhoods already established, including the “Houston Heights” as its own coherent community to the northwest of Downtown Houston.

This portion of the Heights was planned in 1891 by successful businessman Oscar Martin Carter, who saw an opportunity in the higher land northwest of the City’s core to establish a new community. The successful and bustling city of Houston Heights was a small independent municipality until 1918, when the City of Houston annexed it.

For the next seventy years, the area flourished as an economically diverse population, serviced by an electric street car, several local factories, and the Missouri-Kansas-Texas (MKT) railroad. This early infrastructure has persisted in the success of the community to this day. However, Houston growth further west eventually led to the completion of the Katy Freeway (I-10) and a downturn for the Heights neighborhood in the 1950s-1970s.

Reaching New Heights

Today, the Greater Heights area is seen as rapidly changing and re-urbanizing, regaining its reputation as a popular neighborhood. Successful commercial corridors like Shepherd-Durham and 19th Street, as well as vibrant new developments like Sawyer Yards and the MKT demonstrate the appeal of these neighborhoods and solidify their place as true destinations in the region.

New opportunities for development across the area were created in 1996, with the establishment of the Memorial Heights Redevelopment Authority/ TIRZ 5. This has allowed communities in Greater Heights to take inspiration from broader trends in infrastructure and neighborhood improvements such as rail-to-trail conversion and Bayou trail development, leading to projects such as the MKT Trail, White Oak Bayou Trail, and Shepherd-Durham corridor. These projects connect diverse parts of the Greater Heights neighborhoods to each other and a broader Houston network, and contribute to the reputation of the area as active and welcoming.

Greater Heights neighborhoods continue to be sought-after, changing and adapting as rapidly as Houston itself. The existing infrastructure of multi-mobility options, and the neighborhoods’ reputation as an exciting and urban place to be, are a framework for future progress towards a more connected and livable future.

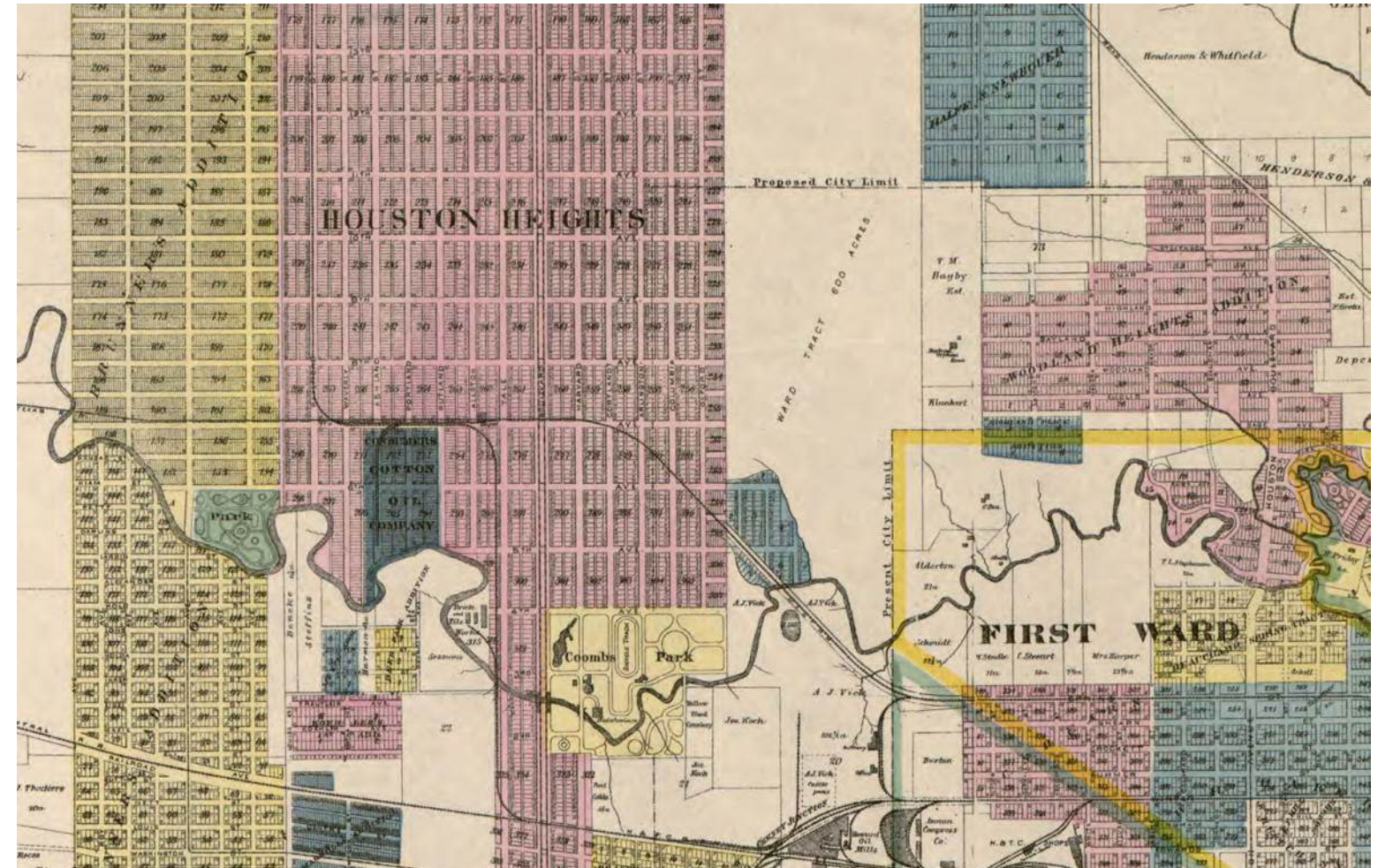


Figure 7. Historic Map of Houston Neighborhoods (Source: City of Houston and Environs, Whitty & Stott, 1895)

Growth, Change, and Mobility Timeline

The Atakapa and a Natural Bayou

The natural land of the area was marshy, and the swamps and bayous traversed by the Indigenous Atakapa people, navigating by foot and on dugout cypress canoes.

Welcome, Houston Heights

The city of Houston Heights was established in 1891 as the first planned community in the area, extending from the White Oak Bayou to above what is now I-45. The community was accessible to Downtown while offering peace and separation from the lower, swampy areas of the big city.

Highway Fever

Freeways including the Katy Freeway (I-10) and the I-610 loop were designed and built in the 1950s, bisecting the Heights above Washington Avenue and furthering a broader goal of the era to increase automobile connectivity in the region.

Modern Mobility

The MKT Trail and the Shepherd Durham Bikeway were among the major mobility projects that answered the community's desire for expanded walking, biking, and multimodal access in the Greater Heights.

City of Houston

The City of Houston was established in 1836, on marshland at the edge of the bayou and with a street grid of 80'-wide blocks, suited for travel by carriage, horse, or foot.

Rail in the Heights

Houston was a railroad city, fueled by the connections made between the port and inland. The MKT railroad was an artery of business and industry in the northwestern corner of the city, running through the Heights. Union Pacific still operates an active rail line adjacent to Cottage Grove.

Streetcar City

Electrified streetcar lines along Studewood Street, Houston Avenue, Washington Avenue, and others connected the First Ward, Old Sixth Ward, Houston Heights, and Woodland Heights neighborhoods to Downtown.

Civic Associations

Through post-war population shifts and exodus to the suburbs fueled by new highways, the inner loop of Houston lost popularity. Civic groups like the Houston Heights Association were established as a way to build community organizing capacity and breathe life into local neighborhoods.

Founding of MHRA

MHRA/TIRZ 5 was established in 1996 and began making mobility, drainage, and quality of life investments in the Greater Heights area. Ongoing investments include comprehensive sidewalk repair, trails, pedestrian bridges, and parks: for example, the development of the White Oak Bayou Trail.

Pre-1850s

1836

1891

1865-1988

1918

1950s

1973

1996

2000s-2020s



Study Area

The Study Area includes an area principally made up of the Greater Heights and surrounding neighborhoods including Woodland Heights and Shady Acres, as shown in Figure 8. To the west, parts of Lazybrook and Timbergrove are included all the way down to Cottage Grove. Just south of I-10, parts of Washington Avenue, Rice Military, West End, Olivewood, and First Ward are included in the Study Area. North Main Street and 20th Street make up the eastern boundary of the Study Area near neighborhoods like Norhill and Sunset Heights.

This area includes many important active transportation links that are cherished destinations by community members. The White Oak Bayou Trail is the principal active transportation artery, connecting to dedicated paths like the MKT Trail, the 11th Street Bikeway, and the Nicholson Trail. Importantly, this area also includes the new and future Shepherd-Durham Bikeways that will strengthen the network and connections to an important commercial corridor for the community.

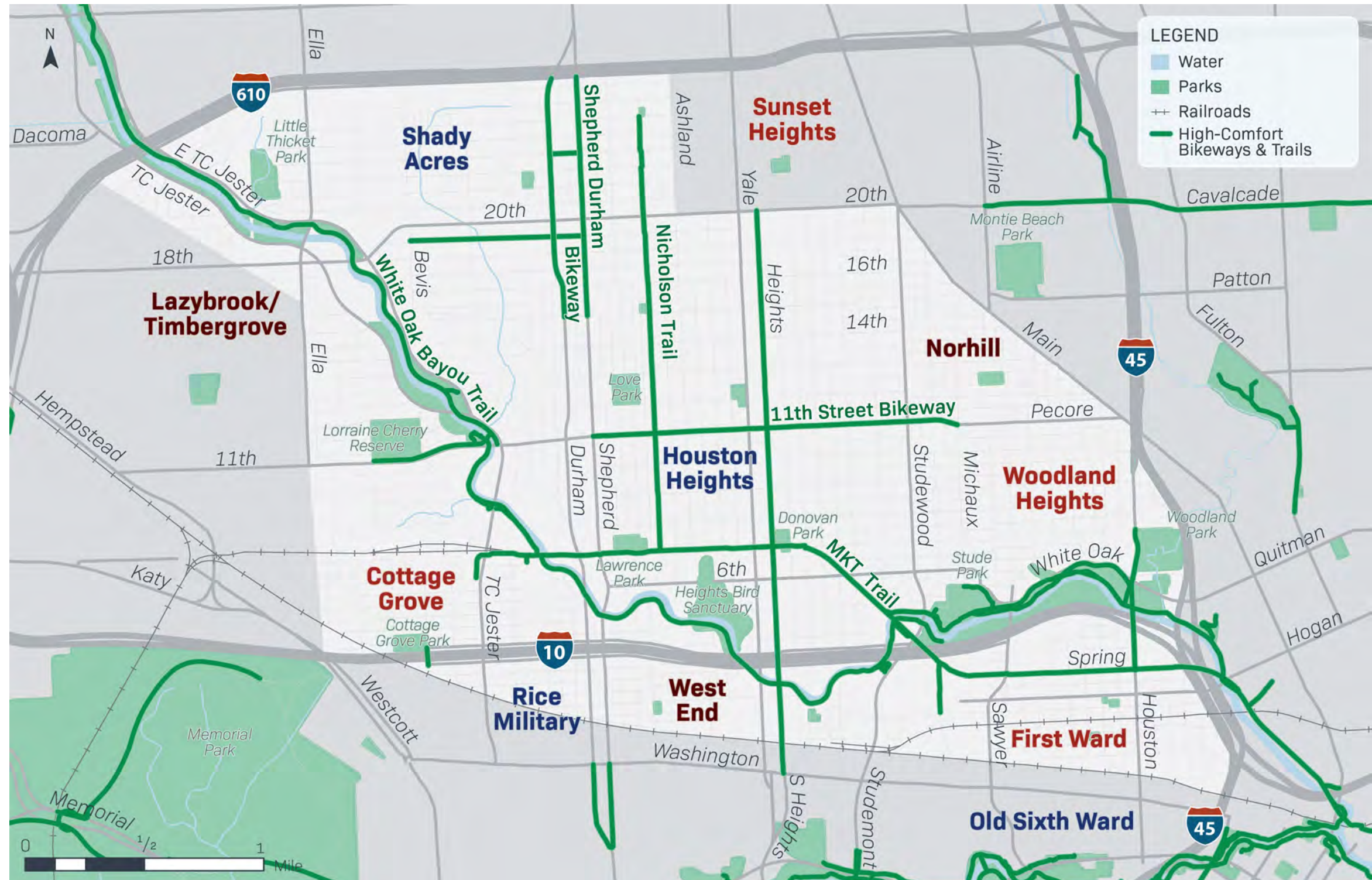


Figure 8. Greater Heights Strategic Connections Plan Study Area Boundary (Source: Team Analysis, 2025)

TIRZ Boundaries

The Memorial Heights Redevelopment Authority, also known as Tax Increment Reinvestment Zone 5 (TIRZ 5), is a jurisdiction created by the City of Houston. TIRZs invest in infrastructure projects with the goal to attract additional investment and economic development within and near its boundaries.

The revenue generated within a TIRZ comes from existing tax revenue. The City sets the property tax base year at the same time the TIRZ is created, and then any increases of that initial revenue each year is called “increment.” The TIRZ Board identifies infrastructure projects via a Capital Improvements Program that is approved by City Council and then implemented by the TIRZ.

MHRA has the ability to leverage debt and utilize federal grant funding to deliver projects. MHRA has experience as a direct recipient of U.S. Department of Transportation funding and is a Federal Transit Administration grantee. To date, MHRA has leveraged over \$85 million in federal funding.

As shown in Figure 9, the Study Area boundaries and the MHRA boundaries are not identical. In fact, parts of the Study Area are within both TIRZ 12 and TIRZ 13. MHRA worked closely with the City to ensure that the Study Area could comprehensively address mobility concerns across neighborhoods and along corridors, even sections outside of the MHRA boundaries.

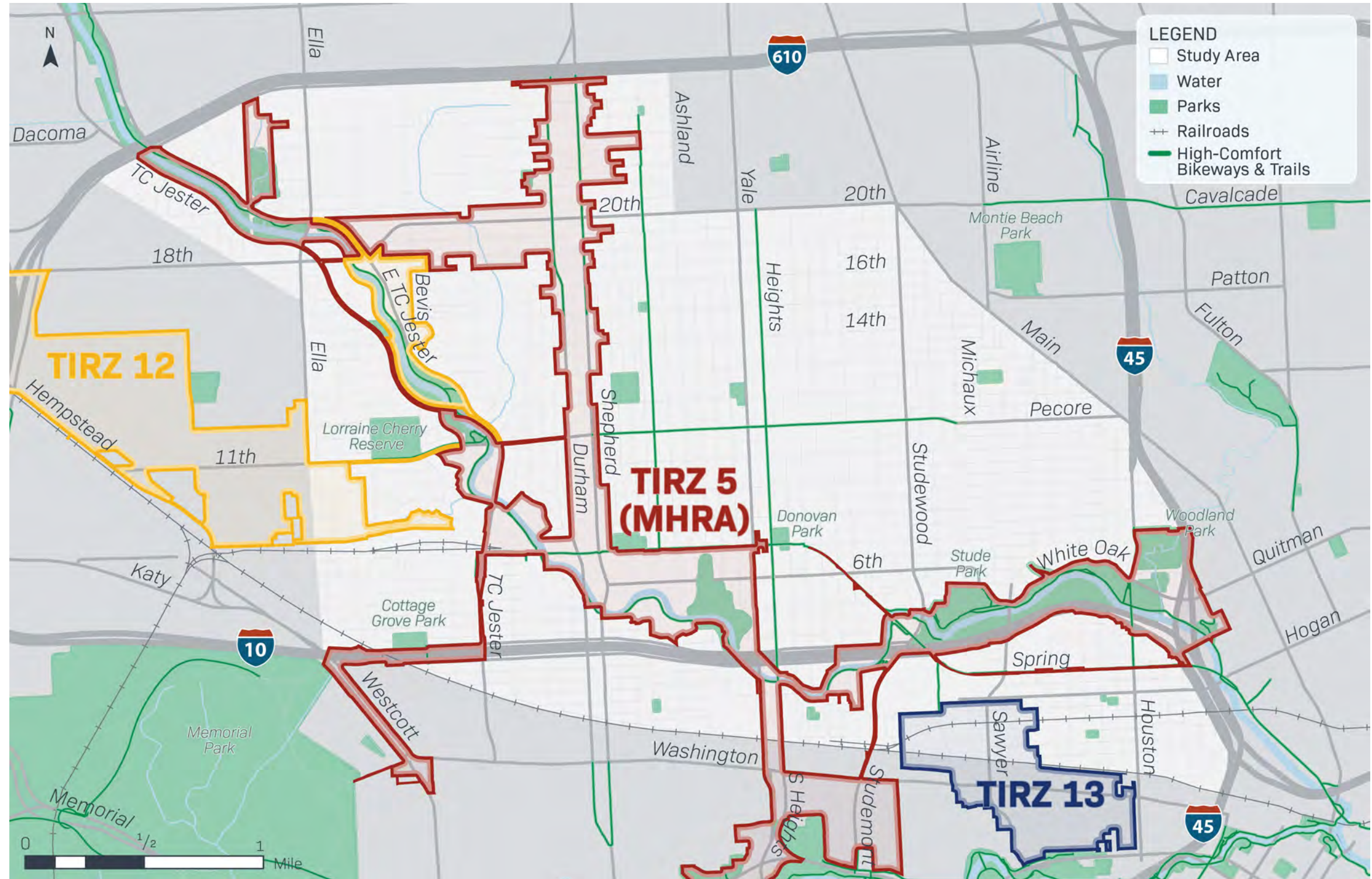


Figure 9. Greater Heights Strategic Connections Plan Study Area and TIRZ Boundaries (Source: City of Houston, 2025)

Houston City Council Districts

Figure 10 displays the two Houston City Council Districts that share boundaries with parts of the Study Area - District C and District H. District C is represented by Abbie Kamin and District H is represented by Mario Castillo Jr.

The boundary splitting the two council districts follows Main Street from I-610, and then Studewood Street to Washington Avenue.

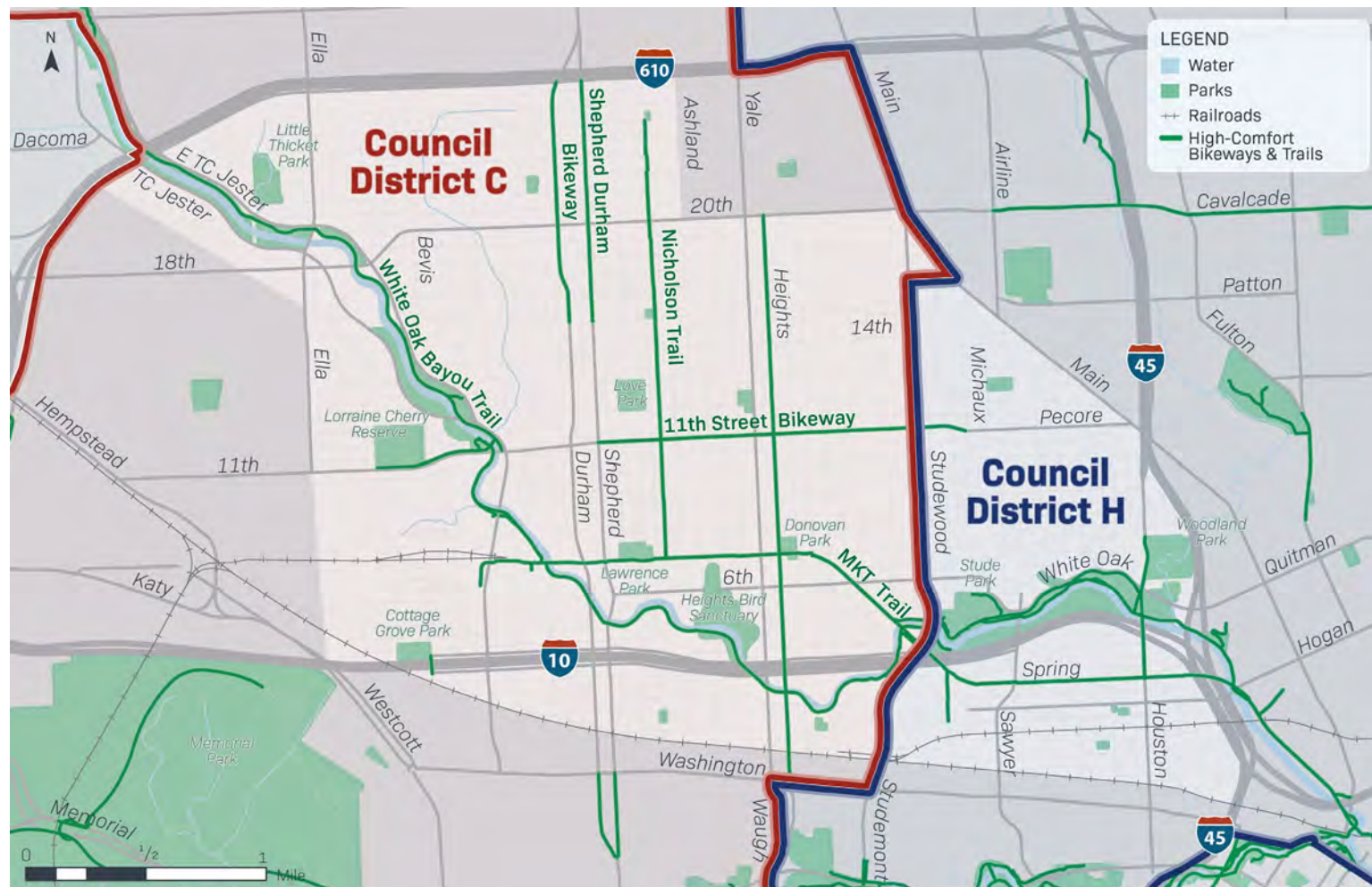


Figure 10. Houston City Council District Boundaries (Source: City of Houston, 2025)

Harris County Precincts

Figure 11 displays the three Harris County Commissioner Precincts with jurisdiction in the Study Area - Precinct 1 (Commissioner Rodney Ellis), Precinct 2 (Commissioner Adrian Garcia), and Precinct 4 (Commissioner Leslie Briones).

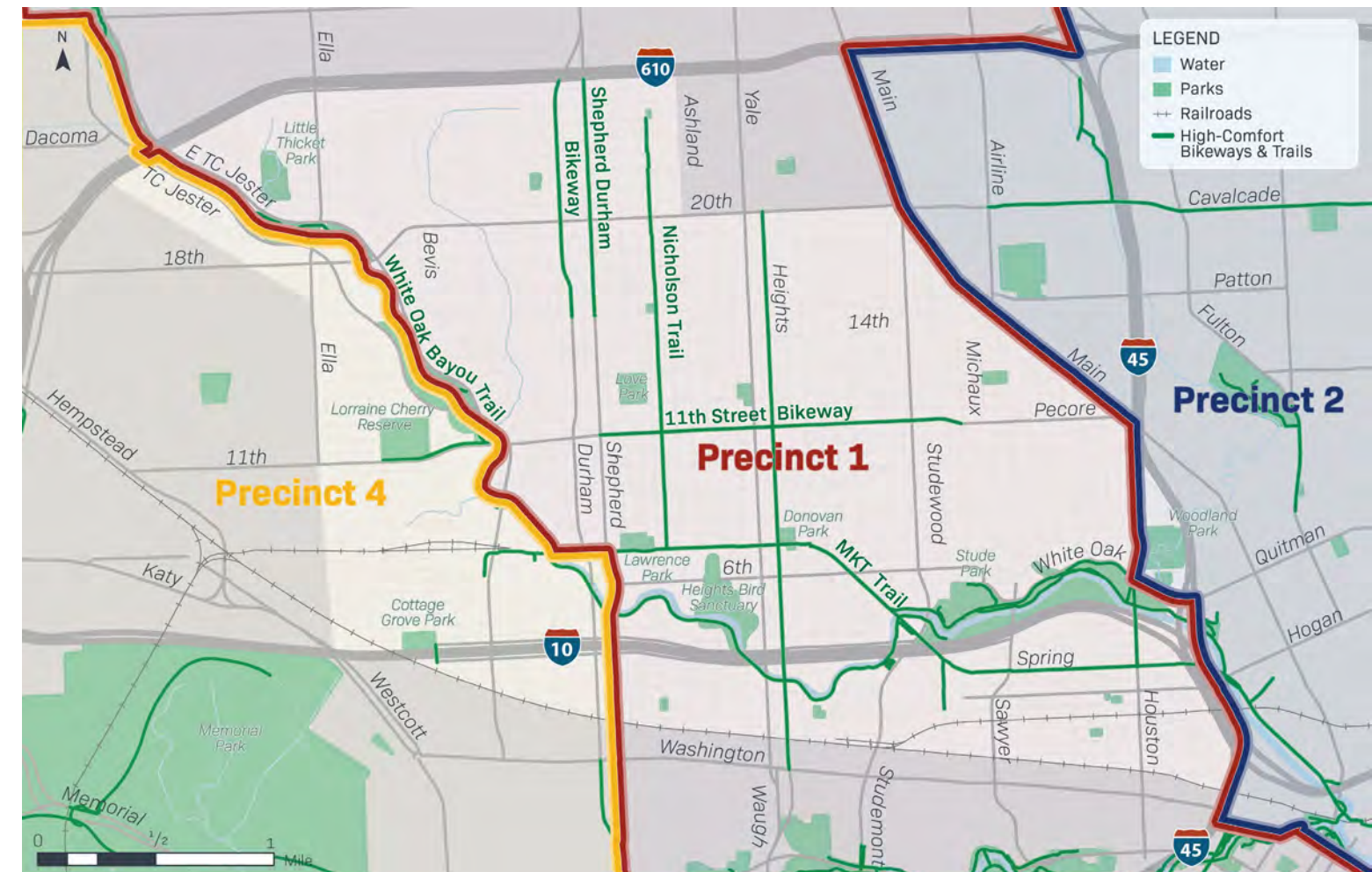


Figure 11. Harris County Precinct Boundaries (Source: Harris County, 2025)

Major Destinations

Figure 12 displays key destinations in and around the Study Area, including some schools, parks, hospitals, community centers, libraries, and commercial corridors.



Schools

More than twelve public schools are located in or near the Study Area, serving the many families that live in these neighborhoods. The Study Area is also home to many private schools, not mapped here.

Parks

The Study Area is dotted with several neighborhood parks like Love Park, Woodland Park, and Cottage Grove Park, as well as regional parks like Stude Park the White Oak Bayou Greenway.

Major Commercial Corridors

Businesses in the Study Area attract visitors from across the city and region to major commercial corridors like Shepherd Drive and Durham Drive, Main Street, 20th Street, Studemont Street, Heights Boulevard and Yale Street, as well as developments like the MKT and Sawyer Yards.

Community Centers Several other community resources are located in the Study Area, including the Heights Hospital.

Libraries

Hospitals

Figure 12. Destinations & Commercial Corridors (Source: Harris County Appraisal District Land Use, 2024; Team Analysis, 2025).

Who Lives in the Study Area?

Population Density

Greater Heights is home to more residents than the City of Galveston and tens of thousands of households as shown in the tables in Figure 13. Figure 14 maps population density in the Study Area. Pockets of density are spread throughout the Study Area, including south of I-10 and west of Heights Boulevard, south of Pecore Street, in the heart of Cottage Grove, and west of Durham Drive, north of 20th Street.

	Study Area	Inner Loop
Total Number of Households	34,000	236,800
Average Household Size	2.01	2.17

	Study Area	Inner Loop
Total & Vulnerable Populations		
Total Population	68,500	513,700
Population Under 18 Years Old	11,500 (17%)	85,600 (17%)
Population 65 Years and Older	6,100 (9%)	63,900 (12%)
Population 75 Years and Older	2,000 (3%)	17,230 (3%)

Figure 13. Population Density (Source: US Census Bureau ACS 5-Year Estimates, 2019-2023).

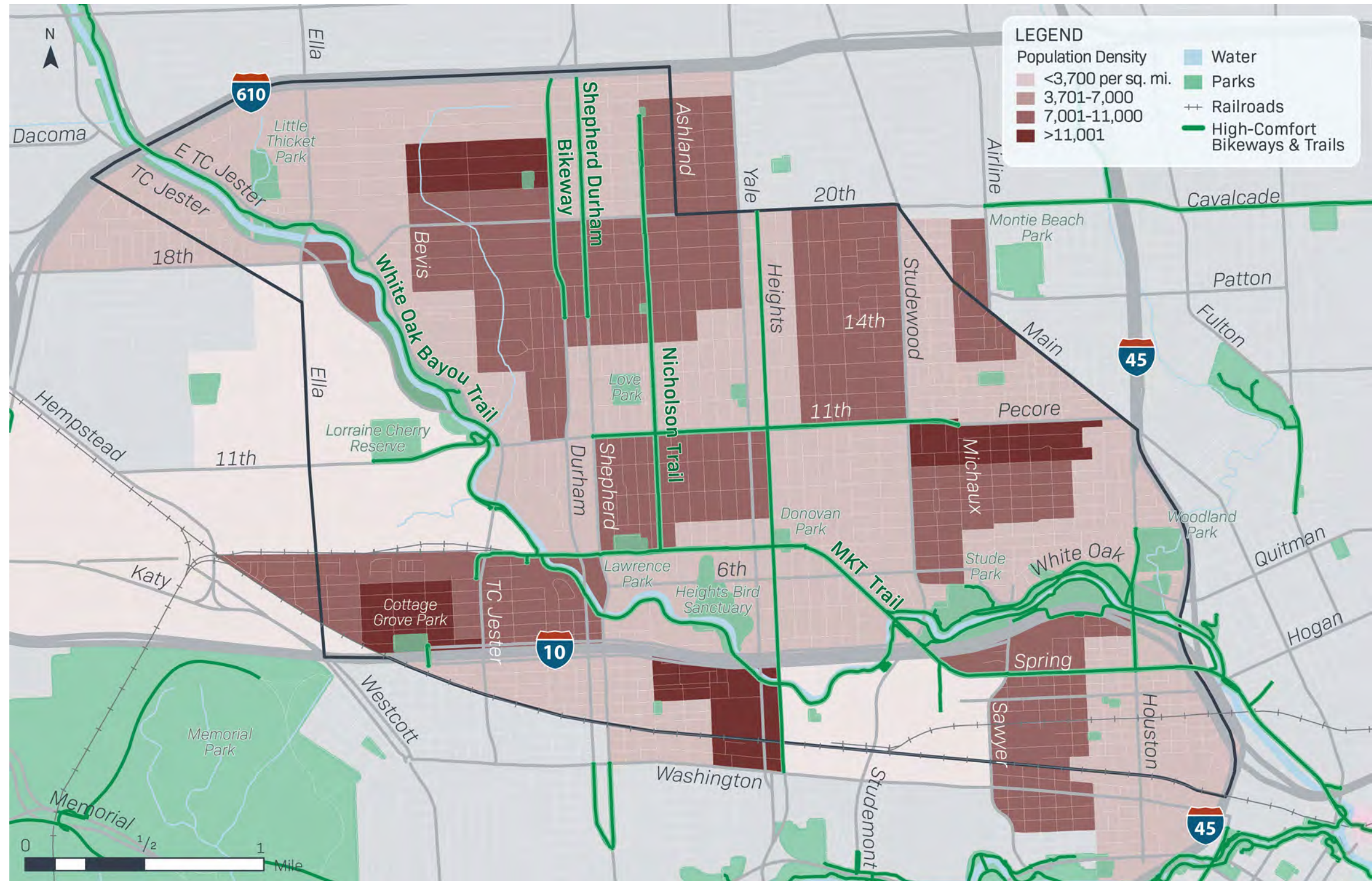


Figure 14. Population Density (Source: US Census Bureau ACS 5-Year Estimates, 2019-2023).

Youth Population Density (under 18 years)

Around 11,500 of the residents in the Study Area are children. Figure 15 shows the population density of youth residents (under the age of 18 years) in the Study Area. Locations with the highest density of youth residents include Cottage Grove west, the Census Block Groups south of Pecore Street near Studewood Street, and near N Main Street closer to the intersection with Airline Drive.

Schools like Hogg Middle, Travis Elementary, and Browning Elementary are located in areas with a high youth population density. Although Cottage Grove has a high youth density, its students are zoned to Memorial Elementary south of I-10 with limited crossings.

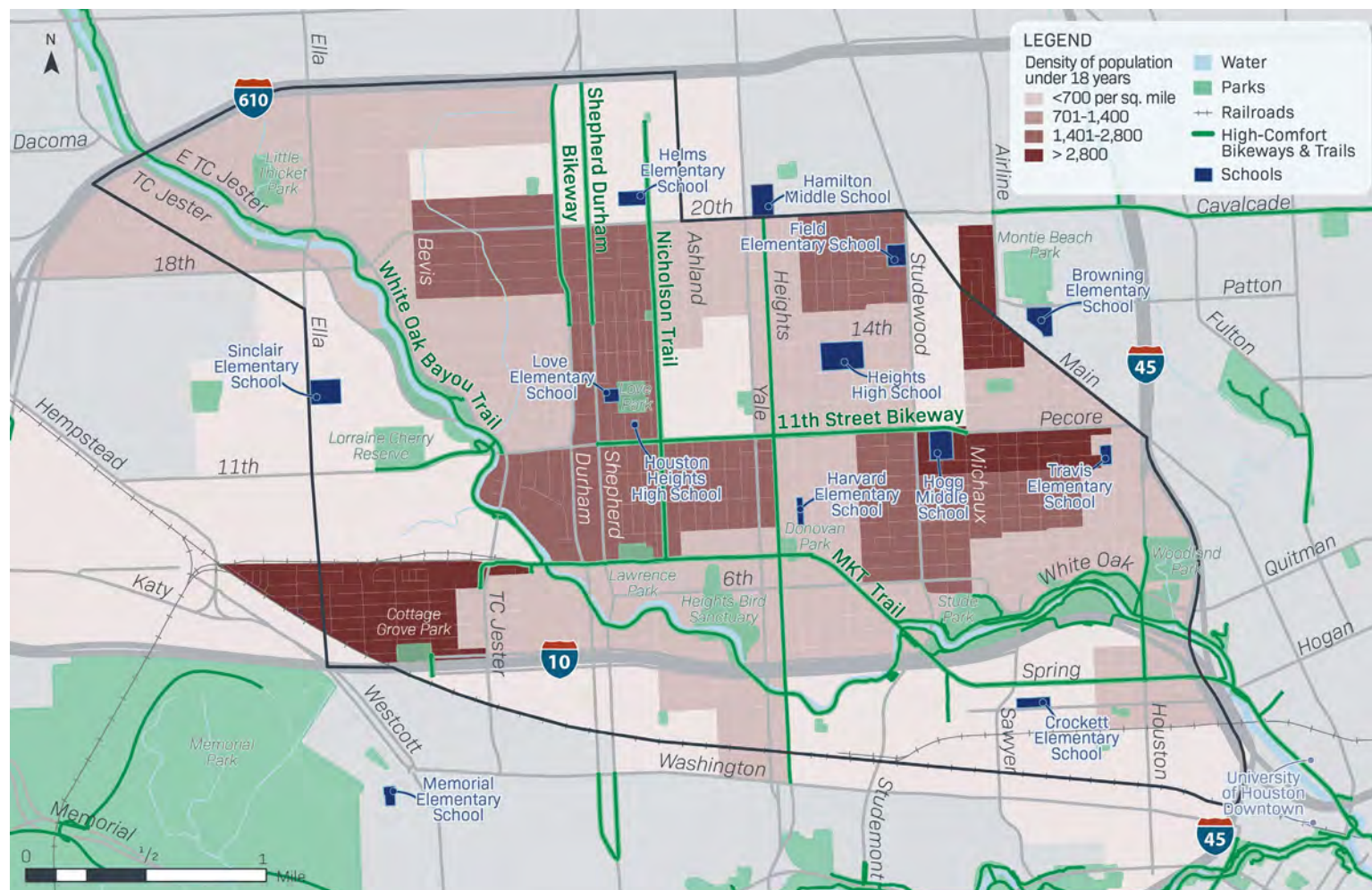


Figure 15. Population Density of Study Area Residents Under the Age of 18 (Source: US Census Bureau ACS 5-Year Estimates, 2019-2023).

Older Adult Population Density (65+ years)

Just over 6,100 residents in the Study Area are 65 years old or older, with around one-third of those over the age of 74. Figure 16 shows the population density of residents 65 years or older. The largest concentration of older residents are east of the Nicholson Trail south of 20th Street and south of Pecore Street near Michaux Street.

There is also a relatively high density in Lazybrook/Timbergrove area and north of 11th Street. Although many adults over the age of 65 work and live independently, safe local streets are critical for ensuring that these residents are able to age in the neighborhoods where they have lived for years.

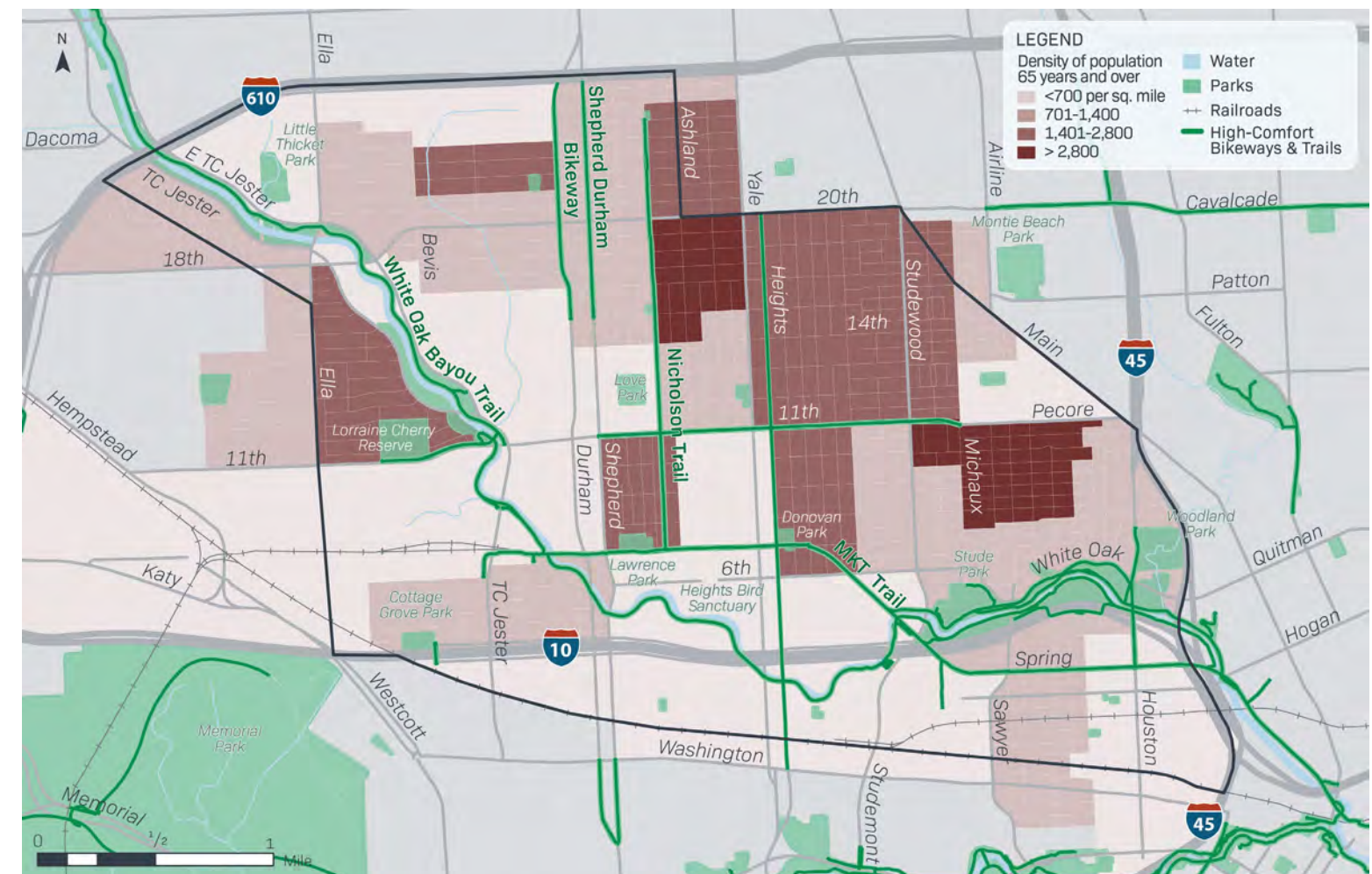


Figure 16. Population Density of Study Area Residents Over the Age of 64 (Source: US Census Bureau ACS 5-Year Estimates, 2019-2023).

Today's Transportation Network

Major Roadways & Signals

Figure 17 shows roadways in the Greater Heights on the City of Houston's Major Thoroughfare and Freeway Plan (MTFP), updated in 2025 as well as signalized intersections.

In most cases, these roadways carry more vehicle volumes than local streets and often serve as locations where many mobility uses and land uses converge with transit lines, bikeways, freight traffic, commercial and office uses, and more.

Definitions

Major Thoroughfare: roadways designed for more traffic, typically with higher posted speed limits spaced out in a one-mile grid pattern.

Major Collector: roadways that gather traffic from smaller roads like local streets or Minor Collectors and distribute them to Major Thoroughfares.

Minor Collector: roadways that gather traffic from local streets and distributes it to Major Collectors.

Transit Corridor: Roadways with an existing or proposed METRO fixed guideway such as light rail or bus rapid transit.

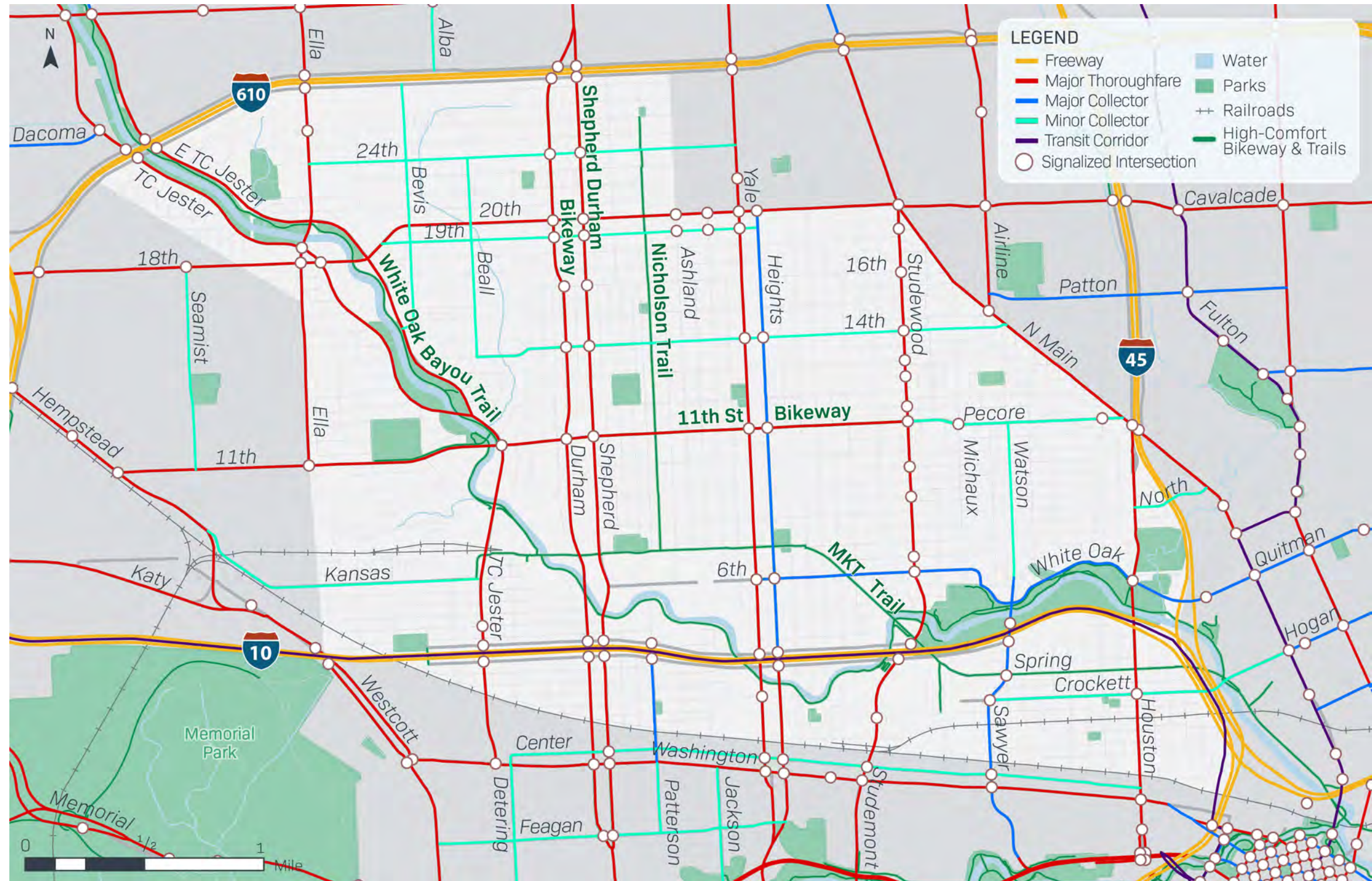


Figure 17. MTFP Classification & Signalized Intersections (Source: City of Houston, 2023 & 2025).

Trip Purpose: How Are People Getting Around?

Commuting to Work

Figure 18 shows how workers in the Greater Heights and the Inner Loop are getting to work, split out by transportation mode. As in the rest of the Inner Loop, most workers in the Study Area are driving alone. While a greater portion of people in Greater Heights are working from home, a smaller percentage are carpooling, riding transit, biking, or walking compared to the rest of the Inner Loop.

Non-Commuting Trips

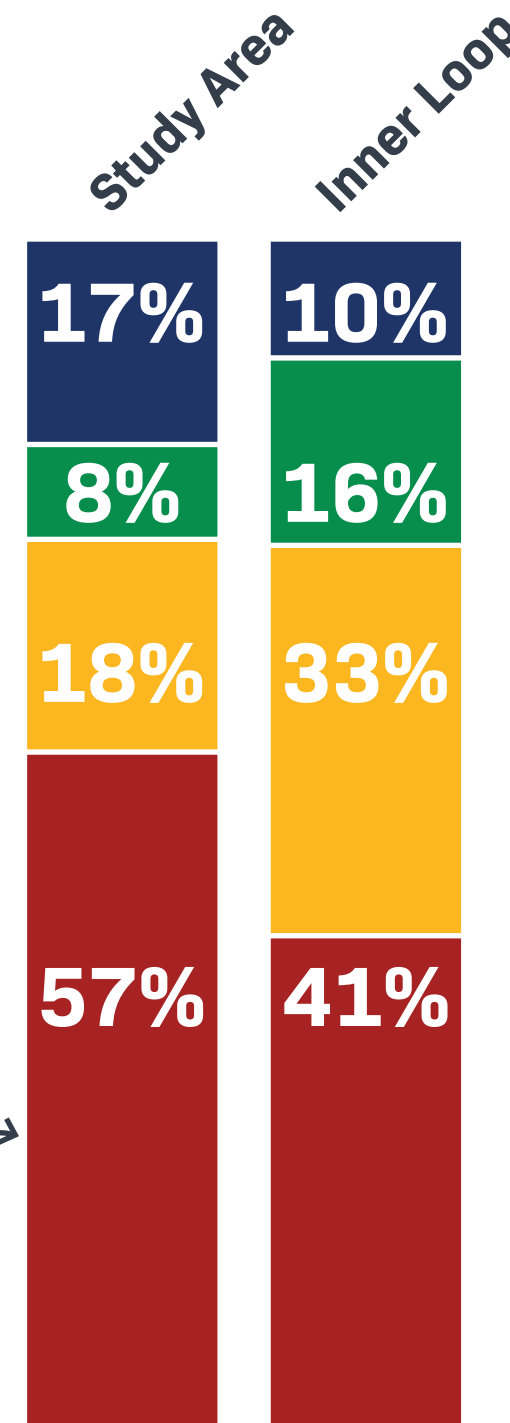
However, the vast majority of trips are not for a daily work commute. Data from the H-GAC Regional Travel Model can show all the types of trips people make and how far they go between different origin and destination zones. Figure 19 categorizes average weekday auto and transit trips into four trip types. In the Study Area, only 17 percent of all trips are commutes to and from work. Another 26 percent of trips are work related, such as driving to a work site or taking the bus to dinner after work.

The majority of trips in the Study Area are trips taken from home to places that are not work. This includes running errands, visiting friends, or going out to eat with family. This number is much higher for the Study Area than for the Inner Loop as a whole, representing a real opportunity to shift non-work-based driving trips to other modes like walking and biking.

Trips from home to other places, like a friend's house, are the most common trip type in the Study Area and represent an opportunity for folks to walk, bike, and ride transit instead of drive.

Commute Mode to Work		
Mode	Study Area	Inner Loop
Drive Alone	68%	67%
Carpool	5%	6.5%
Public Transit	0.5%	3%
Bike	0.5%	1%
Walk	1%	3%
Work from Home/Other	24%	19%

Figure 18. Commute mode to work in the Study Area and Inner Loop (Source: Census ACS 2019-2023 5-year Estimates)



Trip Purpose for Average Weekdays

Work Commute

Daily commute to and from work (Home-based work trip)

Work ↔ Work Trips

Ex: Meeting a client at their office (Non-home based work trip)

Work ↔ Other Places

Ex: Picking up groceries after work (Non-home based non-work trip)

Home ↔ Other Places

Ex: Running errands or going out to eat (Home-based non-work trip)

Figure 19. Trip purpose for auto and transit trips in the Study Area and Inner Loop (Source: H-GAC, 2023)

Trip Distance: How Are People Getting Around?

The length of a trip directly impacts the travel mode a person chooses. If the destination is far away, people tend to drive or take transit. For trips nearby, walking and biking are more viable options, especially if streets are safe and comfortable. Figure 20 shows trip distances in miles for all vehicle and transit trips on a typical weekday in the Study Area (left) and Inner Loop (right).

In both the Study Area and the Inner Loop, around one-third of all vehicle/transit trips are less than three miles, indicating a latent demand for biking trips. Additionally, five percent of all trips are less than one mile, an ideal distance for walking.

One mile is used as a standard for short walking trips based on the City of Houston Infrastructure Design Manual which uses three feet per second as the design basis for pedestrian crossing times. At that speed, a one mile walking trip would take just under 30 minutes.

Three miles is used as a standard for short trips for biking based on 2011 Federal Transit Administration guidance* establishing three miles as a reasonable distance for a bicyclist to travel to access a transit stop or station.

**Final Policy Statement on the Eligibility of Pedestrian and Bicycle Improvements Under Federal Transit Law*

Figure 21 shows data from Replica* estimating the transportation mode for all weekday trips in the Study Area under one mile in spring 2024. 41 percent of these short trips are taken on foot or by bike, revealing how many people are already taking advantage of the local sidewalk, trail, and bikeway network in the Greater Heights.

**Note: Replica is a privately owned data platform that compiles existing "big data" sources such as phone-based location tracking to estimate where, when, and how people are traveling. As with any big data, the resulting estimates should be viewed with caution as they do not represent a total, verified count of travelers.*

Trip Distance for Average Weekday

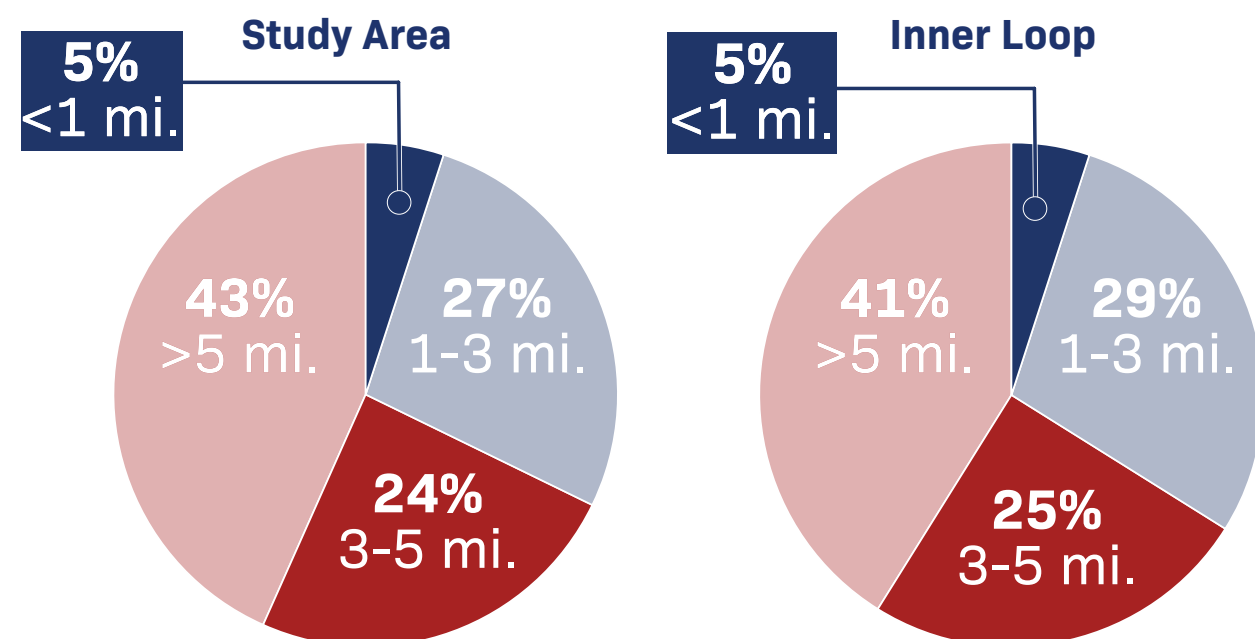


Figure 20. Trip distance for the Study Area (left) and Inner Loop (right) (Source: H-GAC, 2023)

Short Trip (<1 mi.) Travel Mode for Average Weekday

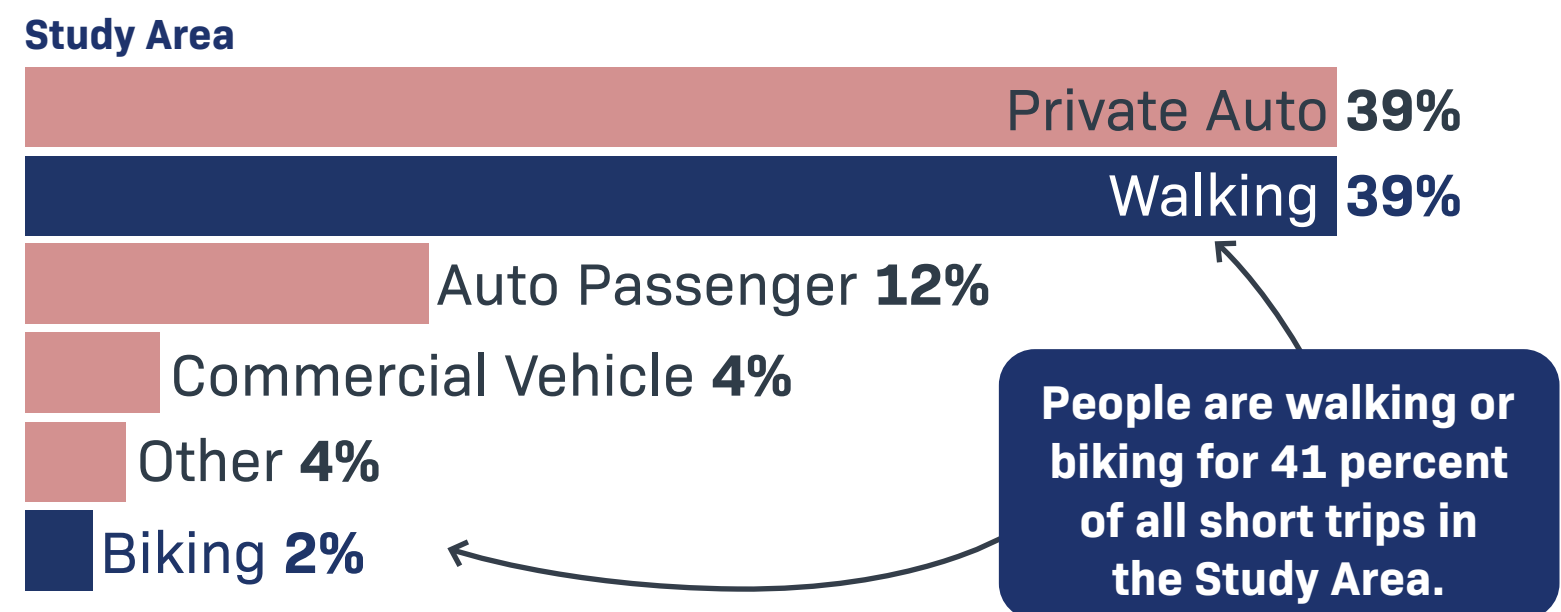


Figure 21. Travel mode for all trips in the Study Area less than 1 mile in distance (Source: ReplicaHQ, Spring 2024)

Vehicle Volumes

Vehicle counts for major streets indicate where more volumes are moving through the Study Area. Figure 22 shows average daily traffic (ADT) based on counts by the City of Houston and TxDOT. Studemont Street carries the highest number of vehicles, along with the Shepherd and Durham Drive corridor. Other major roadways like Studewood Street, Ella Boulevard north of 18th Street, Yale Street, and TC Jester Boulevard all see volumes above 15,000 ADT. Portions of Ella Boulevard, 20th Street, W 11th Street, and Crockett Street see volumes well below 10,000 ADT.

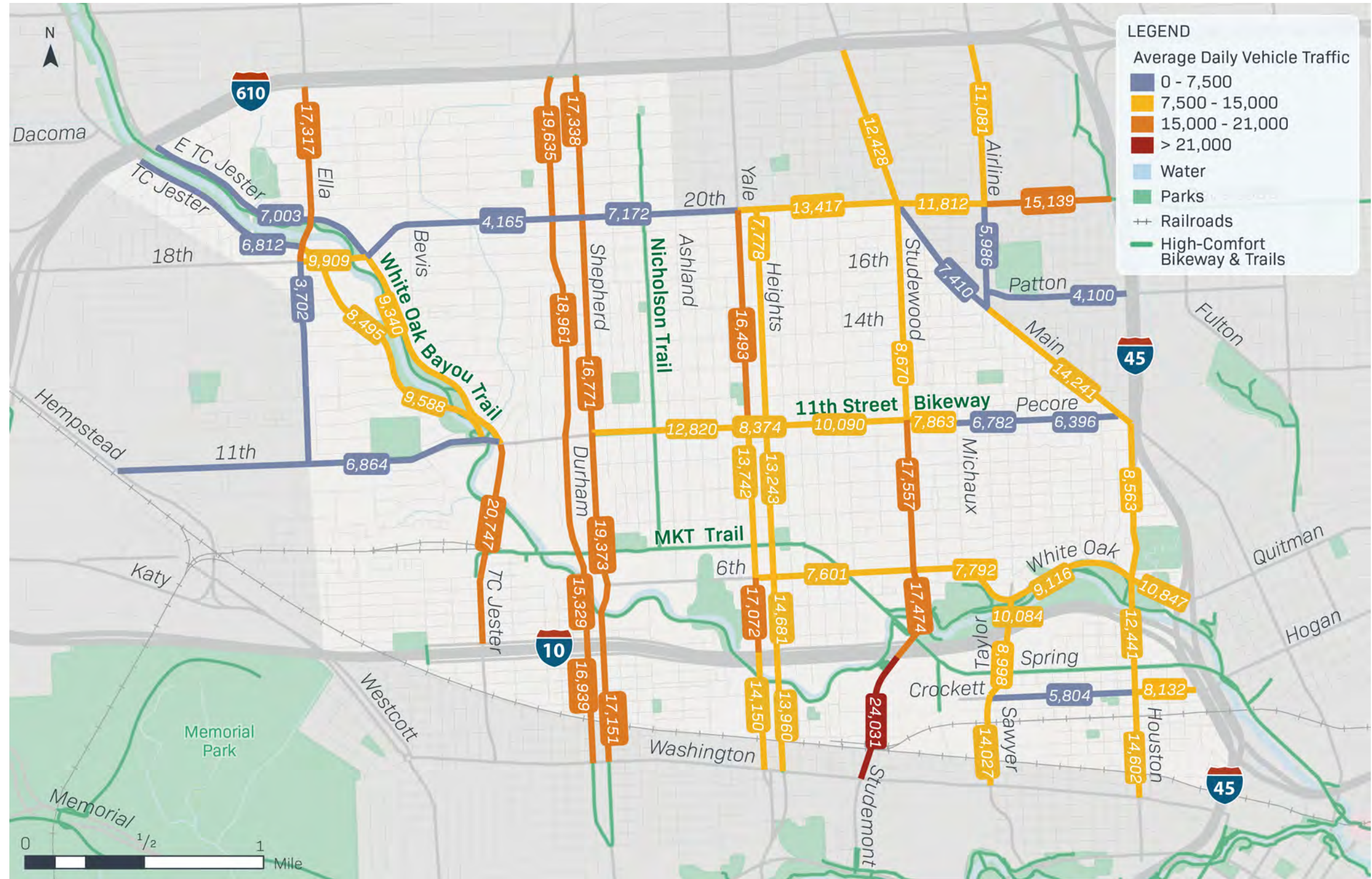
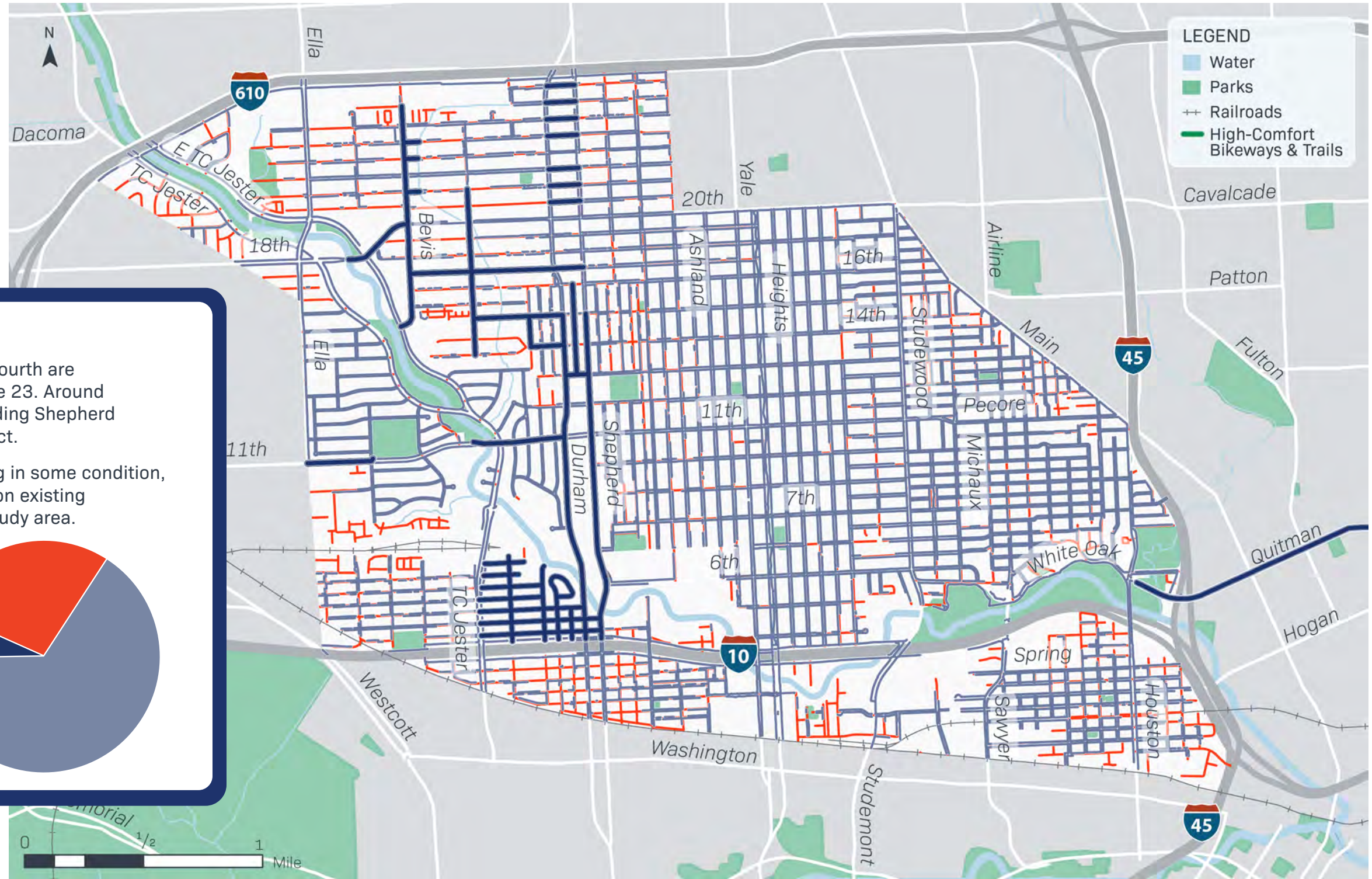


Figure 22. Traffic Volumes (Source: City of Houston, 2019-2025; TxDOT, 2022).

Existing Sidewalks

Figure 23 shows sidewalk coverage in the Study Area as either missing, programmed, or existing. MHRA and other agencies have recently conducted detailed sidewalk assessments in specific block sections to identify areas with needed sidewalk improvements.



Sidewalks in the Study Area

Of all roadways in the Study Area, more than one-fourth are missing sidewalks, as shown in bright red in Figure 23. Around 7 percent are part of a programmed project, including Shepherd Durham Phase 2 and the Cottage Grove East project.

The remaining 66 percent of sidewalks are existing in some condition, ranging from inadequate to fully accessible. Data on existing sidewalk condition is not available for the entire study area.

- 27% Missing
- 7% Programmed
- 66% Existing

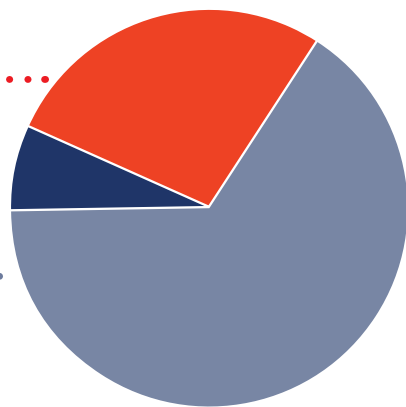


Figure 23. Existing Sidewalk Coverage & Conditions (Source: H-GAC 2024)

Existing Bikeways

Current Connections

Figure 24 shows existing bikeways within the Study Area. Whether it is a protected bicycle lane, a bayou trail, or a designated shared on-street bikeway, the existing bicycle network provides a great starting point for a future connected network in the Greater Heights.

The current bikeway network provides access to key local and regional destinations such as parks, schools, and commercial destinations. The current reach of the bikeway networks allows for access outside the I-610 Loop in some instances, including all the way to Downtown Houston and beyond.

Today's Gaps

The bikeways and trails in the Study Area are not currently connected into a full network, as envisioned in the *Houston Bike Plan*. Current **gaps include east-west connections north of 11th Street**, especially lacking connections between White Oak Bayou, the Shepherd Durham Bikeway, Nicholson Trail, and Heights Boulevard.

South of 11th Street, the gap is in north-south connections, especially across I-10, the Union Pacific Rail Line, and Washington Avenue. Of the few existing bridges across the bayou, several have substandard facilities for people walking and biking. Studemont Street, for example, does not even have an existing sidewalk.

While White Oak Bayou and the trail along the bayou are a great community resource, the bayou is difficult to cross, with only a few exceptions where MHRA and others have made intentional investments in walking and biking infrastructure.

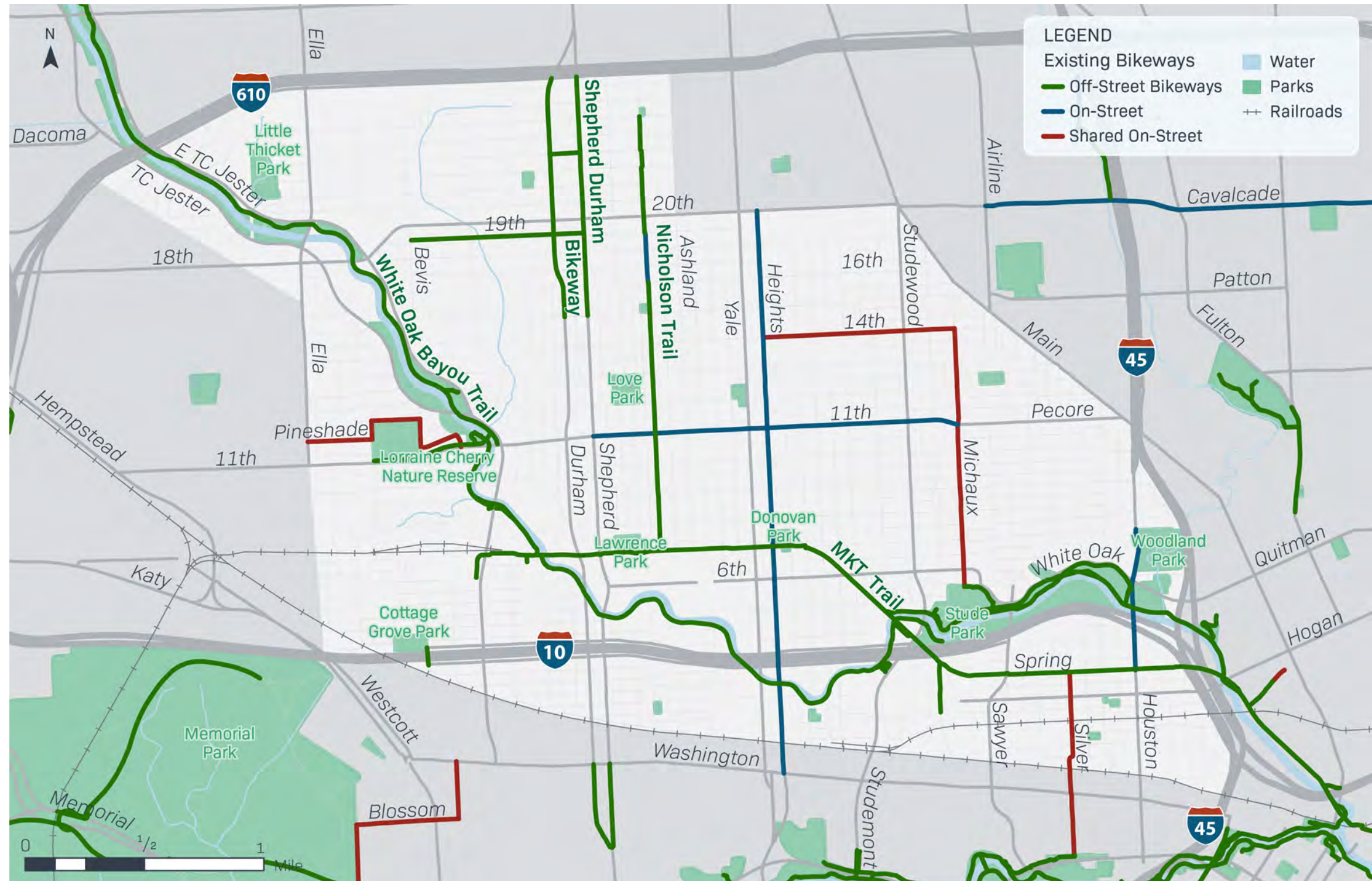


Figure 24. Existing Bikeways (Source: City of Houston, 2024; Team Analysis, 2025)

Programmed Bikeways

Figure 25 displays both existing and programmed bikeways in the Study Area. The term “programmed” means that the MHRA or another agency has allocated funding and a timeline to implement these specific bikeway projects.

These programmed bikeways include MHRA projects like the Phase 2 extension of the Shepherd Durham Project south to I-10. These connections will help build a more completed high-comfort network of bikeways to serve people of all ages and abilities, including students biking to school.

Benefits for All Roadway Users

These bikeway improvements also improve roadway safety and relieve congestion for people who are not biking. For people driving, more people choosing bicycles for short trips means fewer cars on the road. These projects all include safe crossing improvements, updated signage and markings, and encourage responsible vehicle speeds by reducing roadway widths to accommodate the bikeway, making streets more comfortable for people walking.

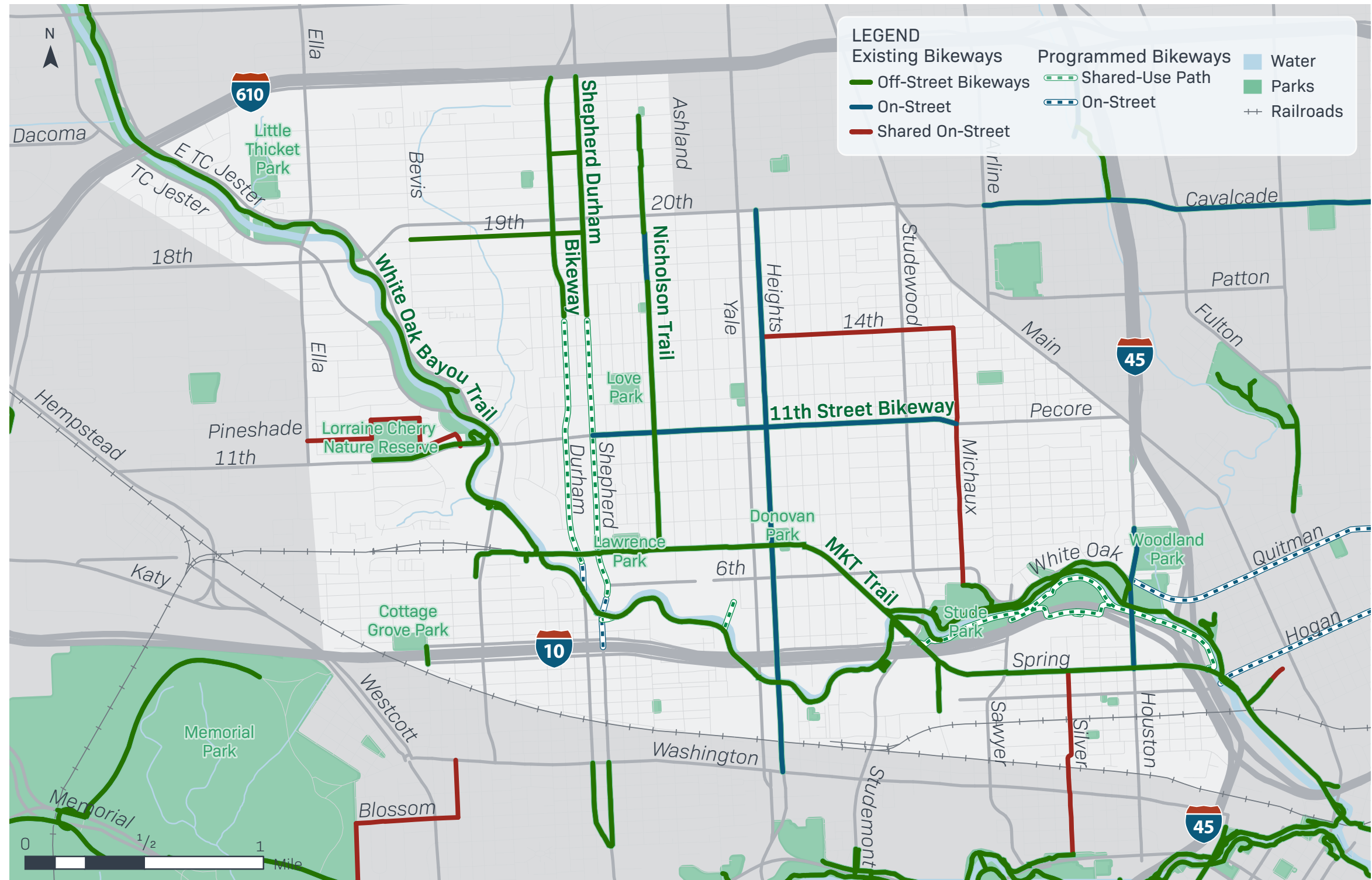


Figure 25. Programmed Bikeways (Source: City of Houston, 2024; Team Analysis, 2025)

Access to Active Streets

Figure 27 shows the streets within an eighth- and quarter-mile from existing bikeways and trails in the Study Area. **These golden areas on the map highlight the areas of the Greater Heights where someone can open their front door and be on a designated trail or bikeway within a block or two.** In other words, the eighth-mile corridors show which neighborhoods have excellent access to an existing “active” street, where walking, biking, and rolling are safer and more comfortable.

Some critical spines of the trail and bikeway network like the Shepherd Durham Bikeway and 11th Street have great connection to the local street grid. However, the White Oak Bayou Greenway is strikingly disconnected from the nearby local streets, underperforming other paths due to limited connections at more regular intervals.

Who Has Access?

Figure 26 shows the number of residents, employees, businesses, and public schools that currently have eighth-mile and quarter-mile access to an active street.

	Distance from Bikeway	
	1/8 mi.	1/4 mi.
Residents	13,700 (20%)	29,000 (42%)
Schools	7	9
Employees	6,300 (18%)	12,200 (35%)
Businesses	990 (25%)	1,900 (48%)

Figure 26. Quarter- and Eighth-Mile Bikesheds (Source: Toole Design, 2025; ESRI Business Summary Report via Data Axle Inc., 2025).

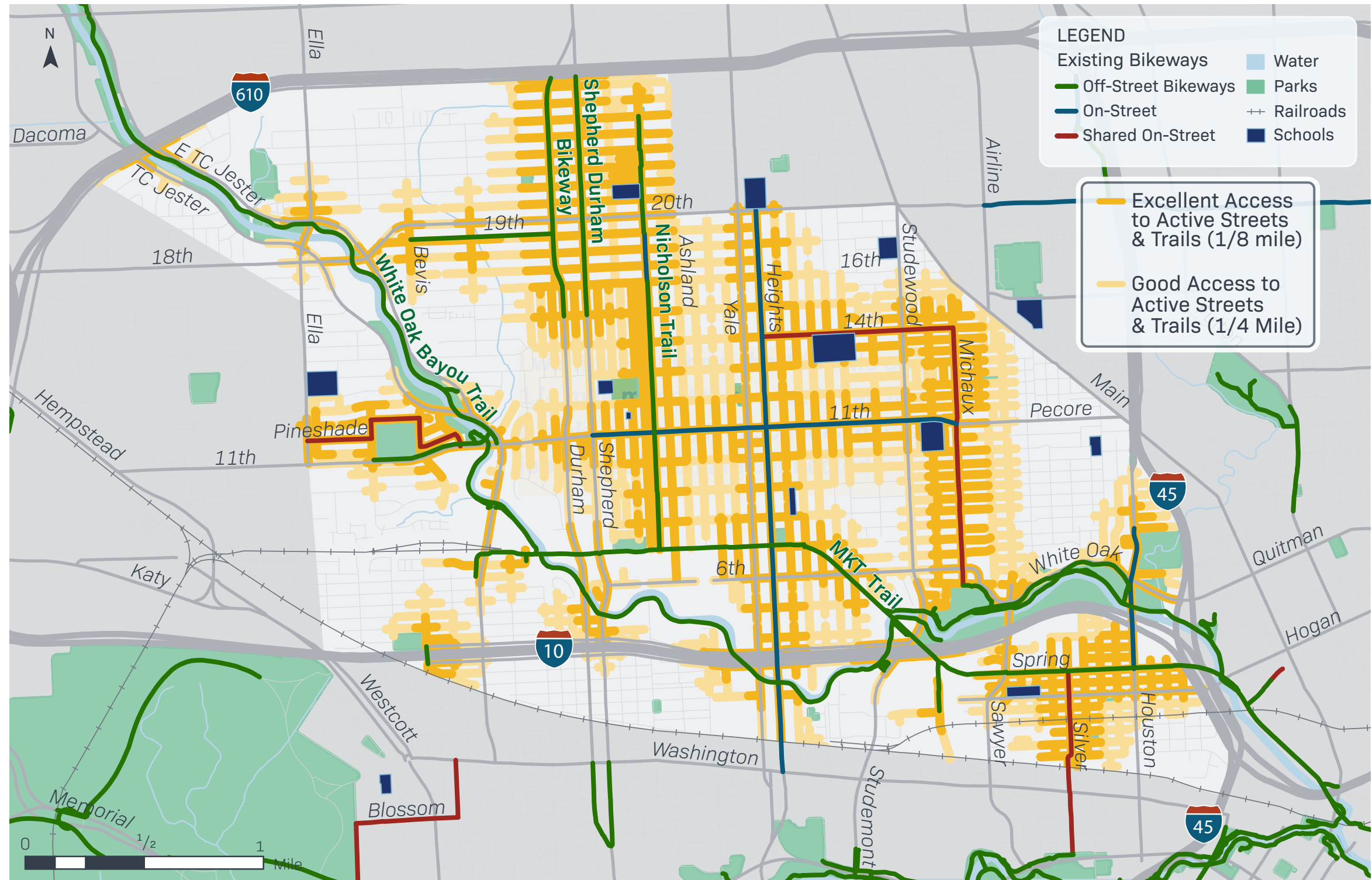


Figure 27. Quarter- and Eighth-Mile Bikesheds (Source: Team Analysis, 2025; Esri Business Summary Report via Data Axle Inc., 2025)

Transit Service

Figure 28 shows the 14 existing METRO local bus routes by frequency, as well as the Heights Community Connector Zone.

Current transit options can help people travel within and to the Study Area. The only two current high-frequency options (every 15 minutes or better) in the Study Area include the 26 Long Point/Cavalcade running east-west, and the 27 Shepherd running north-south.

The Heights Community Connector Zone is one of six in Houston, and was first introduced in April 2025. These zones provide a free, on-call connection to METRO services in places where transit coverage may be limited.

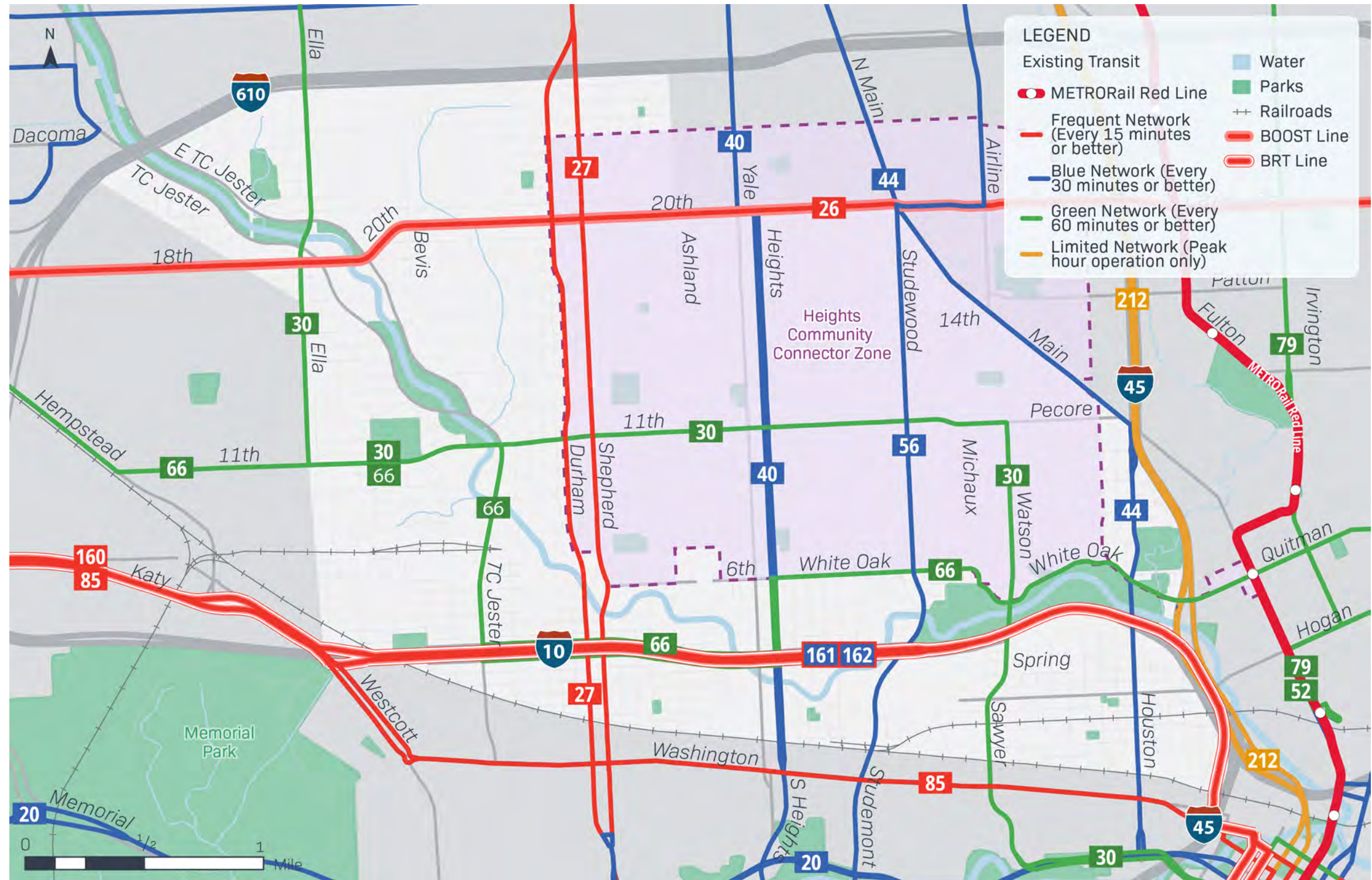


Figure 28. Existing Transit as of June 2025 Service Changes (Source: METRO, 2025)

Transit Ridership

METRO ridership in the Greater Heights area is particularly elevated in areas where high-frequency and well-connected transit lines intersect. Figure 29 demonstrates average weekday boardings and alightings from area bus stops in October 2024. A kernel density estimation was used to create heat maps of bus rider activity to account for potential transfers between lines. This data pre-dates the completion of Phase I of the Shepherd-Durham project and includes more bus stops than are currently along the corridor.

The Heights Boulevard/Yale Street at 20th Street intersection shows the highest concentration of transit ridership, where the 26 Long Point/Cavalcade intersects the 40 Telephone/Heights, providing direct service to Downtown. Lines 44 and 56 go to Downtown and the Texas Medical Center, respectively. These, corresponding with Line 26, see the second highest activity in the area. Line 30 also provides a direct ride to Downtown and attracts more riders. The top 5 ridership intersections are shown in the table below.

Intersection	Avg. Weekday Users	Connecting Lines
20th St @ Heights Blvd/Yale St	421	40 26
N Main St @ Studewood St	239	44 26 56
20th St @ Shepherd Dr/Durham Dr	217	26 27
W 11th St @ Shepherd Dr/Durham Dr	169	30 27
Studemont St @ Summer St (Kroger)	145	56

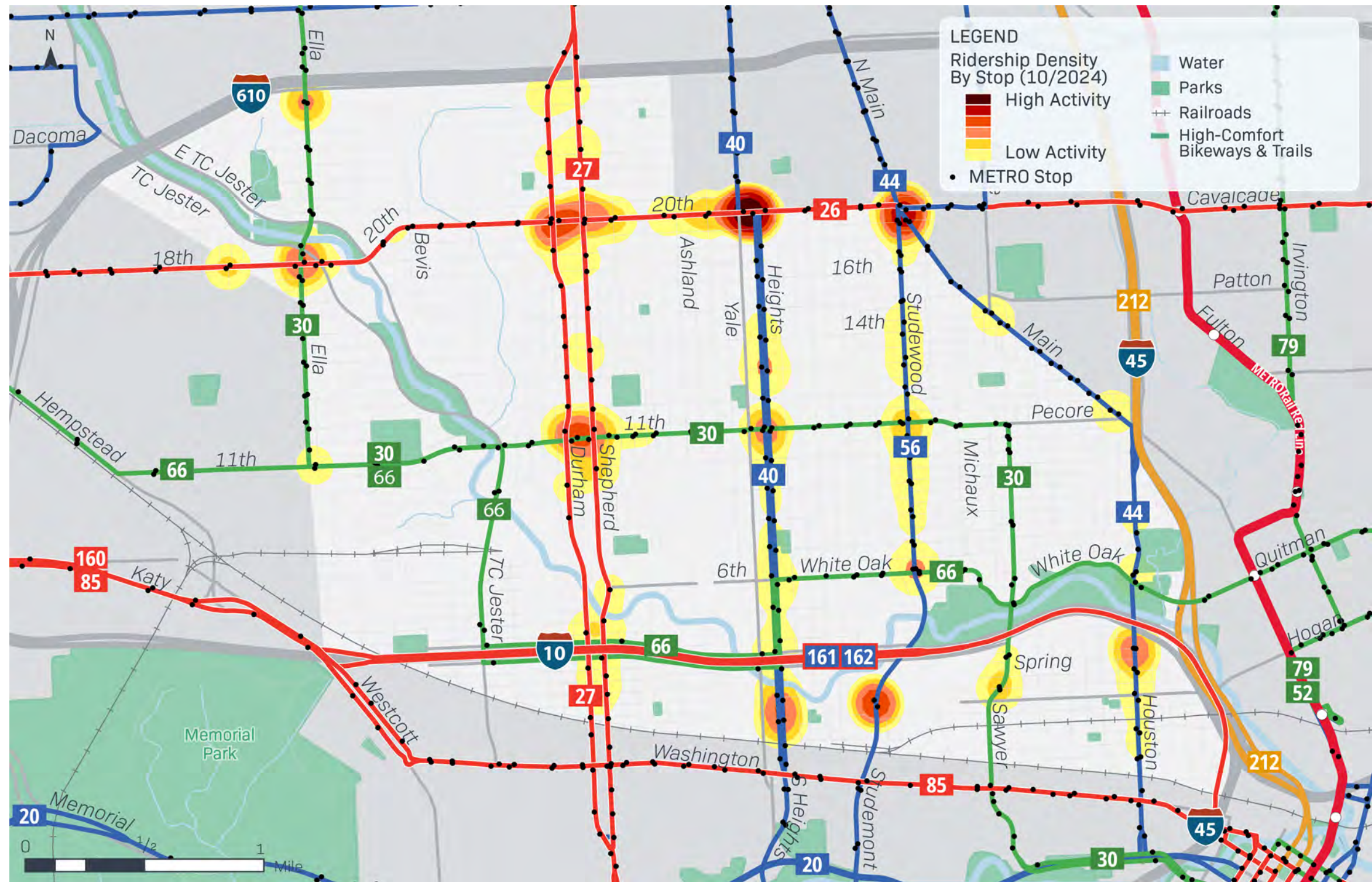


Figure 29. Transit Ridership Density by Stop (Source: METRO, 2024-2025)

Key Safety Indicators

Roadway Speeds

Figure 30 displays lines color coded by recorded 85th percentile speeds on major roadways, with higher speeds in darker color. Each segment is labeled with its 85th percentile speed and its difference with the posted speed limit. The City's default speed limit is 30 mph, with exceptions in the Study Area shown in Figure 31.

The 85th percentile speed shows a pattern of excessive speeding (10 mph over the posted limit) on several roads. These high-speed streets include North Main Street from I-45 to I-610, where the 85th percentile speed reaches 42 mph despite a speed limit of 30 mph. Durham Drive south of W 11th Street is also a problematic zone for speeding, where drivers regularly travel 16 miles per hour over the posted speed limit on a long stretch with no signals until I-10.

Note: This data was collected prior to the completion of Phase 1 of the Shepherd Durham Project.

Posted Speed Limits over 30 mph	
Heights Blvd.	35 mph
T.C. Jester Blvd.	35 mph
E T.C. Jester Blvd. (west of Ella Blvd.)	40 mph
Ella Blvd.	35 mph
W 11th St. (west of Shepherd Dr.)	35 mph
N Shepherd Dr.	35 mph
N Durham Dr. (south of White Oak Bayou)	35 mph

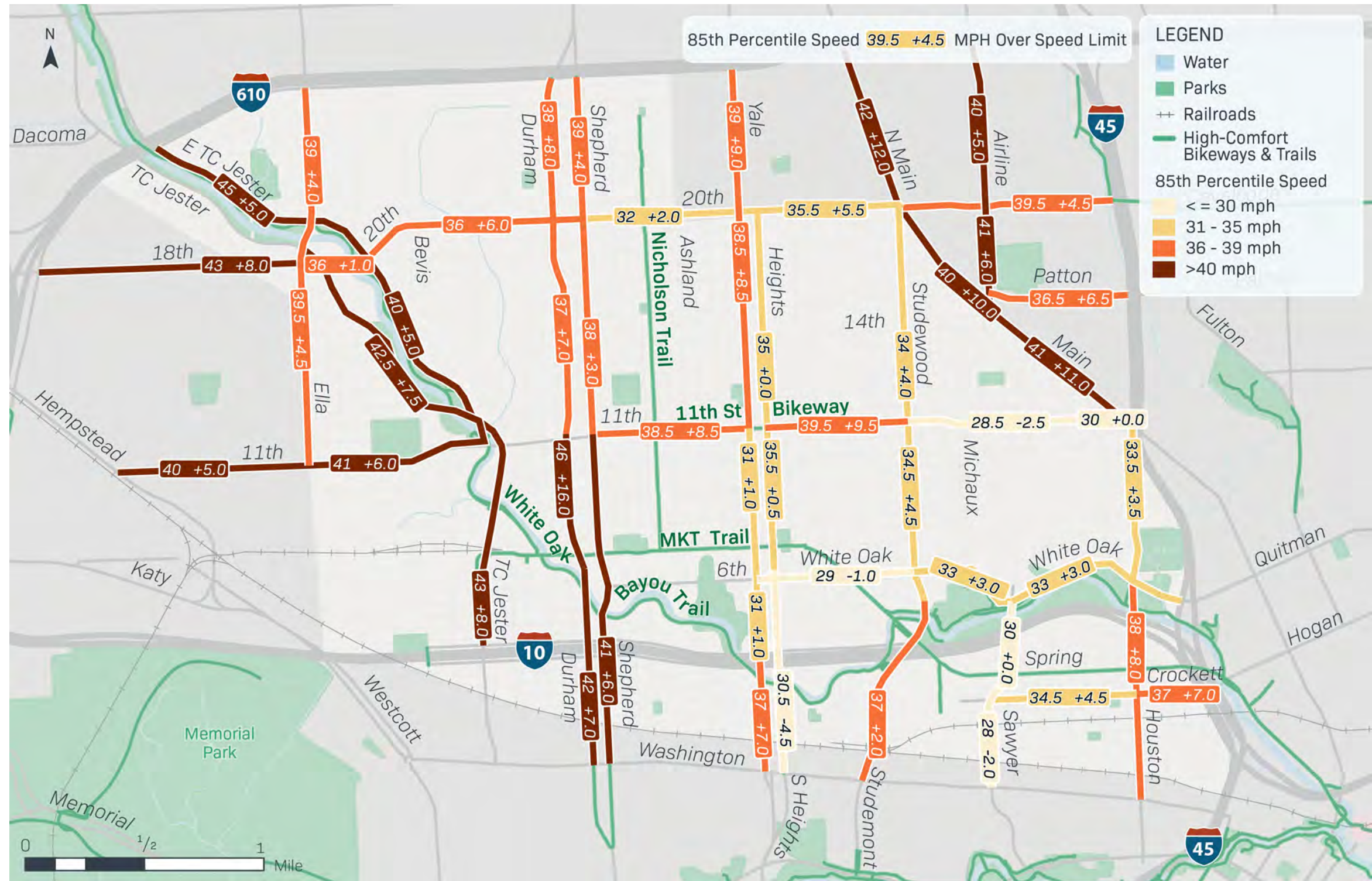


Figure 30. Roadway Posted and 85th Percentile Speeds (Source: City of Houston, 2019-2025).

Figure 31. Posted speed limits over standard 30 mph in the study area (Source: City of Houston, 2025).

Crash Analysis

Figure 32 visualizes crash density in the Study Area between 2020 and 2024. This crash data was collected prior to the Shepherd/Durham reconstruction, which was designed to address crashes on the corridor, as well as other mobility and connectivity improvements. This kernel density estimation was used to visualize where the highest density of crashes occurred on Study Area roadways.

Red lines on the map denote streets designated on the City of Houston’s High-Injury Network: an analysis of the top six percent of roadways in Houston that account for nearly 60 percent of traffic deaths and serious injuries caused by crashes.

While high-crash areas are found throughout the Study Area, high concentrations include on Washington Avenue between Durham Drive and S. Heights Boulevard, Shepherd and Durham Drives at the I-610 intersection, Ella Boulevard and East TC Jester Boulevard, N Main Street, and along Yale Street.

Note: This data was collected prior to the completion of Phase 1 of the Shepherd Durham Project. Some of this data was also collected prior to the completion of the 11th Street Bikeway.

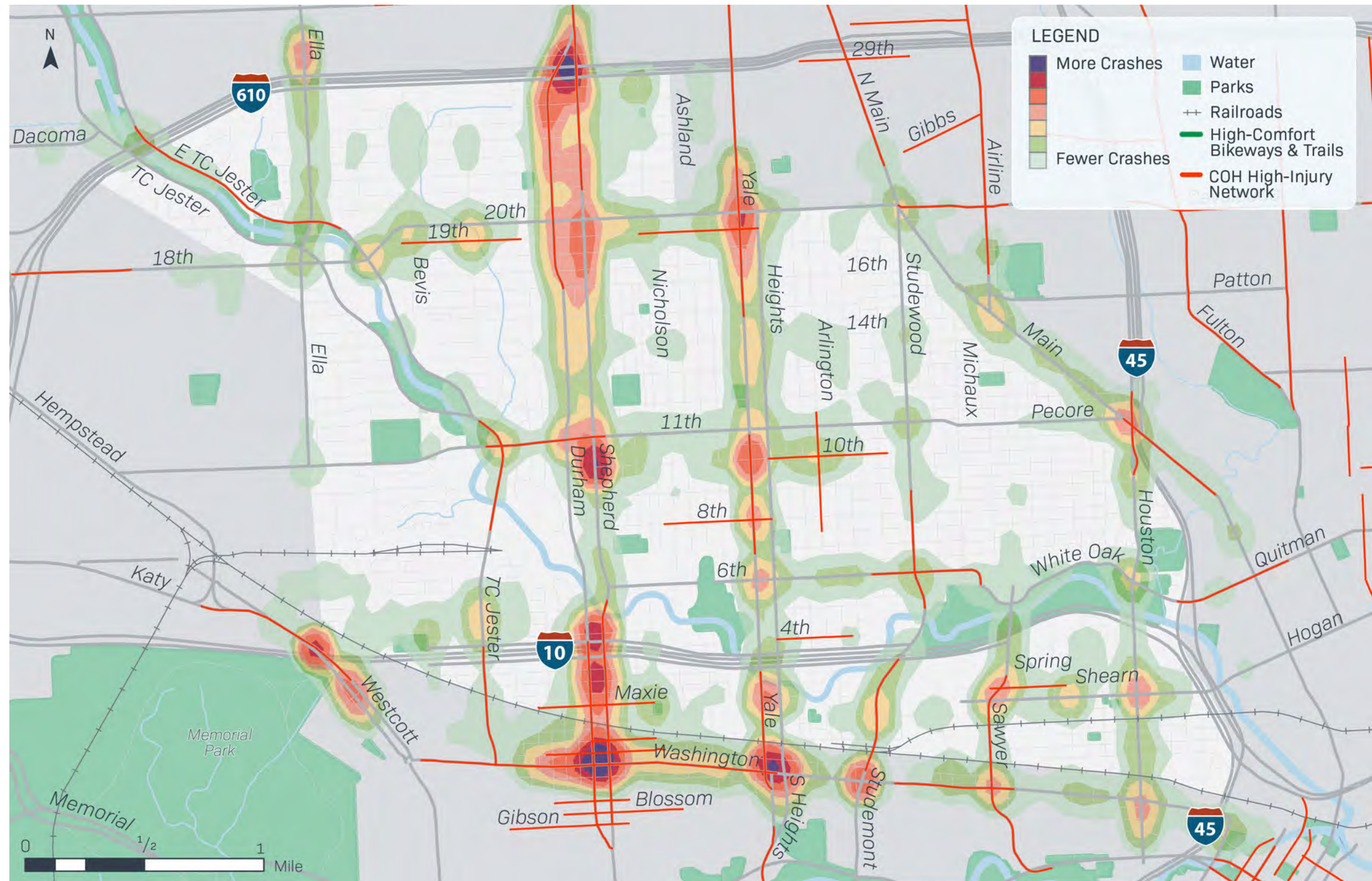


Figure 32. Crash Analysis (Source: TxDOT Crash Records Information System, 2020-2024).

Pedestrian and Cyclist Crash Analysis

While vehicle crashes involving cyclists and pedestrians occur throughout the Study Area, concentrations can be found demonstrating particularly dangerous intersections. Figure 33 displays a kernel density estimation of the largest concentrations of crashes, as well as a record of every individual crash involving a cyclist and pedestrian.

Particularly high densities of crashes are found along Washington Avenue at Shepherd Drive, Durham Drive, Yale Street, and S. Heights Boulevard, and at the intersection of Yale Street, Heights Boulevard, and 20th Street. Other notable intersections include Studewood Street and 6th street, and along the Nicholson Trail and 11th Street Bikeway, at intersections where bikes and pedestrians interact with cars.

Note: This data was collected prior to the completion of Phase 1 of the Shepherd Durham Project. Some of this data was also collected prior to the completion of the 11th Street Bikeway.

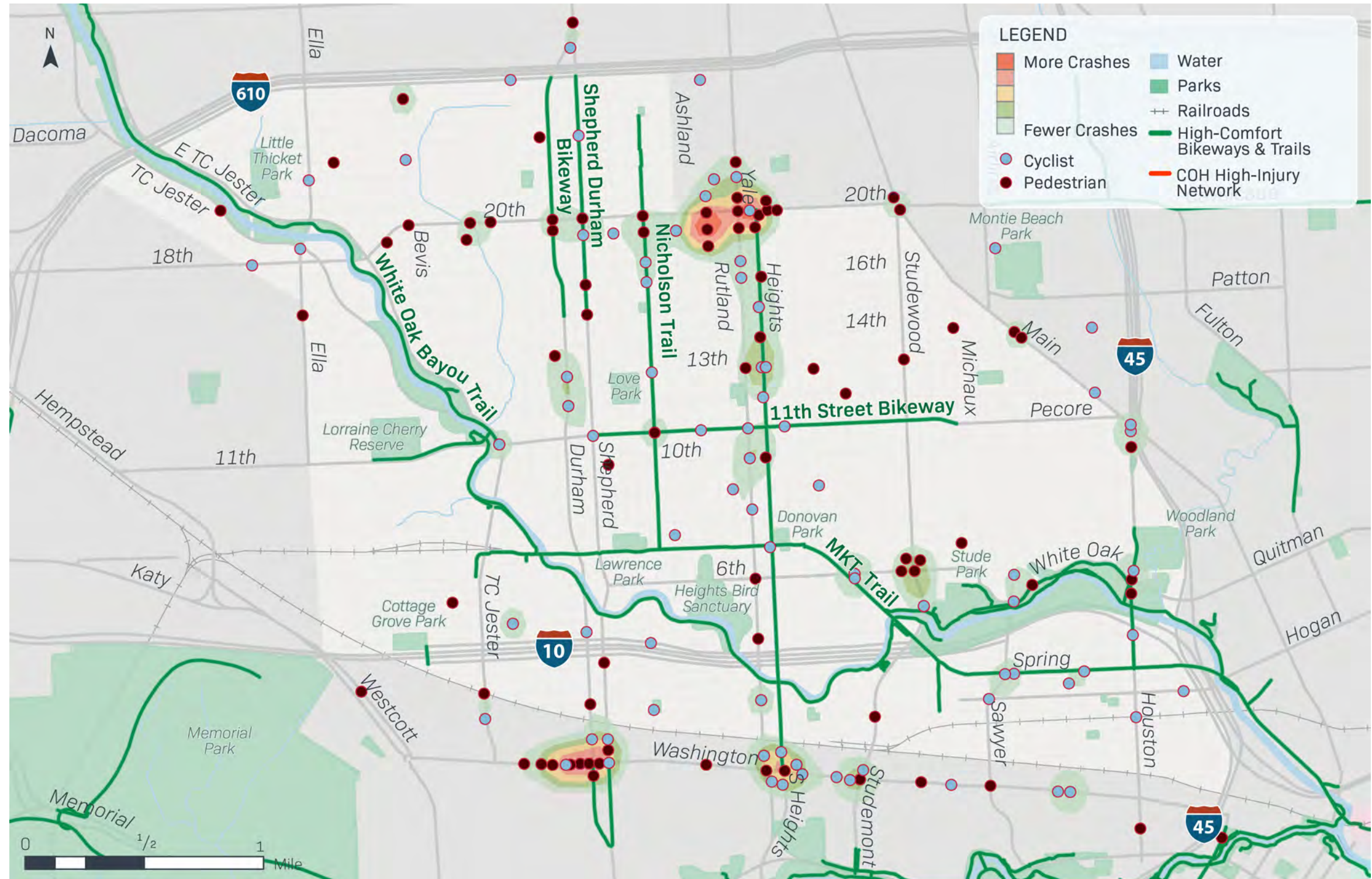


Figure 33. Pedestrian and Cyclist Crash Analysis (Source: TxDOT Crash Records Information System, 2020-2024).

Major Roadway Crossing Distances

Busy roadways, like the ones classified as Major Thoroughfares or Major Collectors, can often act as barriers for safe travel, especially for vulnerable road users like children, older adults, people walking, biking, and using assistive devices like wheelchairs. **The City of Houston's standards recommend safe crossings be spaced 500 to 720 feet** to ensure that people crossing major corridors can get to their destination safely without traveling far out of their way to reach the next crossing. This becomes even more urgent during summer months when people are walking in the heat.

Figure 34 shows the distance between pedestrian crossings along all Major Thoroughfares and Major Collectors. The average distance to the next crossing for all streets in the Study Area is 1,323 feet, nearly twice the recommended crossing distance. Pink and red lines show roadway segments where crossing distances are more than twice and three times the recommendation, respectively.

COH Recommended Crossing Distance: 720 feet
Major Corridors Average Crossing Distance: 1,323 feet
Shepherd Durham Phase 1 Crossing Distance: 485 feet

Phase I of the Shepherd Durham Project added new signals and pedestrian crossings and brought the average crossing distance to 485 feet within the project bounds, better than the City's standard and a threefold improvement compared to the rest of the Study Area.

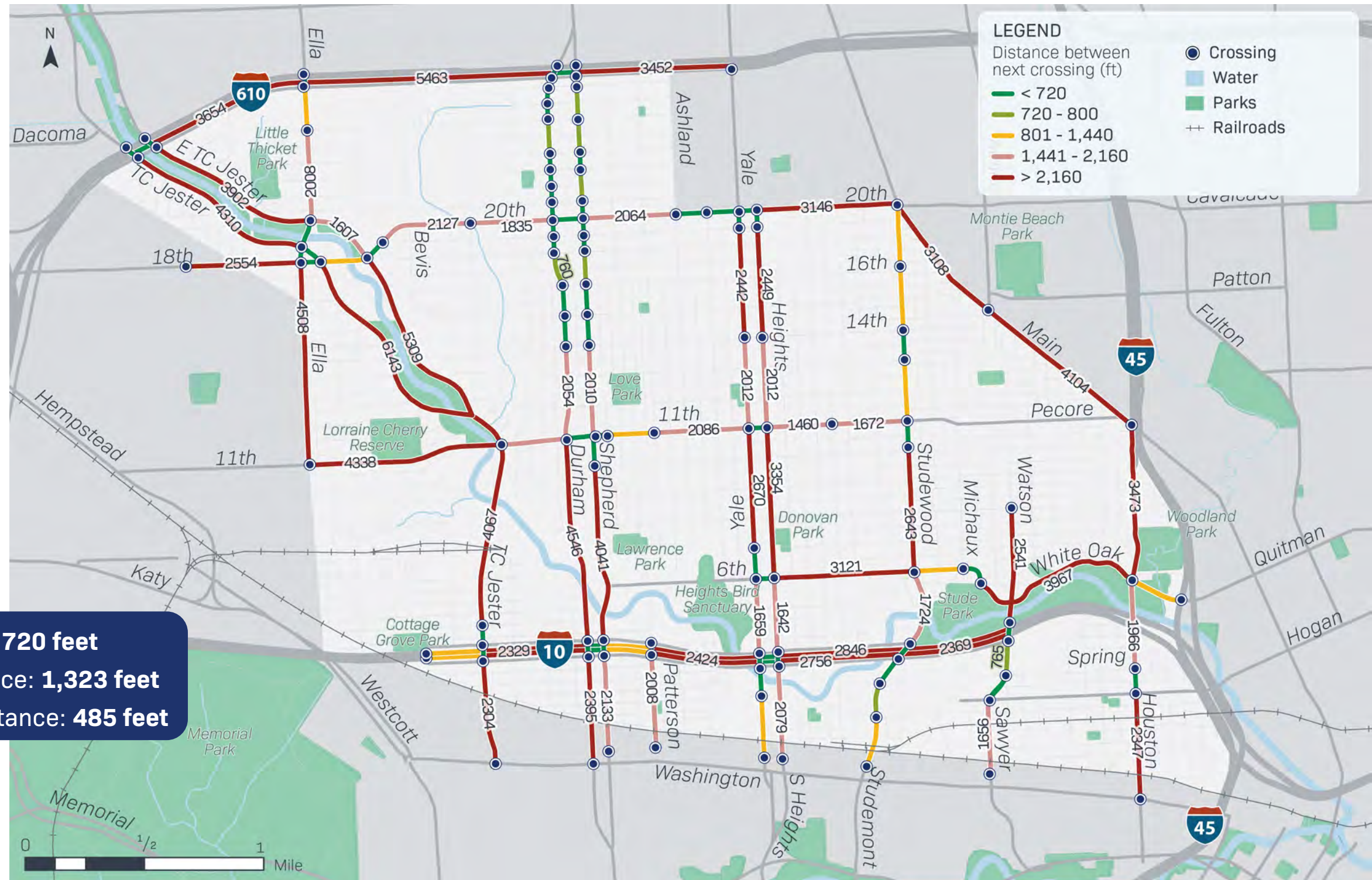


Figure 34. Crossing Distances at Major Thoroughfares (Source: Team Analysis, 2025).

Family-Safe Streets

The comfort level of a street for people walking and biking is determined by many factors, such as availability of paths, safe crossings, vehicle speeds and volumes, among others. To give these data points more meaning, a comfort level was determined from an in-depth analysis of safety and accessibility factors for users outside of a vehicle. The comfort of these streets is shown in Figure 35.

Streets were categorized into four qualifying levels of comfort. Level A represents streets where perceived comfort, in green, is highest—streets where families can comfortably walk or bike and where even the most vulnerable road users are most protected. These include streets with existing trails or wide paths.

Conversely, Level D streets represent those that are most precarious and nearly impossible to travel on outside of a vehicle. Highways and their frontage roads are unsurprisingly Level D streets. Local streets like 20th Street, Yale Street, N Main Street, Ella Boulevard, and others are also Level D. As it stands, these roadways are dangerous and unaccommodating for families.

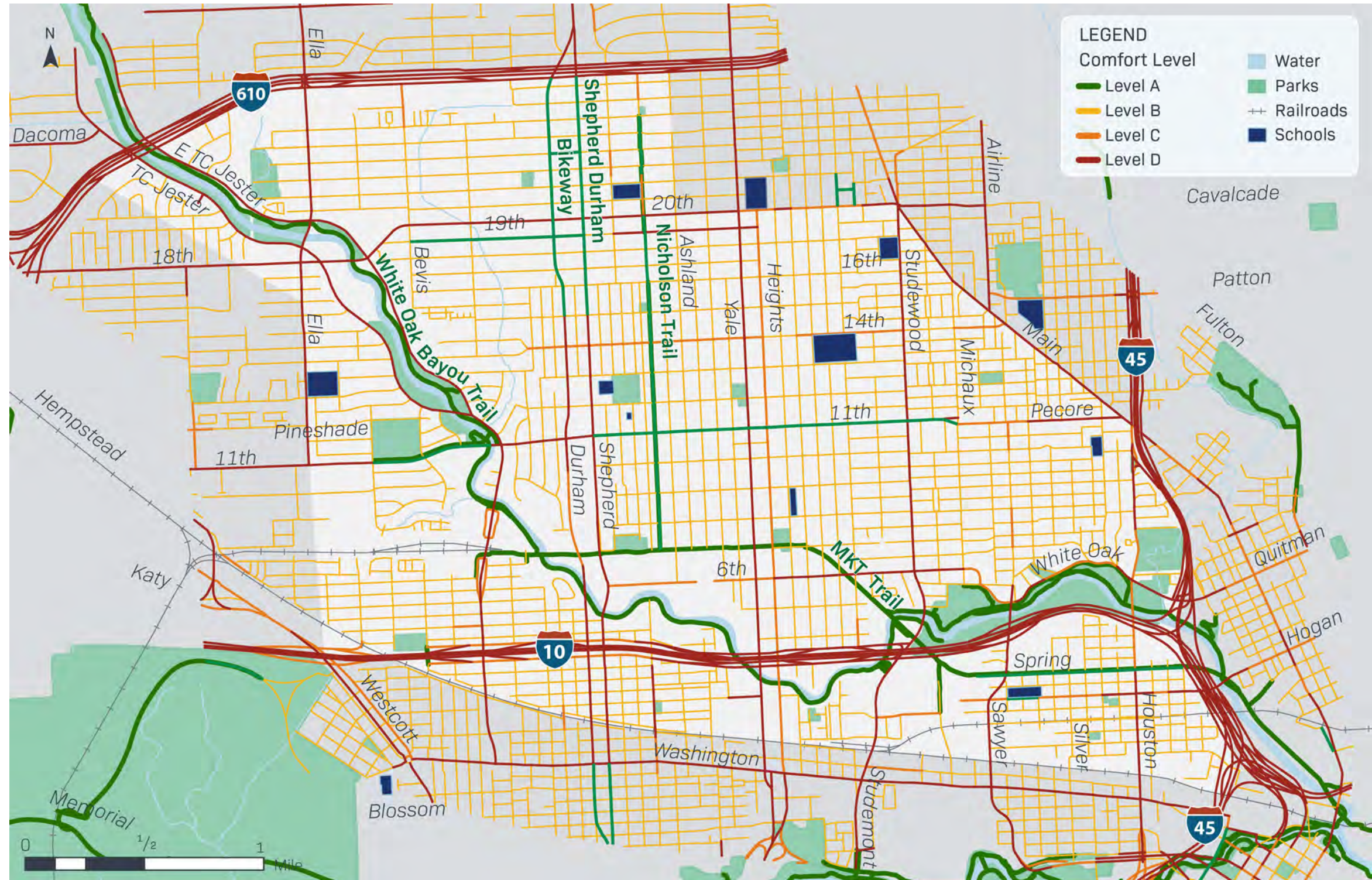


Figure 35. Family-Safe Streets (Source: Team Analysis, 2025)

Mission, Vision, & Goals

Chapter Two

The Greater Heights Strategic Connections Plan

Vision for Mobility

The Greater Heights is an active community built on an historic street grid where safe, easy mobility choices cultivate a healthy, connected, and dynamic place for people to grow up and grow old.



A Vision for Mobility in the Greater Heights

Mission

The Greater Heights Strategic Connections Plan will provide a blueprint for thoughtful design and intentional investment in a set of corridor and intersection projects that realize the Vision of safer, healthier, and more connected streets.

Vision

The Greater Heights is an active community built on an historic street grid where safe, easy mobility choices cultivate a healthy, connected, and dynamic place for people to grow up and grow old.

The Greater Heights Strategic Connections Plan lays the groundwork for implementing mobility initiatives to improve the experience of community members traveling around the Greater Heights area. The Plan serves as a guide for implementing ambitious mobility goals aimed at strengthening the transportation network by addressing existing mobility needs and filling gaps to improve the health and well-being of the Greater Heights community.

The Plan's Vision, Mission, and Goals were identified using two main inputs. First, an analysis of the area's people and infrastructure, found in the Existing Conditions Chapter, highlighted existing gaps and barriers for all modes. Second, direct input from a community survey and interactive map brought to light the key values and concerns of the people who use the street network in the Greater Heights every day. This Plan builds upon existing and planned projects in the area as well as ongoing or planned projects in the surrounding areas.

The Goals developed for the Plan focus on addressing the barriers that community members face in their daily mobility choices and improving their existing experience walking/rolling, biking, taking transit, and driving. These goals will help shape and guide projects that result in a balanced and well-connected multimodal transportation network that is safe, accessible, sustainable, and attractive to all users.

The **Mission** statement describes the purpose of the Plan and how it will work toward the Vision, while the **Vision** describes the aspiration for the Greater Heights in the future. The **Goals** on the next page translate the Vision into outcomes and focus areas.



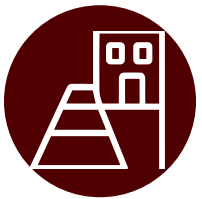
Goals to Bring the Vision Into Reality



Safe Crossings Goal

Improve crossing conditions on key corridors by **reducing the average distance between safe crossings** to bring major roadways into compliance with city standards.

Many major thoroughfares and major collectors in the Study Area are designed with very few crossings and wide vehicle travel lanes, leading to speeds higher than the posted limit and contributing to higher crash rates. This Plan will identify segments along these 15 key corridors that are not currently meeting the City of Houston Infrastructure Design Manual standards, and locate opportunities to add new crossings and improve existing crossings to match the City's spacing and design standards to ensure the safety of all roadway users and encourage responsible speeds.



Complete Sidewalks Goal

Close all gaps in the sidewalk network and repair existing sidewalks that provide important connections to local destinations.

More than one-quarter of all sidewalks in the Study Area are missing, with many more in poor condition. In some neighborhoods such as Shady Acres, West End, and Rice Military, the portion of missing sidewalks is often much higher. This Plan will identify the specific streets and blocks where brand new and repaired sidewalks will help to complete neighborhood walking networks and tie into critical daily destinations like schools, parks, and commercial corridors.



School Streets Goal

Create an environment in which students feel safe walking or biking to school by **implementing safe street infrastructure initially within a 1/4-mile radius of all schools.**

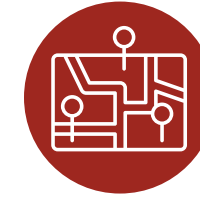
During the first phase of public engagement, community members consistently mentioned a desire for safer streets near schools. Survey respondents identified unsafe intersections and vehicle speeding as the two main concerns for their children's safety. This Plan will pinpoint specific opportunities to make streets and intersections safer within a quarter mile of all schools, giving students more independence and parents more peace of mind. Safer streets near schools also allow parents to drop kids off farther away, knowing they can walk safely to class.



Great Spaces Goal

Design streets and trails as destinations, where abundant shade, lighting, and other amenities encourage people to spend time together outside.

Houston is hot, but the Greater Heights continues to draw people who enjoy the many parks and greenways that define its neighborhoods. This Plan will recommend locations and strategies for adding shade, lighting, seating, and other important amenities to make it more comfortable and safe for residents and visitors to get outside regardless of the weather or time of day.



Connected Network Goal

Increase the share of residents who have excellent or direct access (1/8 mile) to active streets and trails from 20 percent to 100 percent by delivering targeted street and intersection projects that connect to everyday destinations and expand the existing bikeway and trail network.

The neighborhoods of Greater Heights are well-known for their beloved trails and bikeways like the MKT Trail and the new Shepherd Durham pedestrian and bikeway facilities. However, only 20 percent of residents have easy access (one-eighth of a mile) to these great streets. Many of these streets are also disconnected from one another, or end before reaching important community destinations. This Plan will provide the vision for a full network of active streets that provides excellent access to all residents, useful crossings for barriers like White Oak Bayou and I-10, and connections to major destinations such as Memorial Park.

Needs Analysis & Inventory

Chapter Three

The Greater Heights Strategic Connections Plan

Overview

Methodology

Needs Analysis Steps



MEMORIAL HEIGHTS
REDEVELOPMENT AUTHORITY

tei Planning
+ Design

TOOLE
DESIGN

Needs Analysis & Corridor Inventory

This analysis identifies the streets and intersections in the Greater Heights Study Area where walking, bicycling, and rolling are most challenging today and where improvements can have the greatest impact and improve safety and connectivity for everyday trips like reaching school, transit, parks, shops, and trails. By combining data, community feedback, and findings from fieldwork, the project team developed a *People-First Network* – a network of existing active streets and trails, programmed transportation improvement projects, and streets identified through the needs analysis and corridor inventories as high-need for infrastructure improvements to strengthen connectivity and improve safety. This *People-First Network* provides the foundation for the development of a series of project recommendations focused on closing sidewalk gaps, improving crossings and connections, and creating a network of safer, more comfortable walking and bicycling routes across the Greater Heights Study Area. This chapter outlines the methodology used to identify the high-need corridors and intersections where these improvements are most needed.

Overview

Building on mobility goals shaped by community input, GIS data was used to map corridor and intersection characteristics aligned with each project goal. These characteristics were then layered to identify areas where they overlap. The resulting analysis informed the roadway network needed to complete a full *People-First Network (PFN)*, supporting the mission of the Greater Heights Strategic Connections Plan and the study’s goals.

Mission

The Greater Heights Strategic Connections Plan will provide a blueprint for thoughtful design and intentional investment in a set of corridor and intersection projects that realize the Vision of safer, healthier, and more connected streets.

Vision Statement

The Greater Heights community is an active community built on an historic street grid where safe, easy mobility choices cultivate a healthy, connected, and dynamic place for people to grow up and grow old.

Goals



Safe Crossings

Improve crossing conditions on key corridors by **reducing the average distance between safe crossings** to bring major roadways into compliance with city standards.



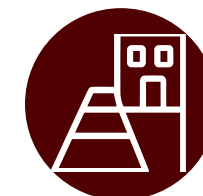
School Streets

Create an environment in which students feel safe walking or riding a bicycle to school by **implementing safe street infrastructure initially within a 1/4-mile radius of all schools**.



Connected Network

Increase the share of residents who have excellent or direct access (1/8 mile) to active streets and trails from 20 percent to 100 percent by delivering targeted street and intersection projects that connect to everyday destinations and expand the existing bikeway and trail network.



Complete Sidewalks

Close all gaps in the sidewalk network and repair existing sidewalks that provide important connections to local destinations.



Great Spaces

Design streets and trails as destinations, where abundant shade, lighting, and other amenities encourage people to spend time together outside.

Methodology for Mapping the *People-First Network*

Building off the existing conditions analysis for this plan, the needs analysis follows a multi-step process to identify streets that are best suited to be considered for the *People-First Network* (PFN). This methodology uses data, public involvement inputs, and fieldwork to ask the following questions:

- 1. What do people need?** Asking this question through a data driven process involved mapping GIS data that corresponds to the stated goals and mapping other notable destinations and demographic context data.
- 2. Where are the biggest barriers?** This question involved overlaying all the mapped features from the first question and processing them into a single prioritization network to identify the areas of highest need for a *People-First Network*.
- 3. Which streets rise to the top?** This step involved visualizing the cumulative scoring of the above two questions to highlight which roadways had the highest need when considering all the combined factors.
- 4. What did we confirm in the field?** This step allowed the team to reality check the highlighted roadways from Step 3 in the field.
- 5. How do we complete a comprehensive *People-First Network*?** This final question involved comparing the community feedback to the needs analysis. In addition, through a series of working meetings the project team finalized the conditions assessments of the *People-First Network* for prioritization and project development and identified additional roadways or connections needed to fully realize the PFN.

The accessibility and proximity analyses used in this Needs Analysis build upon the walk and bike shed models highlighted in the Existing Conditions chapter. The 1/8-mile and 1/4-mile buffers used in Map 4 on the following page are derived from that base dataset, ensuring consistency between the foundational accessibility analysis and the identification of corridors that improve direct connections to existing trails and bikeways.

The datasets used in this analysis represent the most current information available as of 2024–2025, including pedestrian and bicyclist crash records from TxDOT CRIS and publicly available sidewalk and bikeway inventories. While data accuracy is sufficient for planning-level analysis, some map elements (e.g., sidewalk condition and lighting coverage) were validated through field verification. All assumptions and data sources are documented to ensure transparency and reproducibility.

The following sections outline the above-described steps, including conclusions reached through the initial needs analysis of potential network recommendations.

Step 1: What Do People Need?

Mapping GIS Data Corresponding to Project Goals

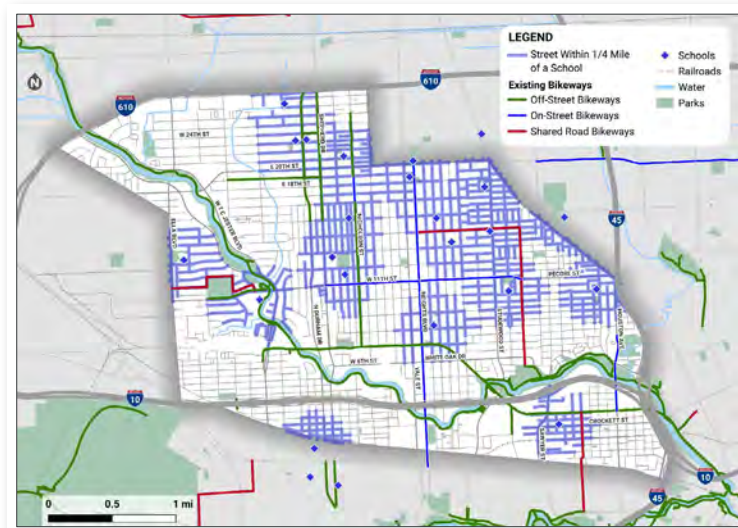


Map 1: Distance Between Marked Crossings

A. Need for shorter distances between marked crossing locations

Map 1 shows distances between crossings of major roadways. Red and light red segments are focus areas and potential links in the *People-First Network* because they have long distances between crossings.

Long distances between crossings make it difficult and often unsafe for people to walk or roll across major streets. Identifying locations where projects can improve crossing safety or reduce the distance someone has to go to cross a street safely will improve mobility across the Greater Heights Study Area and make strides towards achieving this corresponding goal.



Map 2: Access to Schools

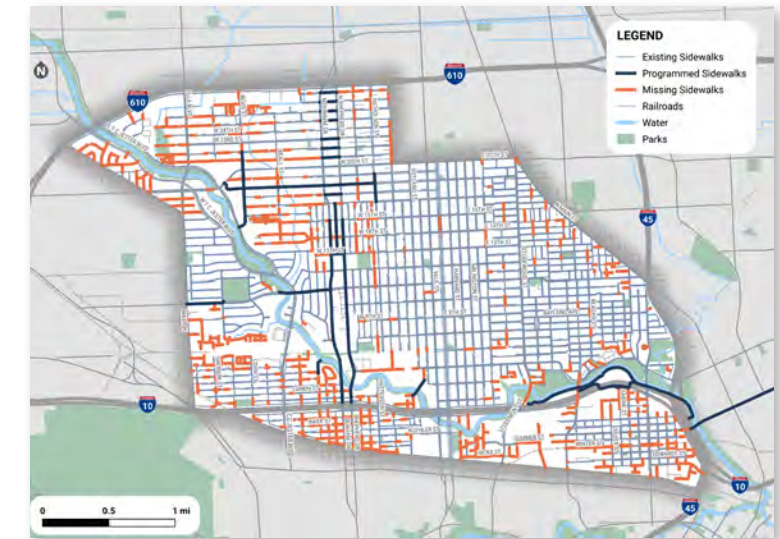
B. Need for safe access to schools

The project team created a quarter-mile buffer around every school, including private schools and Pre-K, and highlighted network segments that fall within this buffer. Blue streets in Map 2 represent focus areas for the *People-First Network*.

C. Need for more sidewalks

Map 3 displays sidewalk gaps in the Study Area based on Houston-Galveston Area Council's 2024 regional sidewalk inventory. Highlighted segments are focus areas and potential links in the *People-First Network* because they lack complete sidewalk connectivity.

Sidewalk gaps especially affect access to parks, schools, and transit. These were among the most common concerns heard from residents. Some sidewalk gaps will be addressed by programmed projects. As part of the Needs Analysis, the project team considered where sidewalk gaps will be addressed through programmed projects in subsequent steps.



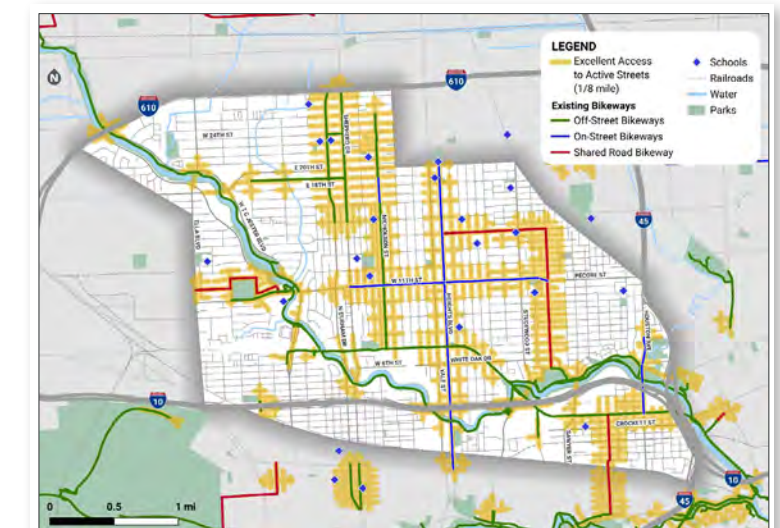
Map 3: Sidewalk Gaps

D. Need to improve access to active streets

Map 4 displays areas with excellent access (1/8 mile or less) to active streets. Active streets are defined as streets and trail corridors where walking, riding a bicycle, and rolling are generally safer and more comfortable. This includes designated walking and bicycle facilities such as White Oak Bayou Trail and the Shepherd Durham Corridor, as well as streets like Michaux Street that are designated as Shared On-Street bikeways by the City of Houston.

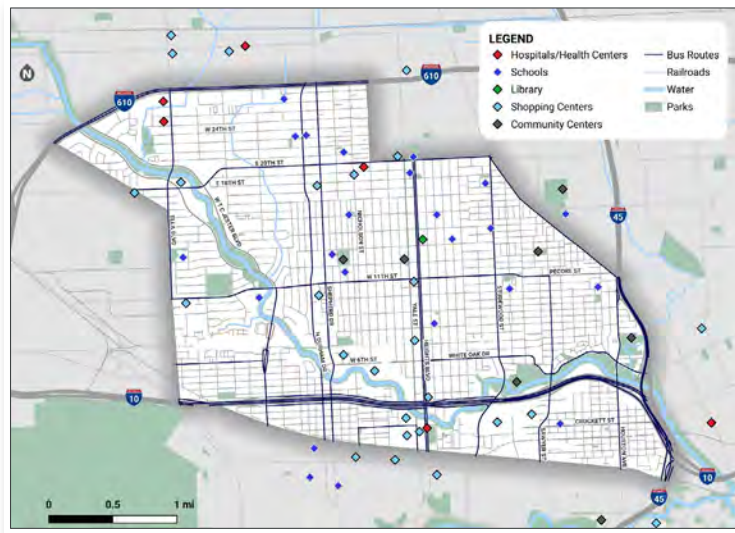
White areas on the map, outside of the yellow segments, are focus areas. Segments in these areas are potential links in the *People-First Network* because they are beyond 1/8 mile from active streets.

Together, Maps 1 through 4 identify where crossing gaps, sidewalk gaps, and distance from active streets overlap the physical barriers that shape everyday movement and community connectivity in the Study Area.



Map 4: Access to Active Streets

Mapping Area Context



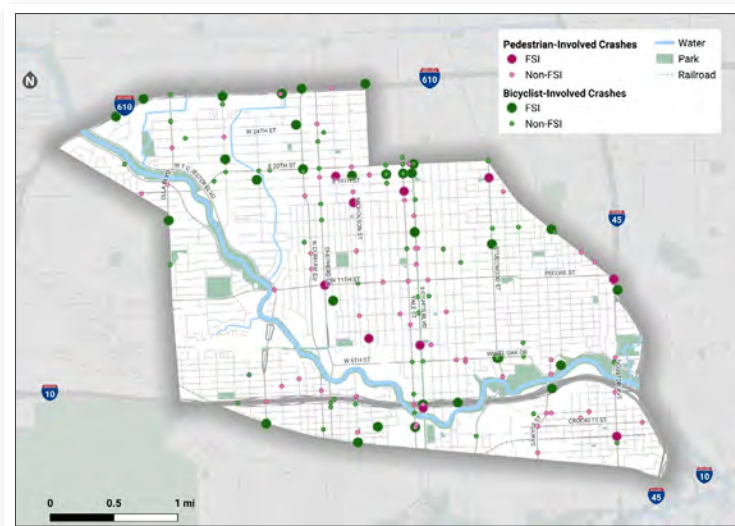
Map 5: Destinations to Daily Life Activities

Destinations for Daily Life Activities

Map 5 displays the locations of various destinations in the study area. *The People-First Network* prioritizes connecting to as many of these destinations as possible with safe walking and bicycle routes.

Crashes Involving Vulnerable Road Users

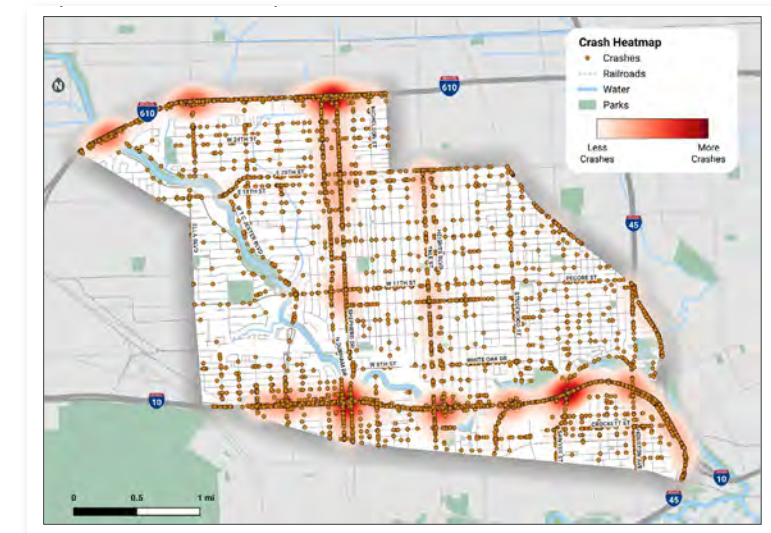
To assess the traffic safety needs of vulnerable road users (VRU) and all road users, the project team queried TxDOT’s Crash Records Information System (CRIS). Map 6 displays crashes involving pedestrians and bicyclists that occurred in the Study Area from 2020 through October 21st, 2025. *The People-First Network* added scoring and prioritization for segments where these VRU crashes occurred. Higher crash volume corridors reflect higher scores for opportunities to improve safety conditions. In the context of this analysis, Fatal and Severe Injury (FSI) Crashes are identified separately from Non-FSI crashes to assess if there are environmental conditions leading to more severe crash outcomes in any given location.



Map 6: Crashes Involving Vulnerable Road Users (2020 through Oct 21, 2025)

High Risk Areas for All Road Users

Map 7 shows the crash density for all crashes between 2020 and October 2025 for all road users in the study area. This review of crash data, in addition to the review of VRU crash data, provides insight into where the highest risk areas are for all road users and can be used as an additional screening and prioritization tool when defining projects’ purpose and need in following tasks.



Map 7: All Crashes 2020-2025 Heat Map

Step 2: Where are the Biggest Barriers?

Where is the *People-First Network* Most Needed?

To identify which streets should be included in the *People-First Network*, the project team added each of the need indicators from the earlier steps to the underlying street map. Each block of every street was evaluated to determine whether it had a sidewalk gap, a missing or difficult crossing, safety concern, or another mobility need. For every condition that applied to a block, that block received one point.

After assigning points for all relevant conditions, each block received a total score representing how many different needs occur in the same place. No weighting was applied; all indicators were treated equally. Blocks with higher scores have more overlapping needs and therefore a higher priority for improvement. Because there were nine total indicators, scores ranged from zero to nine. Appendix D further details the scoring attributes applied to the network.

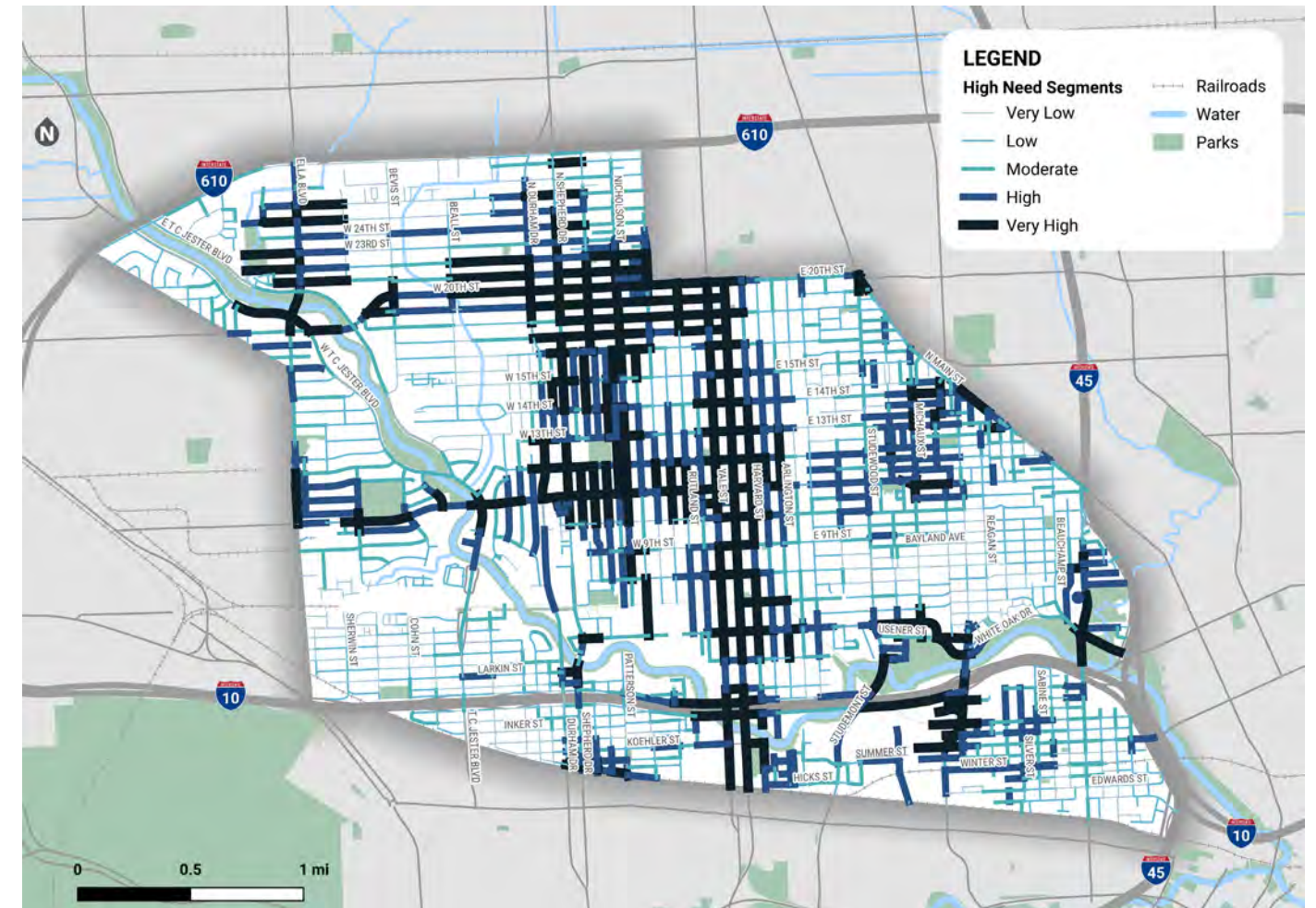
These scores form the basis of the draft *People-First Network*. Step 3 consolidates many different types of information into a single, clear picture of where mobility investments will have the greatest benefit. The analysis from Step 1 illustrated gaps in sidewalks, crossings, and safety. The analysis from Step 2 identified the destinations people travel to most often and the communities that depend most on walking, bicycling, and transit. Combining these elements helps reveal the blocks where multiple needs occur in the same location. These are places that both lack comfortable walking and bicycling conditions and provide access to important destinations such as those shown in Map 5.

This cumulative approach ensures that the *People-First Network* reflects the intersection of infrastructure gaps, safety challenges, and everyday access needs.

Map 8 presents the first iteration of high-need corridors. Dark clusters of network links represent the corridor segments with the highest number of attributes. These darker segments indicate focus areas for potential safety and connectivity improvement projects. To summarize, these areas have some combination of the following attributes:

- Long distances between crossings
- Gaps in the sidewalk network
- Proximity schools
- Proximity to parks
- Proximity to other destinations
- Lack of proximity to Active Streets
- Proximity to a bus stop
- Presence of a vulnerable road user crash

Appendix D shows high-need corridors overlaid with areas of vulnerable populations, such as youth and older adults, as well as households without access to a vehicle. This data further refines project priorities and recommendations.



Map 8: Initial Development of High-Need Corridor Segments

Step 3: Which Streets Rise to the Top?

To develop a cohesive set of corridors for assessment, the cumulative scoring from the previous two steps was reviewed at the segment and corridor level. Corridors were ranked based on the frequency of high-need segments (blocks), the frequency of each corridor’s ranking across all need areas, and consideration for where a continuous selection of segments completed a contiguous connection for users potentially accessing the White Oak Bayou Trail.

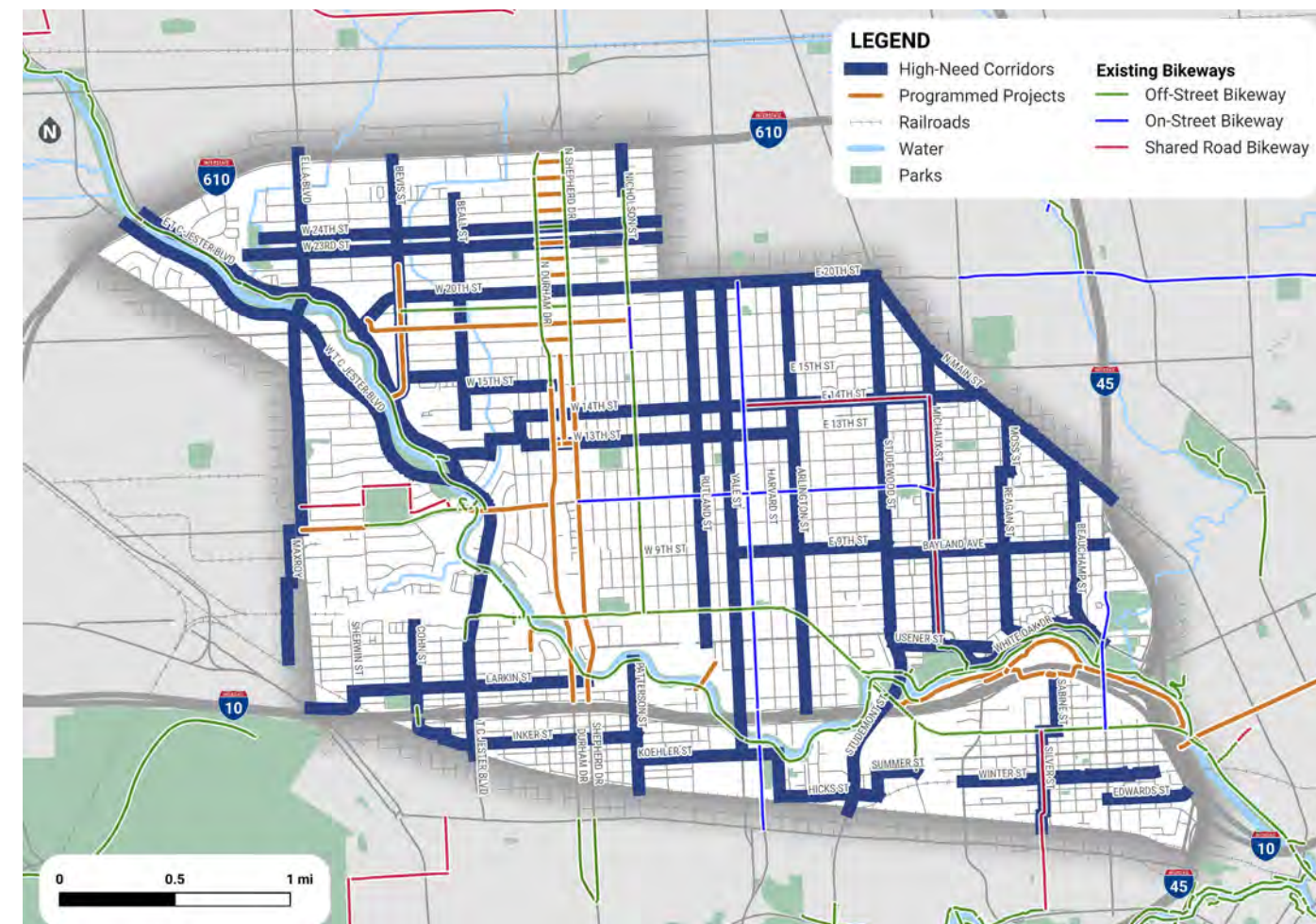
Table 3 in Appendix D displays the highest ranked roadways that resulted from this cumulative attribute scoring. The project team reviewed these initial corridors through a series of working meetings with the client, resulting in a refined list of roadways identified for fieldwork assessment. The initial list of high-need corridors included many streets that have already received investment by MHRA, are programmed for future investment, or run parallel to an existing active street or other corridor on the *People-First Network*. Table 4 in Appendix D also shows where several roadways were removed from the final fieldwork assessment list due to one of these factors. The project team also added other roadways to the list of streets for fieldwork in order to achieve the overall plan goals by (1) extending the *People-First Network* to connect to important destinations and underserved areas, and (2) completing the full network of corridors by connecting key segments of the *People-First Network*. Additions include streets that provide important connections for residents in Shady Acres, Woodland Heights, Norhill, Sunset Heights, First Ward, and the Ella Boulevard to Cottage Grove connection.

After refining the roadway segments, the project team then reviewed how these corridors connect to the existing active streets network

and programmed projects within the Study Area. This connectivity review helped ensure that high-need corridors not only address data-identified needs but also strengthen a continuous, safe, and comfortable network. In developing assignments for conducting fieldwork on these streets, particular focus was given to improving connections to and across the White Oak Bayou Greenway, a key east-west spine for walking and bicycling. This review was used to help finalize the *People-First Network* by prioritizing corridors that close critical gaps and enhance access to regional trails and planned improvements.

The final fieldwork assessment roadways identified in Table 3 in Appendix D begin to highlight a network of streets that, when considered in tandem with programmed projects, can be prioritized for future investments that will improve safe access across the Study Area and provide greater connectivity to White Oak Bayou and other key destinations. The list of roads that were removed from the list as needing further assessment are included in Table 4 in Appendix D, along with a notation on why the selected roadway was not included for further analysis.

The identified high-need corridors, when combined with the programmed projects and the existing active streets and trails in the Study Area, will create a safe and accessible *People-First Network*, as illustrated on the map in Map 9.

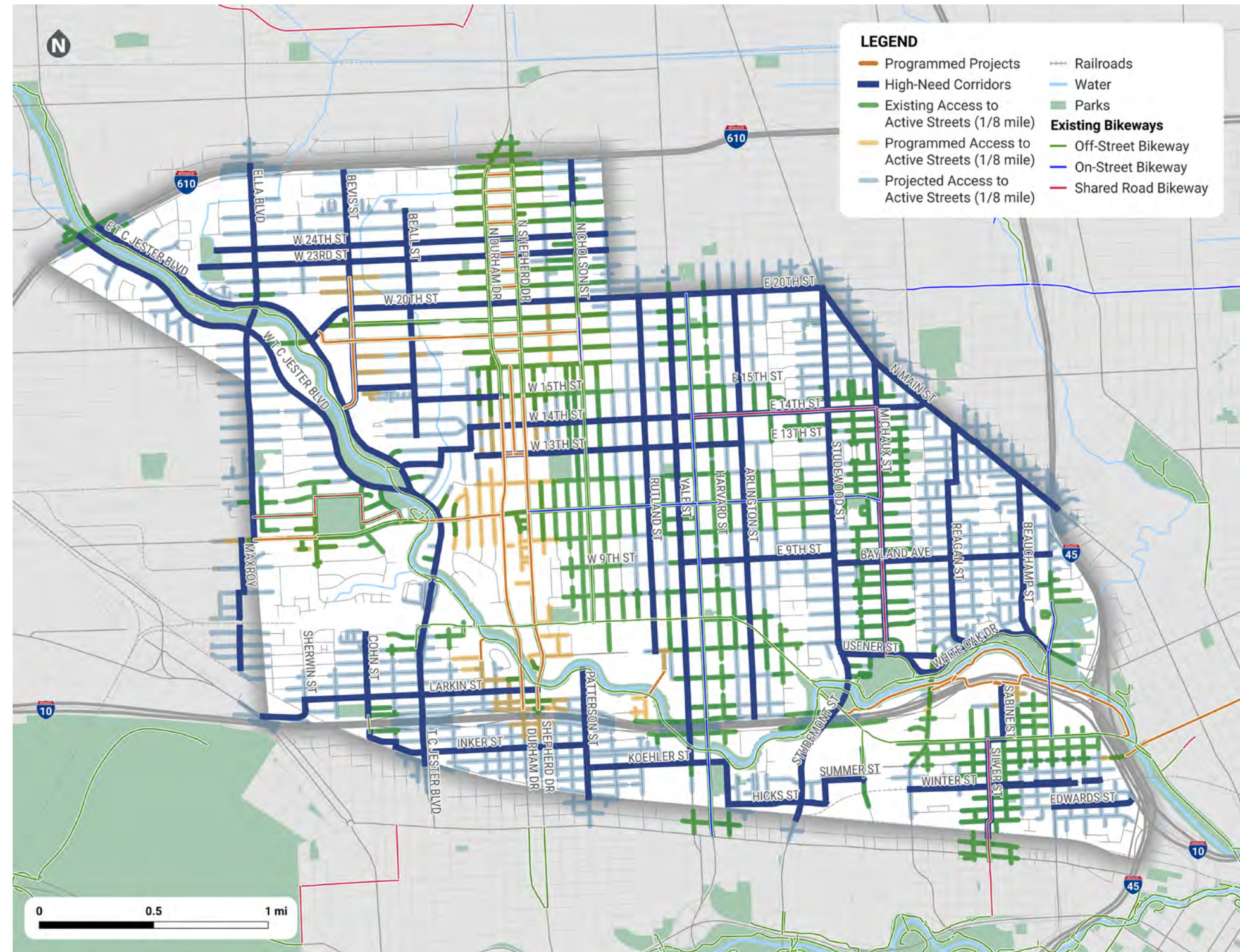


Map 9: The Greater Heights *People-First Network*

Access to Active Streets

High-need corridors, existing active streets, and programmed projects were then viewed in comparison to mapped destinations to assess visually if the proposed *People-First Network* will provide adequate access (within 1/8 mile) to the mapped destinations. Appendix D shows existing and programmed projects, high-need corridors, and destinations mapped.

Finally, the existing, programmed, and identified high-need corridors were viewed in reference to destinations and accessibility to active streets. As an illustrative step to gauge the potential impact of investing in improvements on the high-need corridors, a 1/8 mile walkshed was applied to the high-need corridors to illustrate the projected, improved community access to active streets if the investments were made in these corridors to realize the complete *People-First Network* (Map 10).



Map 10: Existing and Projected Access to Active Streets

Step 4: What Did We Confirm in the Field?

Results of the Needs Analysis established a clear foundation for developing the Corridor Inventory. The priority segments identified herein were used to guide the field data collection phase. Using the Fulcrum digital survey tool, the project team spent two days in the field from November 18th through November 19th conducting an existing conditions inventory of the selected roadways, including details such as presence and conditions of sidewalks, curb ramps, crossings, lighting, shade, and traffic operations. Findings from fieldwork were used to verify and refine the mapped priorities before developing project recommendations.

A total of 736 data points were gathered during fieldwork and focused on selected high-need corridors and intersections to provide a conditions assessment of the selected corridors and support future project recommendations intended to improve conditions of sidewalks, curb ramps, crossings, lighting, and shade. Map 11, along with the additional fieldwork maps in Appendix D, highlight locations of data collected during the project team’s fieldwork, including specific existing conditions data pertaining to sidewalk and curb ramp conditions, intersection and roadway features, and presence of pedestrian-scale lighting.

Fieldwork Results

Field data collected across the Greater Heights Study Area indicates a concentration of high-need intersections and corridors related to missing pedestrian infrastructure, poor curb-ramp conditions, and incomplete sidewalk networks.

During fieldwork, the project team walked each priority corridor and intersection to document

conditions such as sidewalk gaps, broken pavement, missing curb ramps, poor lighting, and unsafe crossings. Each issue was noted and tallied so that locations with multiple overlapping problems would rise to the top.

Intersections, roadway segments, and streets were ranked based on a number of variables, detailed in Appendix D. These ranked corridors and intersections will guide the next phase of the project, when specific improvement concepts are developed.

The highest-need intersections, such as Yale at 4th, 10th, and 15th; Arlington at 14th; and Ella at 24th and 25th, exhibit multiple ADA deficiencies, incomplete crossings, and wide vehicle-dominated geometries that significantly impede safe walking and rolling. Corridor evaluations highlight extensive sidewalk gaps and accessibility issues along Reagan, Arlington, Rutland, 13th, and 14th Streets, as well as in Cottage Grove and the White Oak/Usener/Watson area. These findings provide a clear basis for prioritizing sidewalk and crossing improvements, guiding field verification, design direction, and the development of the full *People-First Network*. The assessment tables in Appendix D highlight observed issues at high-need intersections, specifically noting frequency of poor or missing curb ramps, the severity of conditions as noted in the field comments collected, as well as a summary of improvement needs.



Map 11: Fieldwork Data Points

Key Findings: Corridors

- Missing sidewalks are the most widespread infrastructure gap.
- Arlington, Rutland, Reagan, and 13th have multiple concurrent issues.
- Cottage Grove has systemic sidewalk absences and drainage conflicts.
- Trail access points are major pain points (White Oak, MKT).



Broken sidewalks with drainage conflicts, TC Jester Blvd

Key Findings: Intersections

- Yale crossings are consistently unsafe (4th, 10th, 15th).
- Major arterials (Ella, Main) require enhanced crossings.
- Multiple intersections lack ADA ramps entirely.



Heights Blvd at Koehler St intersection lacks adequate crossing markings

Step 5: How Do We Complete a *People-First Network*?

The final stage or question in this process was assessing what a complete network would look like that could provide access to good active streets throughout the Greater Heights Study Area and fulfill the goals of this Plan. This involved two primary steps:

1. Reviewing community feedback
2. Assessing if there were any gaps in coverage and proposing additional roadways to fill the gaps

Community feedback gathered through the Greater Heights Strategic Connections outreach process reinforced many of the physical gaps identified in this analysis. Participants frequently cited concerns about unsafe crossings, missing sidewalks, and a desire for shaded, comfortable walking routes and protected bikeways.

Map 12 shows all the community feedback points geocoded, combining feedback points from the various engagement events as well as feedback points from the Social Pinpoint Online Feedback Map. These points, along with the specific feedback associated with each point, was then used to identify what types of improvements the community is in support of, and to provide additional scoring for the assessment of the PFN.

What We Heard: Six Major Themes

1. Sidewalk and ADA access are the #1 concern, especially near schools and trail access points.

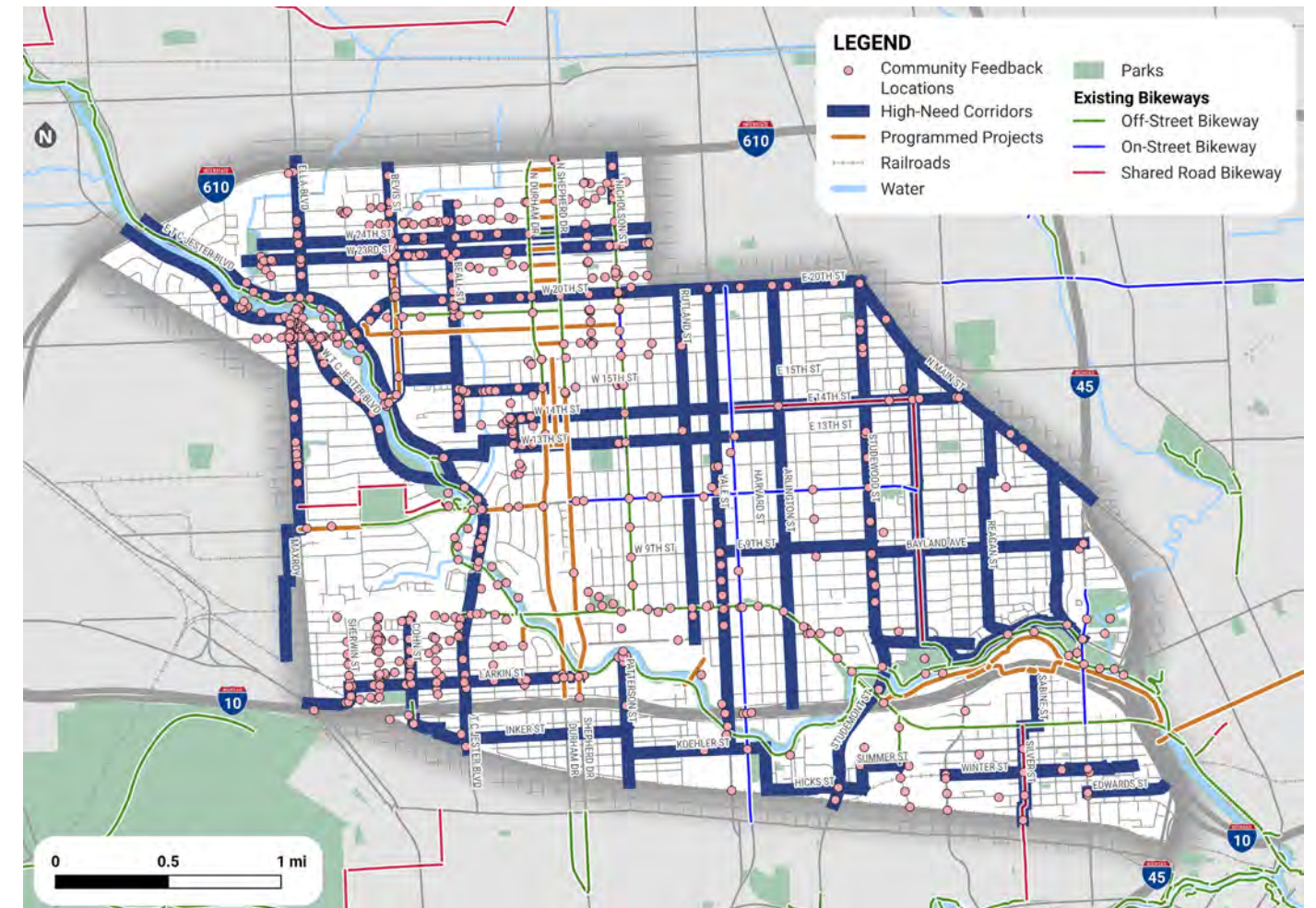
2. Crossing safety is the #2 concern, with a strong geographic pattern: Washington Ave, Yale, Main, 11th, 20th, Ella, and TC Jester.

3. Lighting deficits on trails and access routes are constraining evening and wintertime mobility.

4. There is strong public demand for **completing trail connections** between MKT Trail, White Oak Bayou Greenway, Memorial Park, and the Greater Heights street grid.

5. Traffic management around schools should be a top priority, based on both comment frequency and severity.

6. Residents strongly value **shade and heat mitigation**, which should inform the comfort index of the People-First Network.

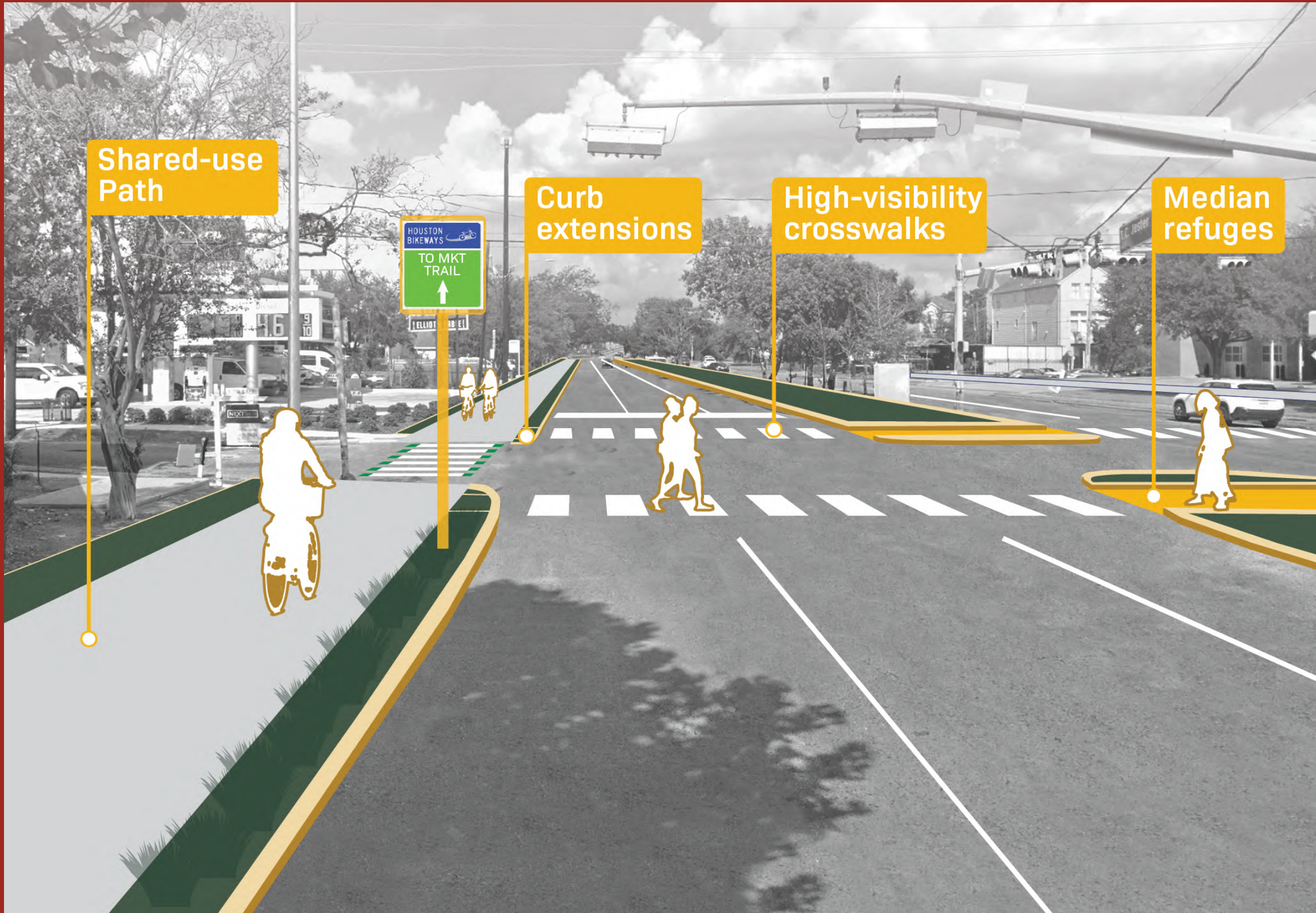


Map 12: Community Feedback Locations

Project Recommendations

Chapter Four

The Greater Heights Strategic Connections Plan



MEMORIAL HEIGHTS
REDEVELOPMENT AUTHORITY

tei Planning
+ Design

TOOLE
DESIGN

Recommended Projects

Investments for Safer Streets in Greater Heights

This chapter presents **more than 54 miles of Project Recommendations along a grid of streets across the Greater Heights**. These recommendations build upon the Greater Heights' strong grid of active transportation corridors including the Shepherd–Durham Corridor, Nicholson Trail, 11th Street Bikeway, White Oak Bayou Trail, and the MKT Trail, with the goal of increasing access to “Active Streets and Trails,” as defined in the Existing Conditions. Together, the projects increase the number of residents who can step out from their front door and be on an active street within only two blocks.

Project Recommendations are derived from the Needs Analysis. They expand the *People-First Network* to meet the mobility goals of the Greater Heights community. This chapter showcases nine **Catalyst Projects**, shown in gold on the next page. Catalyst Projects are investments to most critical corridors that address specific safety and accessibility concerns on streets in Greater Heights with the highest need.

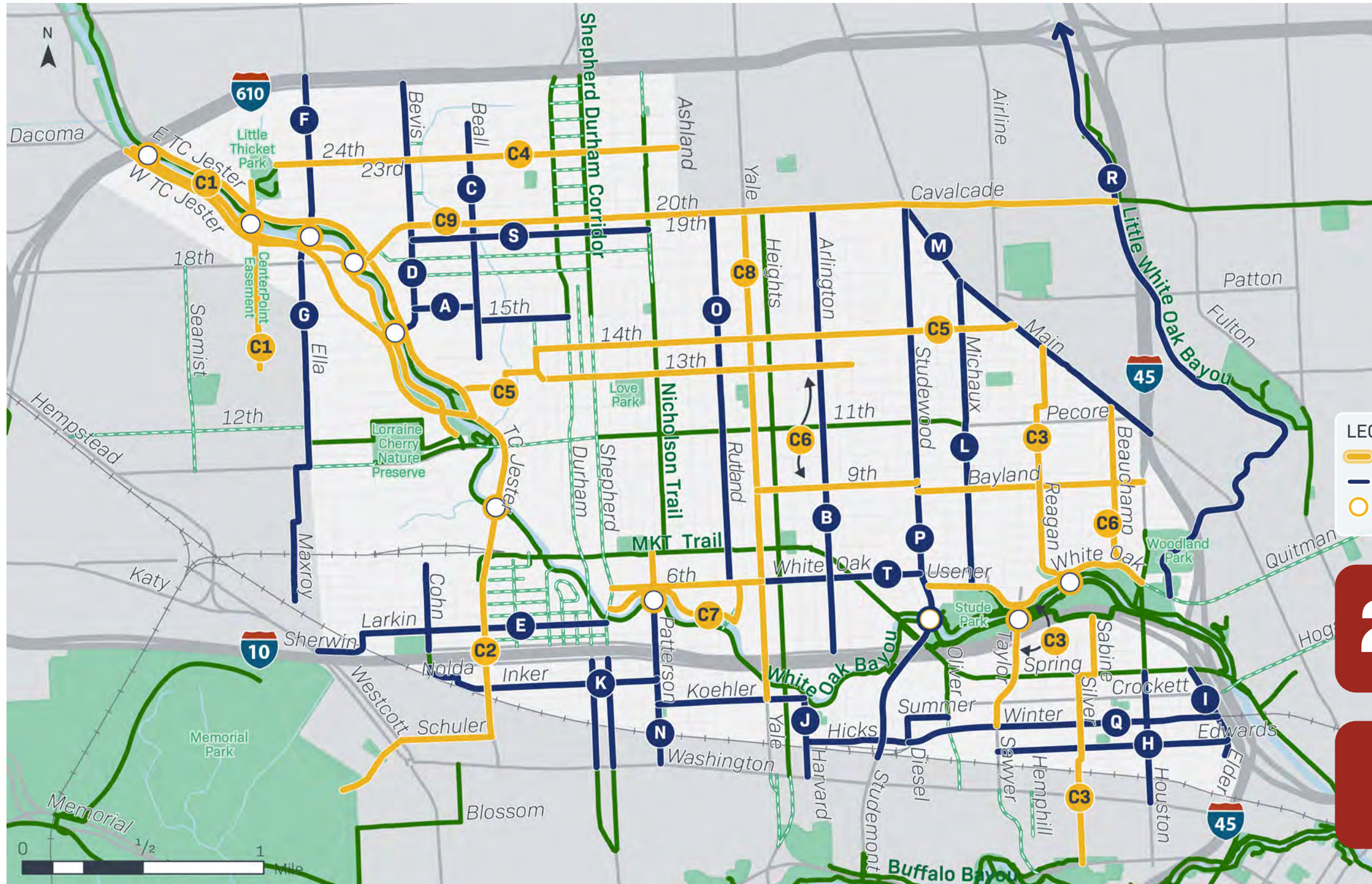
Beyond Catalyst Projects, other projects focus on ensuring that people-first streets are geographically distributed across the Greater Heights and better move people across barriers. For example, Projects tagged as **White Oak Bayou recommendations** address access and safety across and along White Oak Bayou. **Neighborhood Streets** projects address non-thoroughfare corridors that support comfortable walking and biking. Together, all projects strengthen connections between neighborhoods and nearby schools, parks, destinations, and regional amenities such as White Oak Bayou, Memorial Park, Downtown, and adjacent neighborhoods, supporting the *People-First Network*.

The Project Recommendations close network gaps, improve crossings of major barriers, and strengthen east–west and north–south connections. Accomplishing this supports a more complete system for walking and biking that expands access to everyday destinations as well as regional trails, parks, and employment centers.



Together, these projects create *Greater Heights Strategic Connections.*

Recommended Projects



Recommendations

Figure 1 shows the full set of Project Recommendations for the Greater Heights Strategic Connections Plan. Together, these projects help to achieve the Plan’s Vision & Goals by creating a safe, connected grid of *People-First Streets* throughout the Study Area.

These projects build upon the existing network of active streets and trails, and serve as a complement to previous planning efforts like the City of Houston Bike Plan and plans by TIRZ 12 and TIRZ 13.

Table 1 on the following page lists the projects by letter and number corresponding with this map.

LEGEND

Catalyst Project	Active Streets & Trails	Water
Other Project	Programmed Projects	Parks
Key Bayou Crossing Improvement	Railroads	

29 projects

54 miles

Figure 1. Greater Heights Strategic Connections Plan Recommended Projects (Source: Team Analysis, 2026)

Greater Heights Strategic Connections Projects

White Oak

	ID	Project Name	Pg.	Description	Mi.
Catalyst Projects	C1	White Oak Connectivity Vision	79	New trail on Bayou western bank & CPE easements, retrofit & new bridge improvements, and safer crossings of TC Jester Blvd.	6.6
	C2	White Oak to Memorial Park Connection	82	Retrofit improvements on TC Jester Blvd. & Schuler St. for safe walking/biking link from MKT Trail & WOB to Memorial Park	2.1
	C3	Heights to Buffalo Bayou Corridor	84	Retrofit Reagan, Silver, Taylor, & White Oak Sts., Rebuild Sabine & Usener Sts., and a new bridge for connection over WOB & I-10	3.8
	C4	Shady Acres Greenway	86	Reconstruction of W 24th, 23rd, or 22nd Sts with shared-use path & safety improvements from Little Thicket Park to Nicholson St.	1.7
	C5	14th Street Reconstruction	88	Reconstruction of 14th, Grovewood, and Wynnwood Sts. to formalize an east-west Heights link from WOB to N Main St.	2.5
	C6	Heights School Streets	91	Reconstruction/Retrofit Neighborhood Street improvements to 13th, 9th, Bayland, & Beauchamp Sts. for safe school access	3.5
	C7	MKT Trail/Bird Sanctuary Loop	93	New bayou trails, new bridges, & improved (retrofit) local street connections to MKT Trail & local commercial destinations	1.7
	C8	Yale Street Improvements	95	Phased retrofit safety improvements to include sidewalk & crossing improvements with long-term reconstruction options	2.0
	C9	20th Street Reconstruction	99	Reconstruction as two-lane roadway with center turn lane, median, and wider sidewalks with median where ROW allows	3.2
Projects	A	W 15th/W 15 1/2 Streets	101	Retrofit Neighborhood Street improvements; east-west connections from Bevis St. to the Shepherd-Durham Corridor	0.7
	B	Arlington Street	102	Retrofit Neighborhood Street improvements; north-south connections from I-10 to W 20th, incl. safe school streets	1.9
	C	Beall Street	102	Reconstruction Neighborhood Street improvements north of 20th St. & Shared Use Path from 20th St. to 14th St.	1.0
	D	Bevis Street	103	Reconstruction Neighborhood Street improvements north of 20th St. & Shared Use Path from 20th St. to E TC Jester Blvd.	1.0
	E	Cohn Street/Larkin/Sherwin	103	Reconstruction Neighborhood Street improvements in Cottage Grove for future connection to Memorial Park	1.7
	F	Ella Blvd (N)	104	Reconstruction of Ella Blvd. north of 18th St. with safer intersections and curb changes to accommodate a shared-use path on both sides	0.8
	G	Ella Blvd (S) & Maxroy Street	104	Reconstruct Ella Blvd. south of 18th per TIRZ 12 Mobility Plan recommendations; Redesign Maxroy St. as Neighborhood Street	1.3
	H	Edwards Street & Houston Avenue	105	Redesign Edwards St. and Houston Ave. per TIRZ 13 Mobility Plan; for Edwards St., continue improvement west to Elder St.	1.5
	I	Elder Street Trail	105	Build extension of the Spring St. Trail south to Dart St. as part of TxDOT's NHHIP project for improved north-south connectivity	0.4
	J	Harvard/Hicks/Diesel/Summer Streets	106	Retrofit as Neighborhood Streets for better access between Bayou and commercial destinations; Reconstruction of Oliver St.	1.0
	K	Shepherd/Durham & Inker/Koehler	107	Reconstruction of Shepherd & Durham south of I-10 along with key Neighborhood Streets to provide safer east-west access	2.5
	L	Michaux Street	107	Retrofit Neighborhood Street improvements for safer north-south connections from Bayou to N Main St.	1.3
	M	N Main Street	108	Reconstruction to accommodate vehicle volumes & improve visibility and safety of crossings and turning movements	1.4
	N	Patterson Street	109	Retrofit as a Neighborhood Street for improved north-south connections from Bayou, over I-10, and toward Washington Ave.	0.6
	O	Rutland Street	109	Retrofit as Neighborhood Street for improved north-south connections from Bayou to 20th St.	1.6
	P	Studemont & Studewood Streets	110	Retrofit improvements to add safe crossings & Shared Use Paths behind the curb where feasible	2.4
	Q	Winter Street Promenade	110	Reconstruction to accommodate a wide Shared Use Path as outlined in TIRZ 13 Mobility Plan	1.3
	R	Little White Oak Bayou Extension	111	New trail with underpass crossings of major barriers and connection to existing Little White Oak trail at Cavalcade St.	3.0
	S	W 19th Street	111	Reconstruction to include boulevard between Bevis Street and Nicholson Trail, connecting two commercial corridors	1.1
	T	White Oak Drive	112	Retrofit improvements to enhance crossing safety, visibility, and turning movements from Arlington St. to Studewood St.	0.7
		Total Project Mileage			54+

Figure 2. Recommended Projects Table (Source: Team Analysis, 2026)

Together, these projects add new ways to access White Oak Bayou.



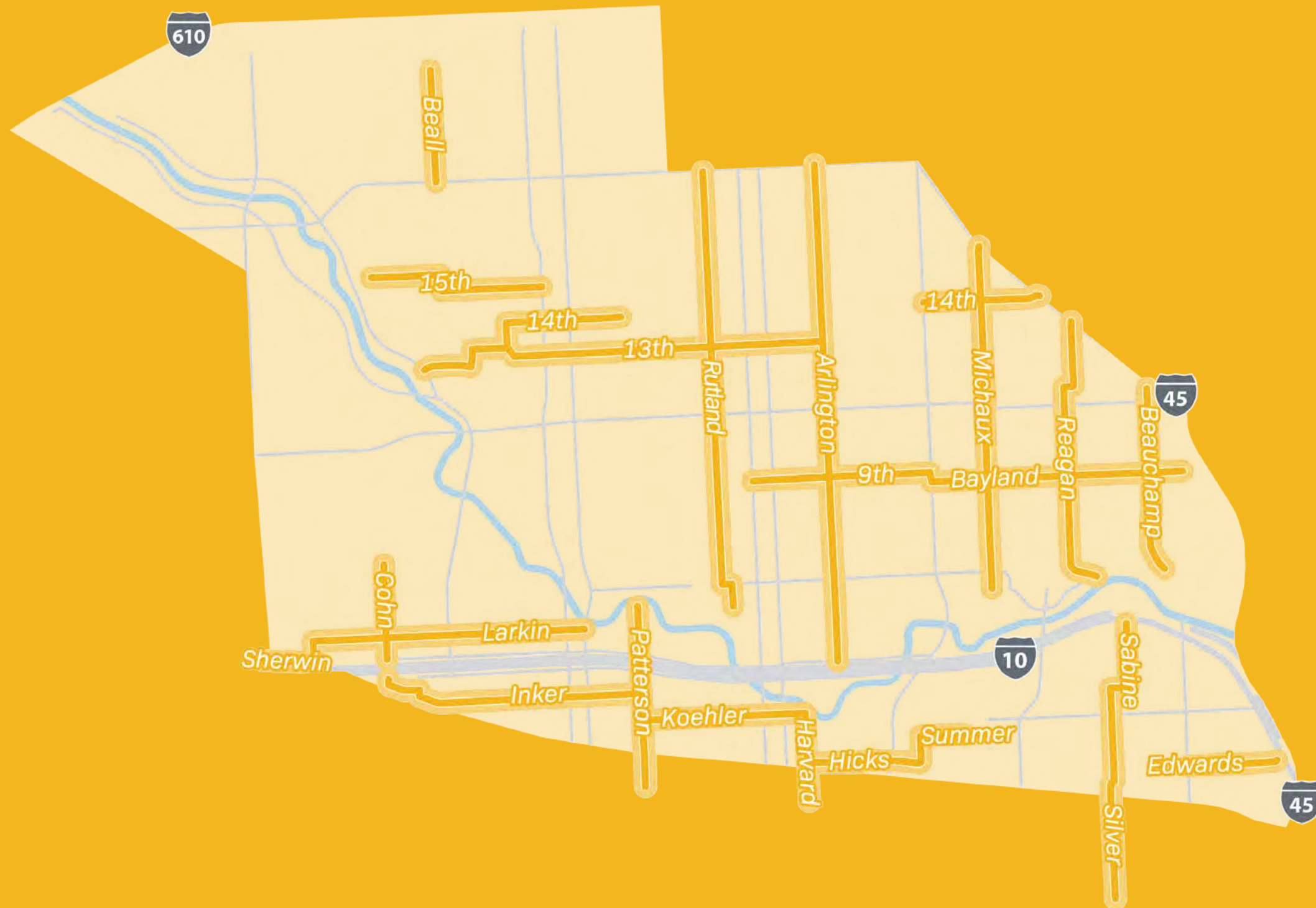
Projects add **more than 36 miles** of stronger neighborhood connections to **White Oak Bayou Greenway**, including 3.2 new miles of Bayou trails.

Together, these projects create new & safer connections across major barriers.



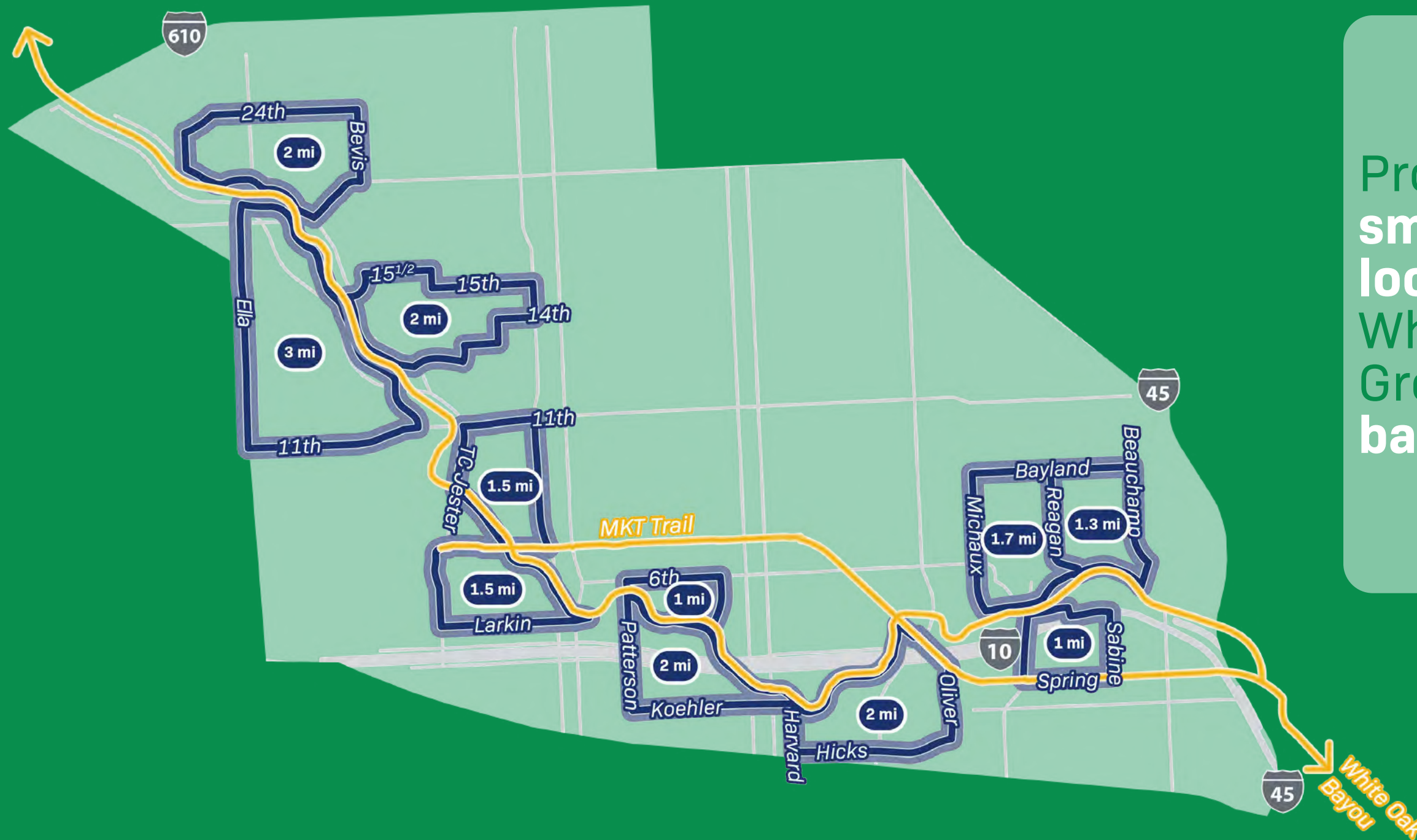
When combined with existing & programmed projects, this plan improves **6 I-10 crossings, 9 Bayou crossings, & adds 5 new Bayou bridges.**

Together, these projects build great local streets to grow up & grow old.



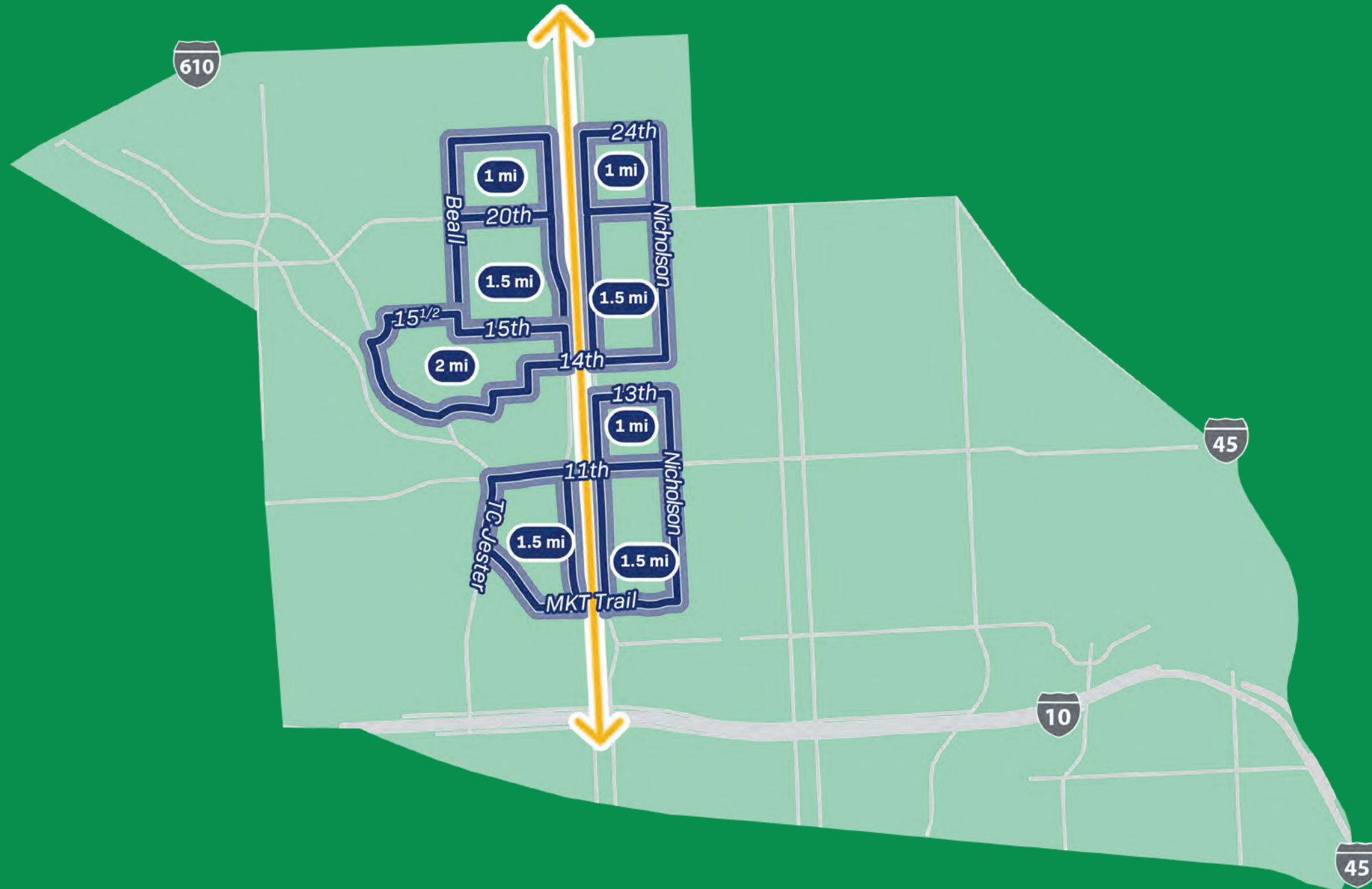
Projects create **more than 18 miles of Neighborhood Streets** where safer local roadways help foster a **sense of place & connection.**

Together, these projects bring the Bayou & local street grid closer together.

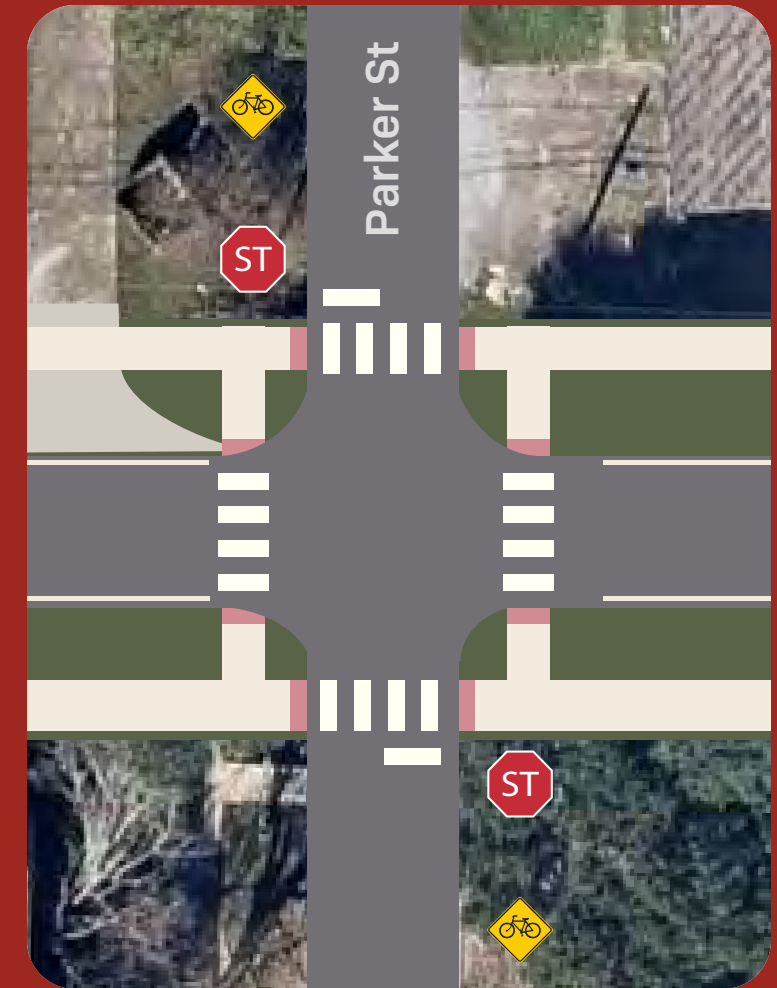


Projects establish small neighborhood loops that make White Oak Bayou Greenway feel like a backyard amenity.

Together, these projects unlock new ways to get to the Shepherd-Durham Corridor.



Projects establish **small neighborhood loops** that redefine how people **stroll & shop** along the Shepherd-Durham Corridor.



Project Profiles



Catalyst Project Profiles

What Is a Catalyst Project?

Catalyst Projects are strategic transportation investments identified for their potential to spur meaningful change and deliver the greatest improvements to community and regional connectivity, particularly for people walking and biking. These projects were selected through a multi-layered evaluation process that

integrated public engagement input, discussions with MHRA board members, and quantitative and qualitative analysis conducted by the project team. The resulting Catalyst Projects represent the highest-impact opportunities to advance a cohesive, connected, and accessible active transportation network.

Many catalyst projects represent larger investments and/or are located well outside of TIRZ 5 boundaries. These projects are ideal candidates for collaboration with MHRA's many infrastructure partners, including the City of Houston.

Due to their size, some projects are phased for implementation of quick-win improvements that bring lower cost safety improvements today, with larger transformational investments in the future. For more information about implementation approach and phasing, see the Implementation Strategy Chapter.

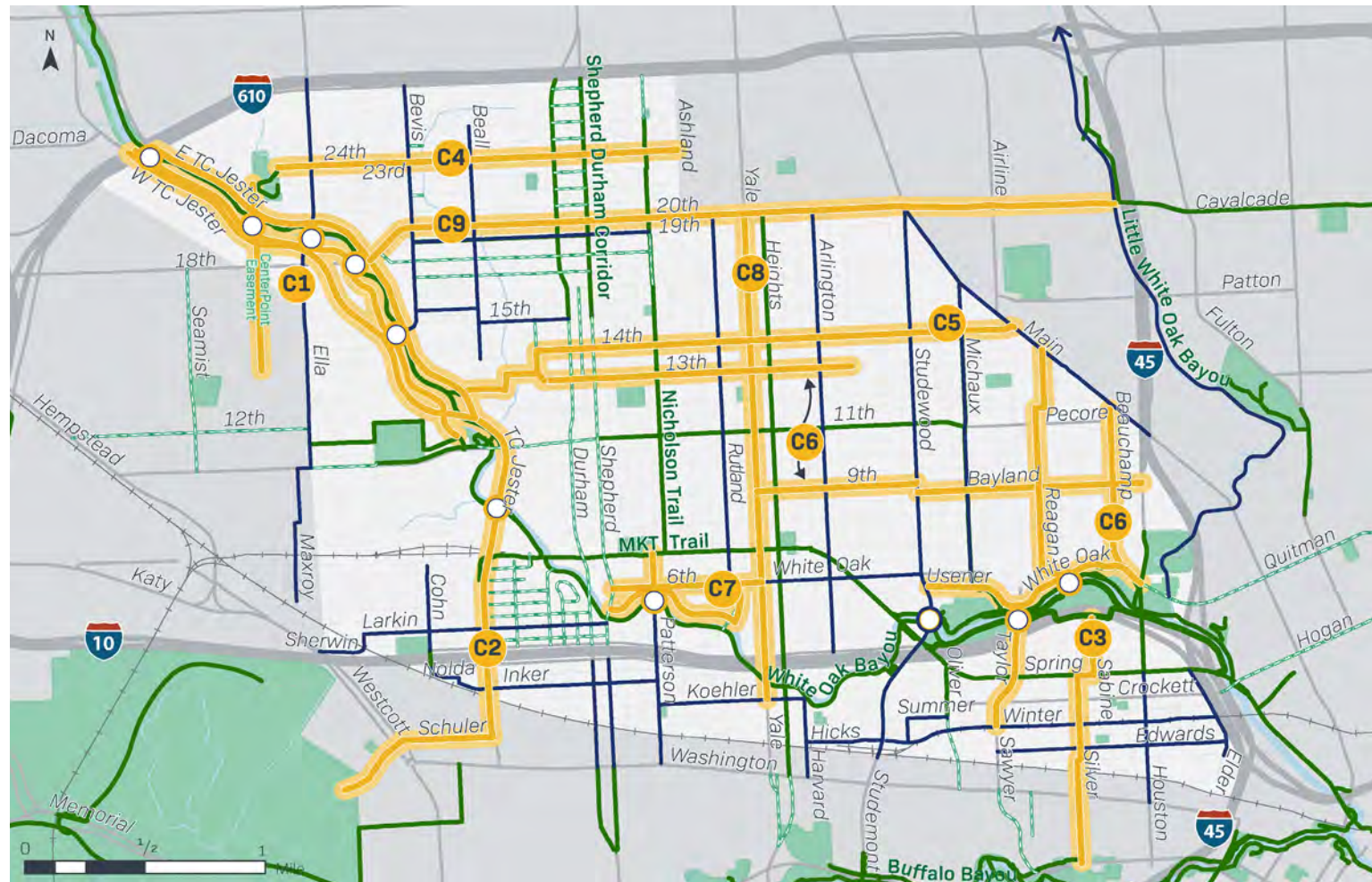
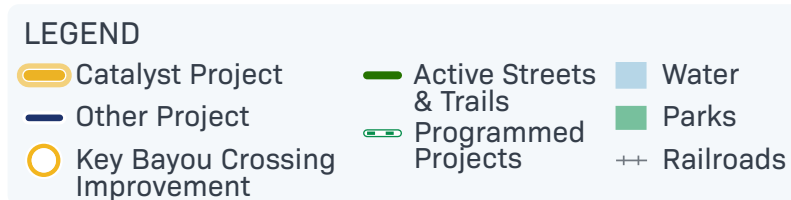


Figure 3. Catalyst Projects (Source: Team Analysis, 2026)



ID	Project Name	Description
C1	White Oak Connectivity Vision	Redefines how the neighborhoods north of 11th St access White Oak Bayou Greenway
C2	White Oak to Memorial Park Connection	Establishes a safe connection from the White Oak Bayou Greenway and MKT Trail to Memorial Park
C3	Heights to Buffalo Bayou Corridor	Creates stronger connections from Woodland Heights, First Ward, & Old Sixth Ward to Stude Park & the White Oak Bayou Greenway
C4	Shady Acres Greenway	Creates a neighborhood shared-use path to serve as a key link between Little Thicket Park, the Shepherd-Durham Corridor, & Nicholson St Trail
C5	14th Street Reconstruction	Formalizes an east-west connection from White Oak Bayou Greenway to N Main St through multiple neighborhoods
C6	Heights School Streets	Redesigns key corridors as safe ways for students to walk & bike to school, specifically for Love, Travis, & Harvard Elementaries, Hogg Middle, & Heights High
C7	MKT Trail/Bird Sanctuary Loop	Strengthen the connectivity along the MKT Trail, the Bayou, and local streets to the Houston Heights Bird Sanctuary, the M-K-T & other local businesses
C8	Yale Street Improvements	Imagines a phased approach to improving the design of Yale St to address one of the Study Area's high-crash corridors
C9	20th Street Reconstruction	Imagines the major commercial corridor with safe crossings & improved amenities for people walking to local shops & destinations

C1 White Oak Connectivity Vision

Extents: I-610 - 11th St

Length: 6.4 mi.

MTFP Class: Major Thoroughfare

Project Description

The White Oak Bayou Greenway is one of the Study Area's greatest assets. It is difficult to access because White Oak Bayou, East TC Jester, and West TC Jester Boulevard are all difficult to cross. As a result, bayou-side communities like Shady Acres, Lazybrook, Timbergrove, and Clark Pines can feel distant from the Bayou.

The White Oak Connectivity Vision project proposes a **set of phased investments that redesign unsafe crossings of East and West TC Jester Boulevard, propose new and improved bridge crossings of the Bayou, and add new shared-use paths** to better connect nearby communities to the greenspace assets along White Oak. These improvements can happen in three interrelated phases:

Three Project Phases

1: Road Crossings & Bridge Feasibility

- Formalize existing desire lines along White Oak Bayou to improve connectivity from the greenway to the local street network
- Enhance safety at key intersections of E & W TC Jester Blvd with targeted crossing improvements
- Study feasibility of adding cantilevered trail bridges on the existing I-610, Ella Boulevard, and W 18th Street bridges
- Study feasibility of building two new bridges across the Bayou near Little Thicket Park and Bevis Street

2: New Greenways & Improved Bridges

- Add new shared-use paths along the west bank of White Oak Bayou from Bryce Street to I-610, along the CenterPoint Energy (CPE) easement from W TC Jester Boulevard to Cindy Lane, and along the CPE from E TC Jester Boulevard to Little Thicket Park
- Where feasible, implement cantilevered trail bridges studied in Phase 1

3: New Bridges & Roadway Redesign

- Construct two new trail bridges across the Bayou in line with the Phase 1 feasibility studies
- Redesign E & W TC Jester Boulevard to encourage safe speeds and improve crossing safety at all intersections. This may include a full redesign and reconstruction of the intersections with Ella Boulevard, W 18th Street, and W 20th Street.

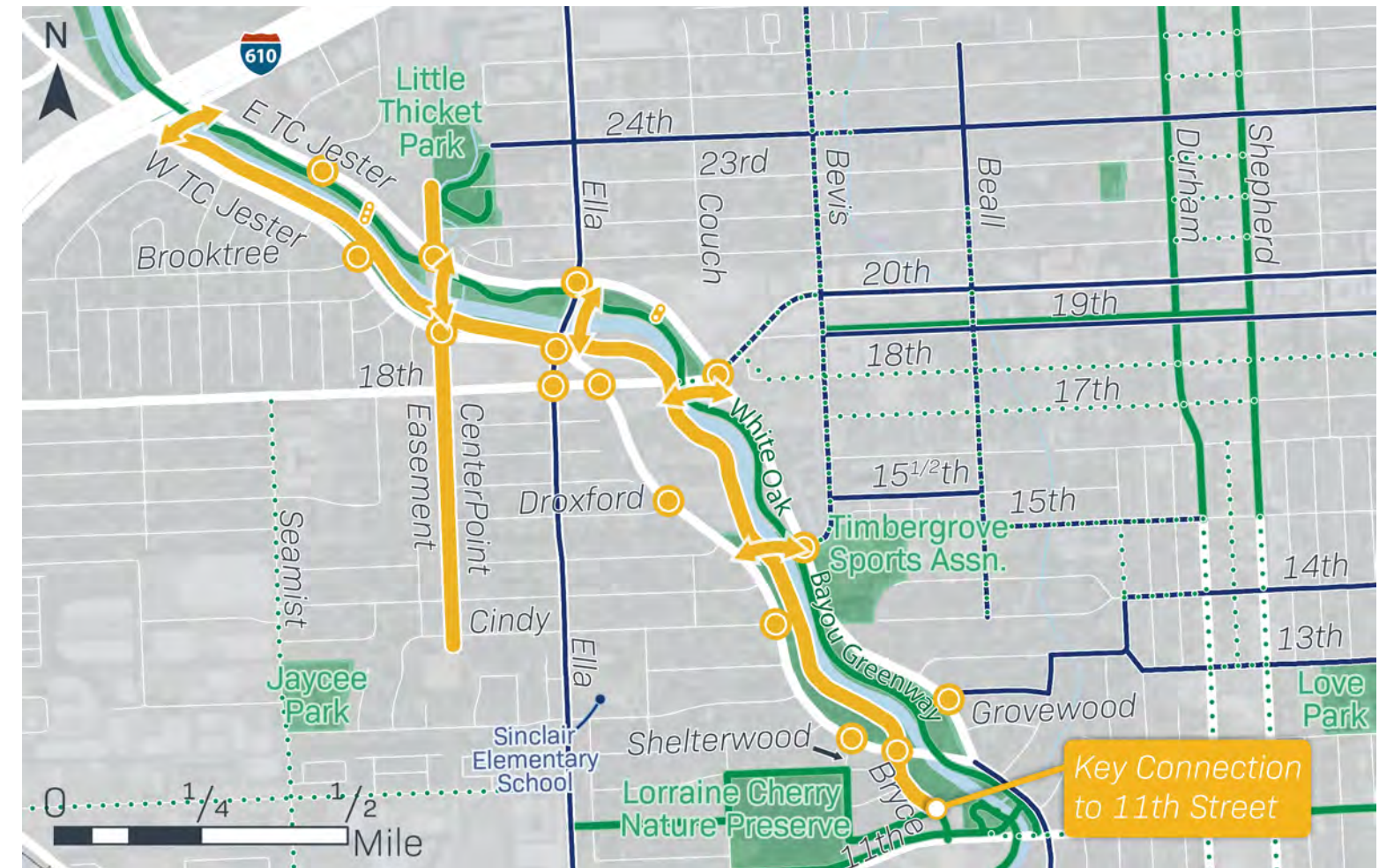


Figure 4. White Oak Connectivity Vision Project (Source: Team Analysis, 2026)

Destinations

- Little Thicket Park
- White Oak Bayou Greenway
- Timbergrove Sports Association
- Lorraine Cherry Nature Preserve
- MKT Trail

Details

- TC Jester Blvd ROW: 120'
- TC Jester Blvd pavement width: 80' - 95'

LEGEND

Recommendation

- Project (Highlighted)
- Intersection/Crossing
- Bridge
- Formalized Desire Line
- Project (Background)

Map

- Programmed Project
- Bikeways and Trails
- Railroads
- Schools
- Water
- Parks and Greenspace

C1 White Oak Connectivity Vision



Figure 5. White Oak Connectivity Vision Sample Intersection (Source: Team Analysis, 2026)

E & W TC Jester Blvd Safe Crossings

Speed and volume data for E & W TC Jester Boulevard shows that these corridors have high speeds and relatively low volumes through much of the Study Area, making them uncomfortable and dangerous to cross. Phase 1 of the White Oak Connectivity Vision project includes a **redesign of key local street intersections along both E & W TC Jester Boulevard to improve safety for people walking, biking, and rolling to get across these major roadways to the White Oak Bayou Greenway.**

W TC Jester Blvd Intersections:

- Millwood Drive
- Brooktree Drive (shown in Figure 5)
- Lazybrook Drive
- Crossing east of CPE easement
- Droxford Drive
- Cindy Lane
- Grovewood Lane
- Shelterwood Drive
- Bryce Street

E TC Jester Blvd Intersections:

- Pinegate Drive
- Hackett Drive
- Crossings near CPE easement (for future connection to Little Thicket Park)
- Bevis Street (under design by TIRZ 12)
- Wynnwood Lane
- Grovewood Lane (enhance existing crossing infrastructure)
- Select areas with desire paths

When implemented with improvements to major crossings at Ella Boulevard, W 18th Street, and W 20th Street, MHRA will be able to reduce the distance between safe crossings along E TC Jester Boulevard from 3,290 feet to 759 feet and along W TC Jester Boulevard from 3,343 feet to 771 feet.

Recommended Improvements

Phase 1 crossing improvements should include **high-visibility crosswalks across both the major and local roadway, improved directional curb ramps, repairs to sidewalks near the intersection, reduced turning radii at the median opening and local street to encourage safer turning movements by vehicles, a reconstructed median nose to allow for a pedestrian refuge in the median, painted edge lines along the major roadway, and direct shared-use path connections to existing or proposed future trail infrastructure along White Oak Bayou.** Improvements should also consider opportunities to improve visibility of people walking and biking through pedestrian-scale lighting, signage, and other strategies.

Figure 5 exemplifies these improvements at the intersection of W TC Jester Boulevard & Brooktree Drive.

Implementation Considerations

Implementation of Phase 1 crossing improvements should pay close attention to reducing turning conflicts and encouraging vehicles to drive at or below the existing speed limit to improve safety for people walking and biking to and across the Bayou.

C1 White Oak Connectivity Vision



Figure 6. Ella Blvd/TC Jester Blvd/W 18th St Intersection Redesign (Source: Team Analysis, 2026)

Roadway Redesign

Phase 3 of the White Oak Connectivity Vision includes a **retrofit of the E & W TC Jester Boulevard corridors and a reconstruction of major intersections that currently have a high rate of crashes**, including the E TC Jester Boulevard intersections with Ella Boulevard and W 20th Street as well as the W TC Jester Boulevard Intersections with Ella Boulevard and W 18th Street. Some of these existing intersections do not include basic safety infrastructure for people walking and biking, such as pedestrian signals.

Figure 6 illustrates recommended improvements at four of these intersections to improve safety and connectivity at a high-traffic commercial location along the Bayou.

Improvements Include:

- New pedestrian signals, curb ramps, and improved sidewalks connecting to all intersections
- If ROW allows, add a 10-foot shared use path on both sides of Ella Boulevard from the bridge to W 18th Street
- Right-size turning radii at all intersections to allow people to make safer turning movements and reduce the potential conflict between people driving, walking, and biking by decreasing the crossing distances at intersections
- Improve visibility of all roadway users through high-visibility crosswalks, improved lighting, and leading pedestrian intervals at intersections
- Addition of a cantilevered trail bridge on the downstream side of the Ella Boulevard and W 18th Street bridges, with direct ties into current and new trail connections on both sides of the Bayou
- Improved transit stops for the 26 Long Point/Cavalcade and 30 Clinton/Ella METRO bus routes, including optimizing stop locations for faster service

Implementation Considerations

Implementation of Phase 3 will require crossing safety analyses and mobility studies to better appropriate right-of-way space to all users in a reconstruction. Coordination with Harris County Flood Control District will be critical to determine appropriate studies and guidelines for new bridges over the floodway.

② White Oak to Memorial Park Connection

Extents: TC Jester - Memorial Park

Length: 2 mi.

MTFP Class: Major Thoroughfare, Local

Project Description

The White Oak to Memorial Park Connection will establish a safer connection for people walking, biking, and rolling from the Heights to Memorial Park, a consistent request by community members throughout community engagement for the plan. The project focuses on improving safe, comfortable north-south travel along TC Jester Boulevard, particularly at crossings of White Oak Bayou and at major intersections such as Larkin Street. While TC Jester Boulevard provides a direct route between key destinations, its current design leads to high vehicle speeds without frequent, safe crossings for people walking and biking.

This project proposes targeted **roadway and intersection improvements to improve crossings of TC Jester Boulevard and add a shared-use path or wide sidewalk behind the curb** to strengthen access to the White Oak Bayou Greenway. The project includes a **retrofit of Schuler Street to build upon the soon-to-be improved crossings at Westcott Street and Washington Avenue.**

The Schuler Street connection will allow for a safer option to access Memorial Park. The project would also include the potential for a **cantilevered bridge on the downstream side of TC Jester Boulevard** over White Oak Bayou.

Improvements focus on shortening crossing distances, improving visibility, adding pedestrian refuge, and creating continuous, accessible sidewalks and shared use paths. Updated pavement striping helps align vehicle speeds with the street's context and improves safety for people crossing. Bridge crossings over White Oak Bayou and the UPRR freight rail line limit available space and will require special design treatments, such as physical separation between sidewalks and travel lanes.

Together, these changes reinforce TC Jester Boulevard's role as a People-First corridor, improving how neighborhoods connect to the Bayou and nearby parks.

Destinations

- White Oak Bayou Greenway
- MKT Trail and adjacent commercial destinations
- Memorial Park
- Memorial Elementary School
- Lawrence Park, Cottage Grove Park, and Love Park

Details

- TC Jester Blvd ROW: 100' - 120'
- TC Jester Blvd pavement width: 70' - 85'
- Schuler St ROW: 45'
- Schuler St pavement width: 17' - 30'

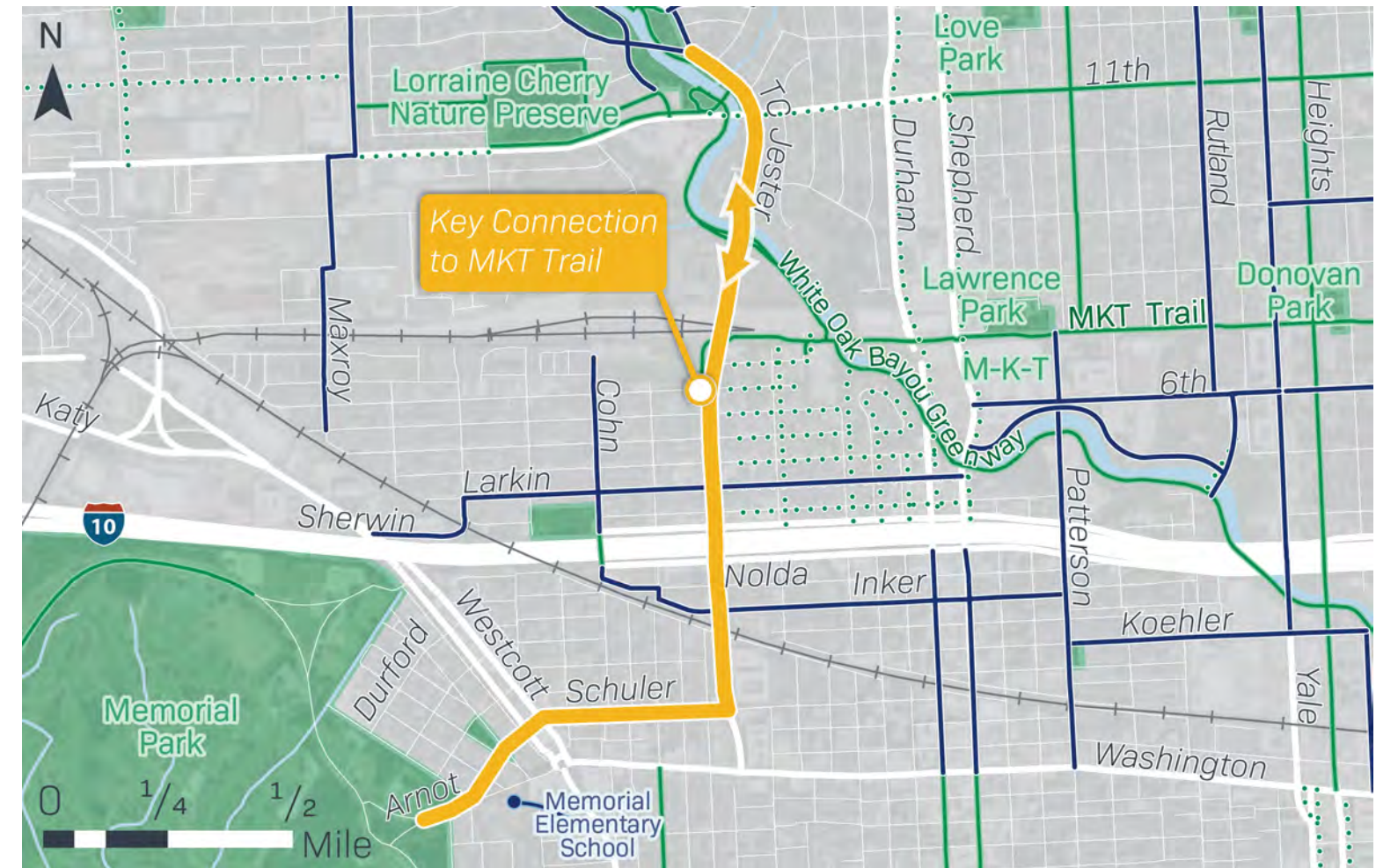
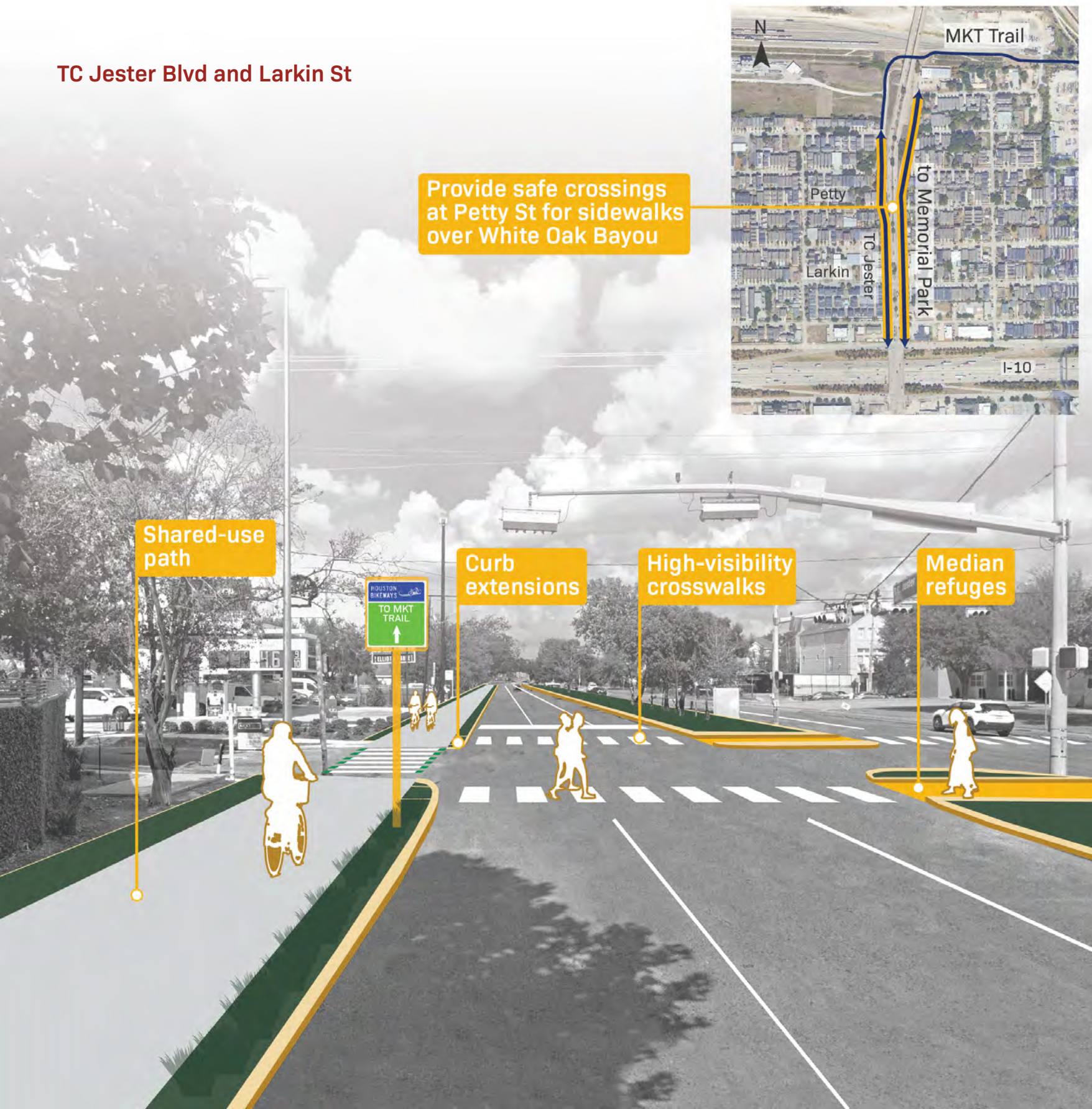


Figure 7. White Oak to Memorial Park Connection Project (Source: Team Analysis, 2026)



② White Oak to Memorial Park Connection

TC Jester Blvd and Larkin St



Project Description

Figure 8 illustrates a typical intersection along TC Jester Boulevard, showing how design changes can improve safety and comfort for people walking, biking, and rolling. The illustration also highlights a **redesigned crossing at Larkin Street with high-visibility crosswalks, reduced turning radii to slow vehicle turns, and median refuge islands** that allow people to cross in shorter, safer stages.

The visual also shows **widened sidewalks behind the curb separated from traffic, improved curb ramps aligned with crossings, and clear bicycle crossings through the intersection. These features reduce conflicts between people driving, walking, and biking and improve access to the White Oak Bayou Greenway** on both sides of the street. Improvements near Petty Street strengthen the corridor’s role as a gateway between TC Jester Boulevard and the MKT Trail, improving wayfinding and access between neighborhood streets and regional trails.

By clearly defining travel lanes and tightening corners, the design encourages safer vehicle speeds and improves sight lines at intersections. The result is an intersection that feels more comfortable to cross, better supports access to trails and parks, and reflects the plan’s goals of improving safety, closing sidewalk gaps, and creating streets that serve as community connectors rather than barriers.

Implementation Considerations

- Bridge crossings and complex intersections along TC Jester Boulevard include space constraints, slip lanes, and wide median openings that will require targeted design treatments to safely connect sidewalks, trails, and bridge approaches.
- Intersection improvements should be coordinated with existing and programmed projects along TC Jester Boulevard and nearby corridors.
- Crossing improvements at Larkin Street should be coordinated with nearby crossings, such as Brooktree Drive, to provide consistent spacing between safe crossing locations.
- Short-term improvements may focus on striping, curb ramp upgrades, and refuge islands, while long-term phases may include sidewalk widening and structural separation at bridge locations.

Figure 8. White Oak to Memorial Park Typical Intersection Redesign (Source: Team Analysis, 2026)

C3 Heights to Buffalo Bayou Corridor

Extents: N Main St - Buffalo Bayou

Length: 3.8 mi.

MTFP Class: Local & Major Collectors

Project Description

Community feedback and the existing conditions analysis underscored a real need for safer, more frequent, and accessible north-south connections across the UPRR freight rail line, I-10, and White Oak Bayou, especially for people walking, biking, and rolling. The Heights to Buffalo Bayou Corridor envisions a **set of corridor, intersection, and bridge projects that would reimagine the way that residents in First Ward, Old Sixth Ward, Woodland Heights, and Houston Heights access the White Oak Bayou Greenway and cross the Bayou and I-10.** This project would build upon the existing momentum of TxDOT's White Oak I-10 Project and its new greenway trail on the Bayou's south bank as well as planned improvements to Stude Park and Little White Oak Bayou.

Destinations

- N Main Street Commercial Corridor
- Stude Park
- White Oak Bayou Greenway
- MKT Trail/Spring Street Trail
- Woodland Park
- Crockett Elementary School
- Arts District Houston
- MECA Houston
- Buffalo Bayou Park & Greenway

Details

- Typical ROW: 50' - 60'
- Typical Pavement width: 20' - 40'

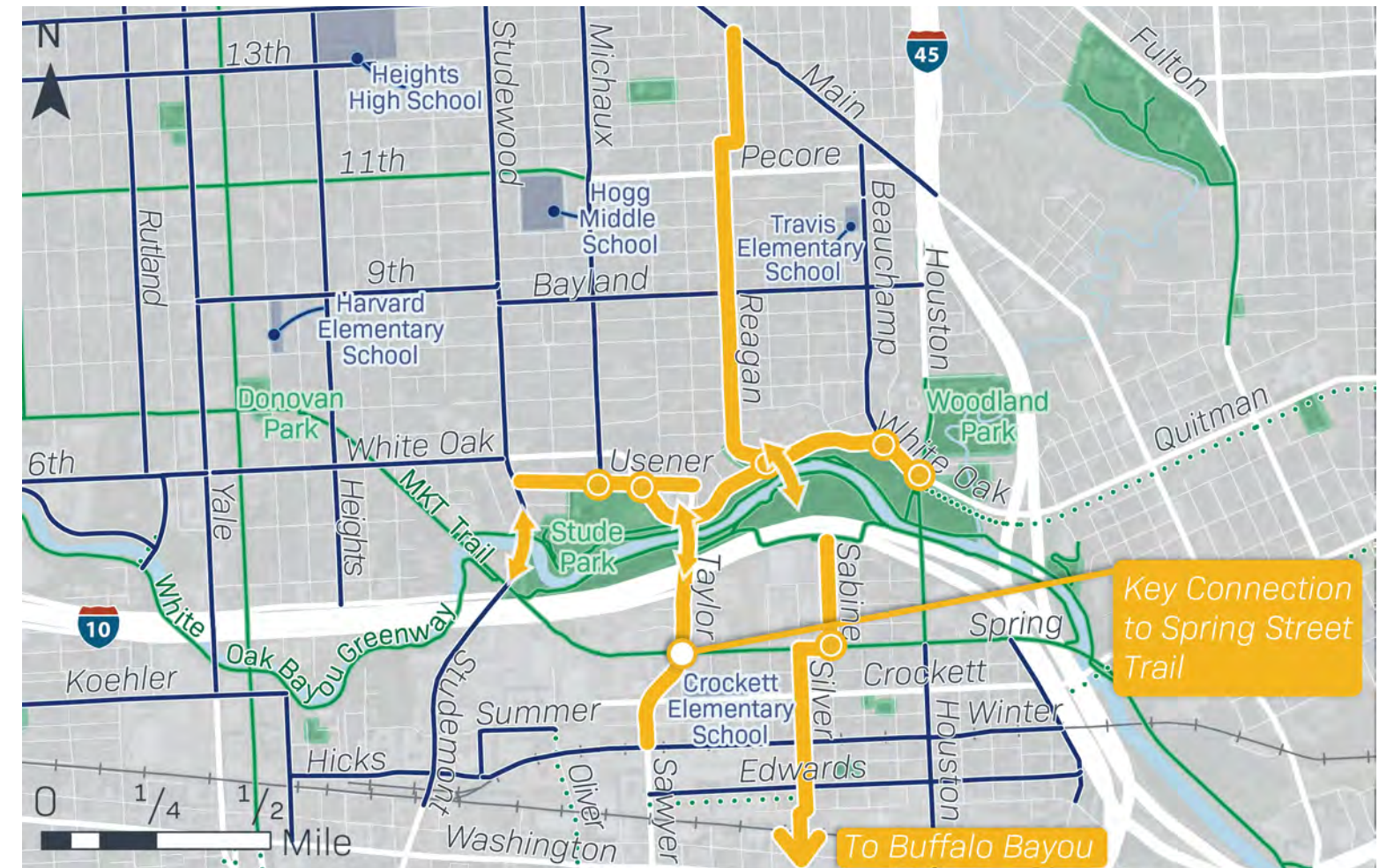


Figure 9. Heights to Buffalo Bayou Corridor Project Map (Source: Team Analysis, 2026)

LEGEND

Recommendation

- Project (Highlighted)
- Intersection/Crossing
- ↗ Bridge
- Project (Background)

Map

- ⋯ Programmed Project
- Bikeways and Trails
- ++ Railroads
- Schools
- Water
- Parks and Greenspace

C3 Heights to Buffalo Bayou Corridor

Key Project Components

Silver St & Sabine St Retrofit - New and improved sidewalks and safe crossings along Silver and Sabine Streets from I-10 to the UPRR rail line will reaffirm these corridors as the key north-south active streets in First Ward, connect to the new trail under I-10, and can ultimately provide a safe connection to Buffalo Bayou Park.

Neighborhood Street Treatments on Reagan St - As a north-south link from N Main Street to Ley Plaza Park and the Bayou, Reagan, Gladys, and Moss Streets are great candidates for Neighborhood Street improvements to connect to multiple churches, Travis Elementary Street, and Proctor Plaza Park.

White Oak Dr & Usener St Redesign - Investments in White Oak Drive and Usener Street would align with recent upgrades to the Bayou Greenway and planned improvements to Stude Park. This project would establish safe crossings to the park and Bayou with roadway retrofits that encourage drivers to use the posted speed along White Oak Drive. This includes tightening the turning radii at intersections along White Oak Drive to help drivers better see oncoming traffic at Gladys, Beauchamp, and Sabine Streets. This project would include a safe crossing for people from Stude Park to Michaux Street.

New & Improved Bridge Crossings - Nearly 1.2 miles between the MKT Bridge and Houston Avenue bridge leaves people walking and biking with few and infrequent options to travel safely across I-10 and the Bayou. This project proposes (1) constructing a new trail bridge in Stude Park or White Oak Park for greenway users to cross the Bayou, (2) studying the feasibility of cantilevered trail bridges on the downstream side of the Studewood Street bridge and the Taylor Street bridges, and (3) Constructing a shared-use path along Taylor Street from I-10 to Winter Street to add options for additional connectivity for the First Ward neighborhood.

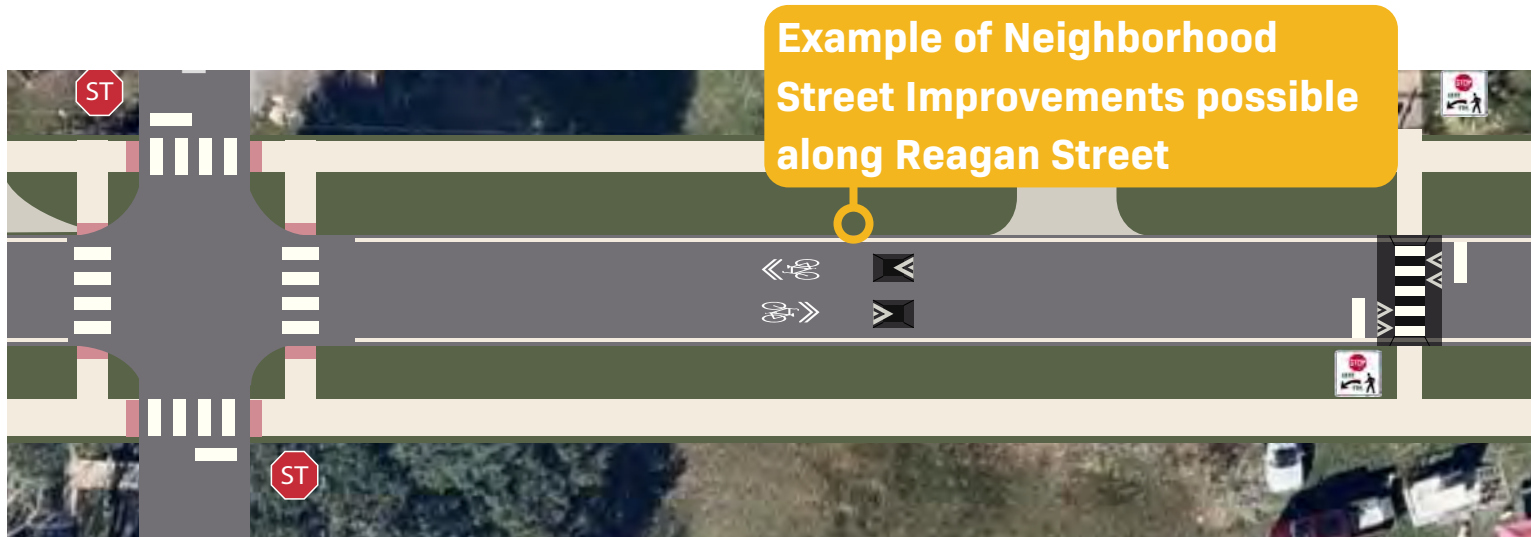


Figure 10. Neighborhood Street Improvements Example (Source: Team Analysis, 2026)

Implementation Considerations

Coordination with TxDOT regarding both the I-10 White Oak Bayou Project and its future shared use paths along White Oak Bayou is critical to implementation of this project. This project intends to leverage the new trails and new connection to White Oak Bayou via Sabine Street in Old Sixth Ward.

Retrofit Neighborhood Street improvements should follow the guidance from the Neighborhood Streets section of this document. Figure 11 below demonstrates potential interventions in a retrofit project on a street with open-ditch drainage.



Figure 11. Gladys St at White Oak Dr Intersection Realignment (Source: Team Analysis, 2026)

c4 Shady Acres Greenway

Extents: Little Thicket Park - Ashland

Length: 1.7 mi.

MTFP Class: Minor Collector

Project Description

W 24th Street serves as a key east-west connection for the Shady Acres neighborhood. Proposed investments will reimagine this corridor as the Shady Acres Greenway, **a comfortable, shaded shared-use path for families to push a stroller or walk to Ella Boulevard, the Shepherd-Durham Corridor, and Nicholson Street.** The project will enhance corridor safety by improving pedestrian access and reducing conflict points at intersections and driveways. The Shady Acres Greenway will serve as a primary active street with safe and direct connections to Little Thicket Park, Helms Elementary, and key neighborhood destinations. These same recommendations may also apply to W 22nd Street or W 23rd Street if it is determined that these corridors better serve Shady Acres and Little Thicket Park.

Proposed improvements prioritize:

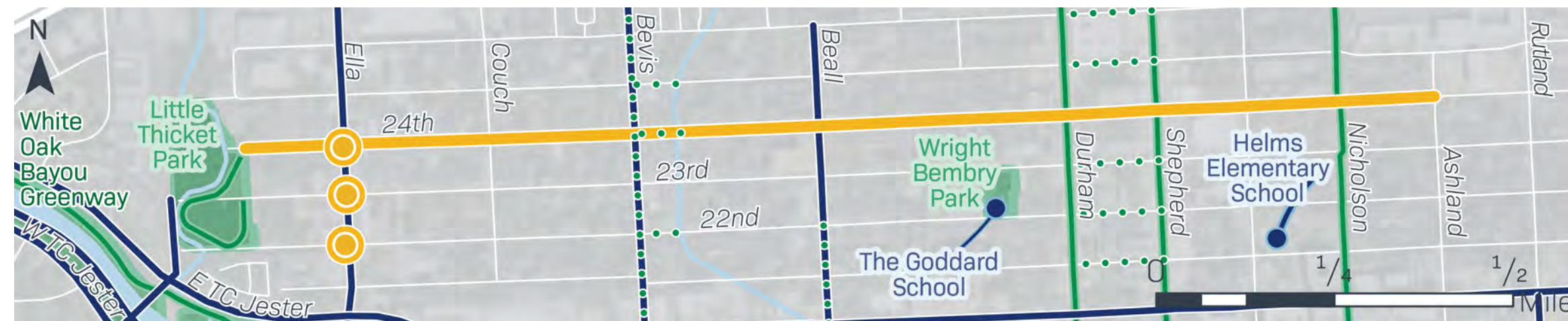
- Conversion of open ditch drainage to curb-and-gutter drainage
- Installation of speed cushions at regular intervals
- Addition of a 10-12' shared use path on one side of the street and a consistent 6' sidewalk on the other side, equaling 1.4 miles of new sidewalks
- Installation of street trees and pedestrian lighting
- High-visibility crosswalks at all intersections
- Curb extensions at key intersections

Destinations

- Little Thicket Park
- N Shepherd Drive and N Durham Drive
- Nicholson Hike and Bike Trail
- HEB
- Turkey Gully
- The Goddard School
- Helms Elementary School

Details

- W 24th St ROW: 70'
- W 24th St pavement width: 15' at park, 26' typical



LEGEND

Recommendation

- Project (Highlighted)
- Intersection/Crossing
- Project (Background)

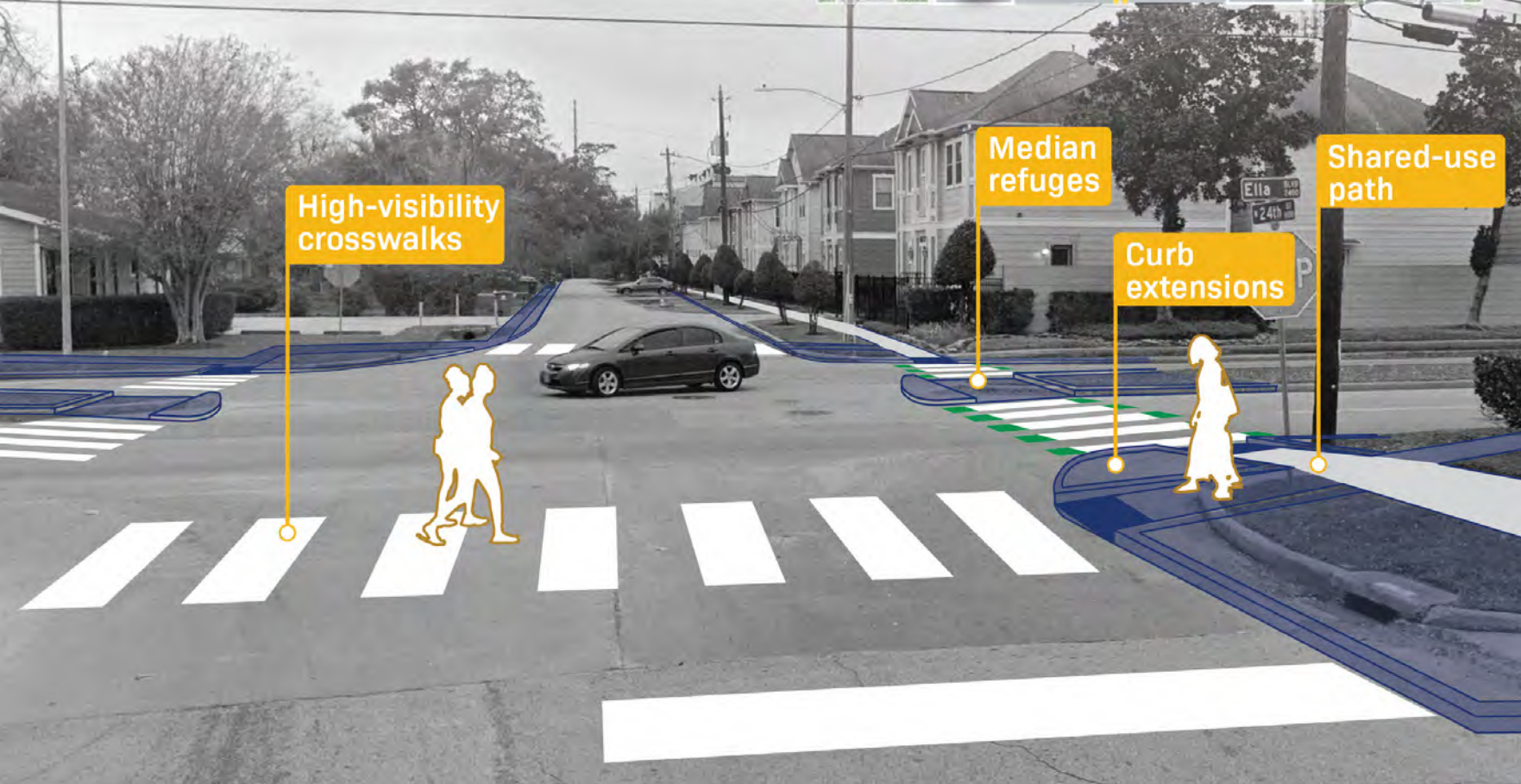
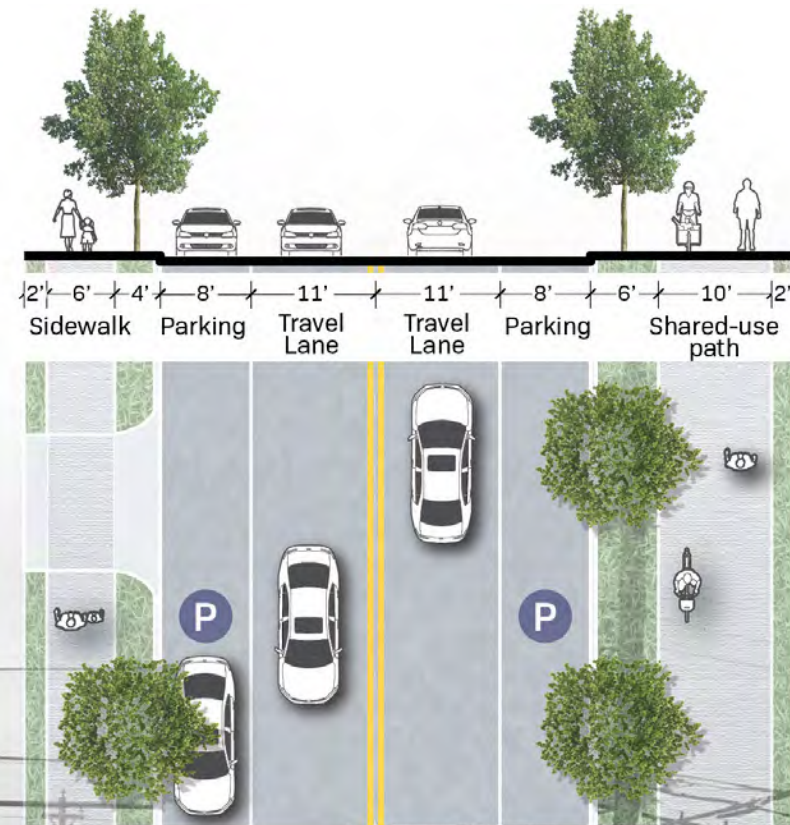
Map

- Programmed Project
- Bikeways and Trails
- Railroads
- Schools
- Water
- Parks and Greenspace

Figure 12. Shady Acres Greenway Project Map (Source: Team Analysis, 2026)

C4 Shady Acres Greenway

W 24th St and Ella Blvd



Project Description

The project visual illustrates recommended intersection improvements at W 24th Street and Ella Boulevard. The illustration highlights a **redesigned crossing at Ella Boulevard with high-visibility crosswalks, curb extensions to slow vehicle turning movements, and median refuge islands that allow people to cross in shorter, safer stages.** These improvements are representative of typical intersection improvements along the corridor, reflecting how design changes can improve safety and comfort for people walking, biking, and rolling.

The redesigned intersection visualization also shows **widened sidewalks, a shared use path separated from traffic, curb ramps aligned with street crossings, and improvements to pedestrian pathways intersecting with driveways.** These features reduce conflicts between users and modes, improve sight lines at intersections, and improve access to Little Thicket Park – reflecting a new W 24th Street corridor that is reflective of the plan’s goals of creating people-first streets that serve the needs of the community. Design of the project should include a signal warrant analysis for W 22nd, W 23rd Street and W 24th Street to determine where a new signal could further solidify safe crossings of Ella Boulevard for neighborhood access to Little Thicket Park.

Implementation Considerations

- Conflict points at commercial and residential driveways, especially at intersections, will need to be addressed to ensure driveways meet City of Houston driveway placement criteria
- Open ditch to curb and gutter conversion will be required to accommodate continuous sidewalks and a shared use path.
- Pavement width reduces significantly west of Ella Boulevard in the last block connecting to Little Thicket Park
- The continuation of the sidewalk and shared use path across this intersection is a priority to provide enhanced and direct connection to Little Thicket Park

Figure 13. Shady Acres Greenway Intersection Example (Source: Team Analysis, 2026)

C5 14th Street Reconstruction

Extents: E TC Jester - N Main St

Length: 2.5 mi.

MTFP Class: Minor Collector

Project Description

The 14th Street Reconstruction Project is intended to improve safety, accessibility, and comfort along one of the Heights’ most important east–west neighborhood corridors with access to the White Oak Bayou Greenway via Wynnwood and Grovewood Streets. While 14th Street provides direct access to schools, trails, and nearby destinations, current conditions vary widely along the corridor. Sidewalk gaps, inconsistent widths, open drainage ditches, and the absence of bicycle facilities make walking and biking uncomfortable in many locations. About 1.4 miles of sidewalk are currently missing.

This project proposes an approach that responds to changes in right-of-way and roadway width along the corridor. **Improvements focus on creating continuous sidewalks, improving crossings at key intersections, encouraging responsible vehicle speeds, and introducing bicycle accommodations that better connect to existing trails and nearby corridors.** Depending on available space, bicycle accommodations may include shared lane markings on narrower segments and dedicated bike lanes where roadway width allows, particularly east of Studewood Street. Open ditch to curb and gutter conversion would occur only in limited segments where needed to close sidewalk gaps or improve accessibility.

Destinations

- Heights High School
- Heights Christian School
- Apostles Houston Church
- Nicholson Street Trail to Heights Hike and Bike Trail
- White Oak Bayou Greenway
- Love Elementary School, Browning Elementary School, and the Heights Neighborhood Library

Details

- Wynnwood Ln/Grovewood Ln ROW: 60’
- Wynnwood Ln/Grovewood Ln pavement width: 25’
- 14th St ROW: 50’ - 70’
- 14th St Pavement width: 20’ - 30’

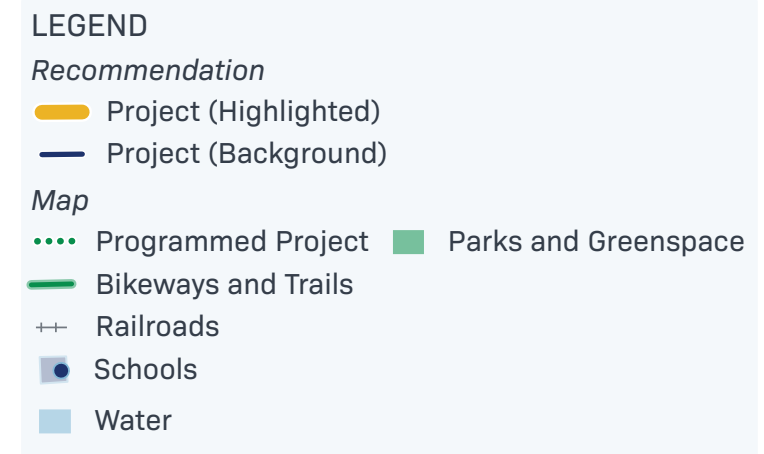
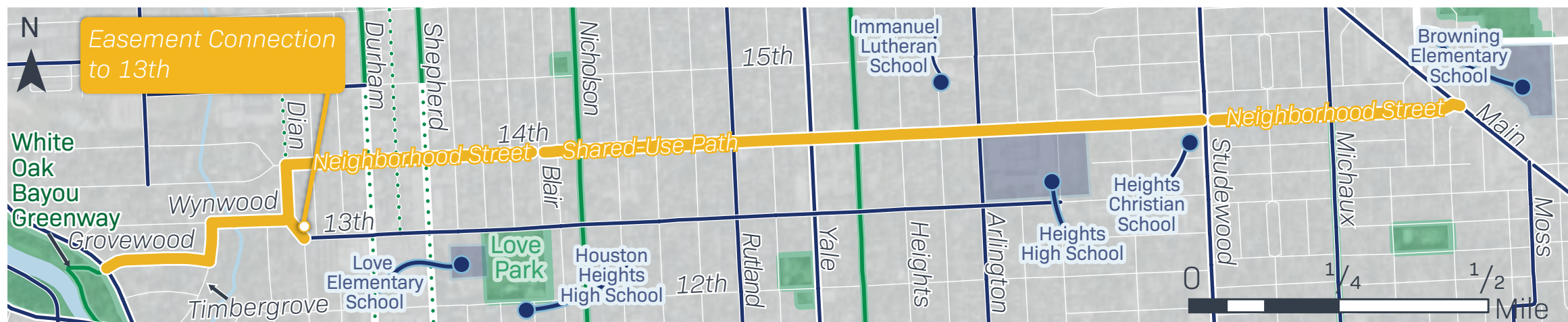
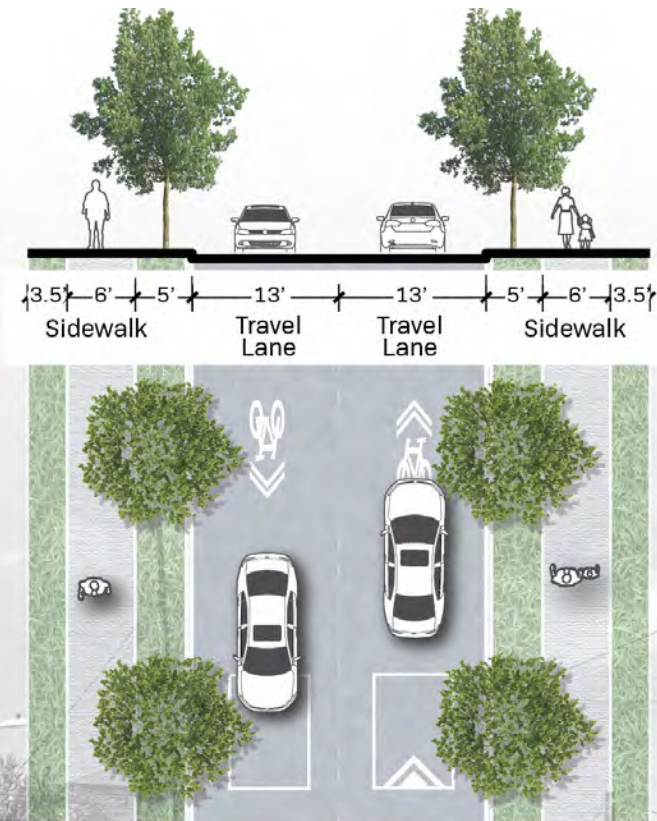


Figure 14. 14th St Reconstruction Project Map (Source: Team Analysis, 2026)

C5 14th Street Reconstruction

14th St - Dian to Blair



Neighborhood Street: Dian Street to Blair Street

The Dian Street to Blair Street segment shows a **Neighborhood Street design (see page 96 for more details on Neighborhood Streets)** that emphasizes responsible vehicle speeds and a **shared roadway for all modes**. The visual includes six-foot sidewalks, speed cushions to slow vehicle speeds, and high-visibility crosswalks at intersections. Sharrows are shown in the travel lanes to reinforce that bicyclists are expected and supported on this street.

Curb extensions are highlighted at intersections to reduce crossing distances and reinforce the residential character of the corridor. Wayfinding signage directs people toward key connections such as the White Oak Bayou Greenway, strengthening connections between neighborhood streets and the regional trail system.

Lighting and visibility: Supplemental pedestrian-scale lighting may be needed to improve nighttime visibility along residential blocks and at intersections.

Implementation Considerations

This portion of the corridor functions primarily as a neighborhood street with constrained right-of-way and frequent residential driveways. Implementation should prioritize pedestrian safety and traffic management while preserving neighborhood access. Key considerations include:

- Sidewalk continuity: Sidewalk gaps and narrow segments should be addressed using standard-width sidewalks where space allows, with targeted widening at key pedestrian desire lines.
- Bicycle accommodations: Due to limited right-of-way, bicycle accommodations are expected to rely on shared-lane treatments and wayfinding rather than dedicated bike lanes.
- Driveway management: Frequent driveways require careful sidewalk design to maintain accessible cross slopes and reduce conflicts between vehicles and pedestrians.



Figure 15. 14th St Reconstruction Project Neighborhood Street Example (Source: Team Analysis, 2026)

C5 14th Street Reconstruction

14th St - Blair to Studewood

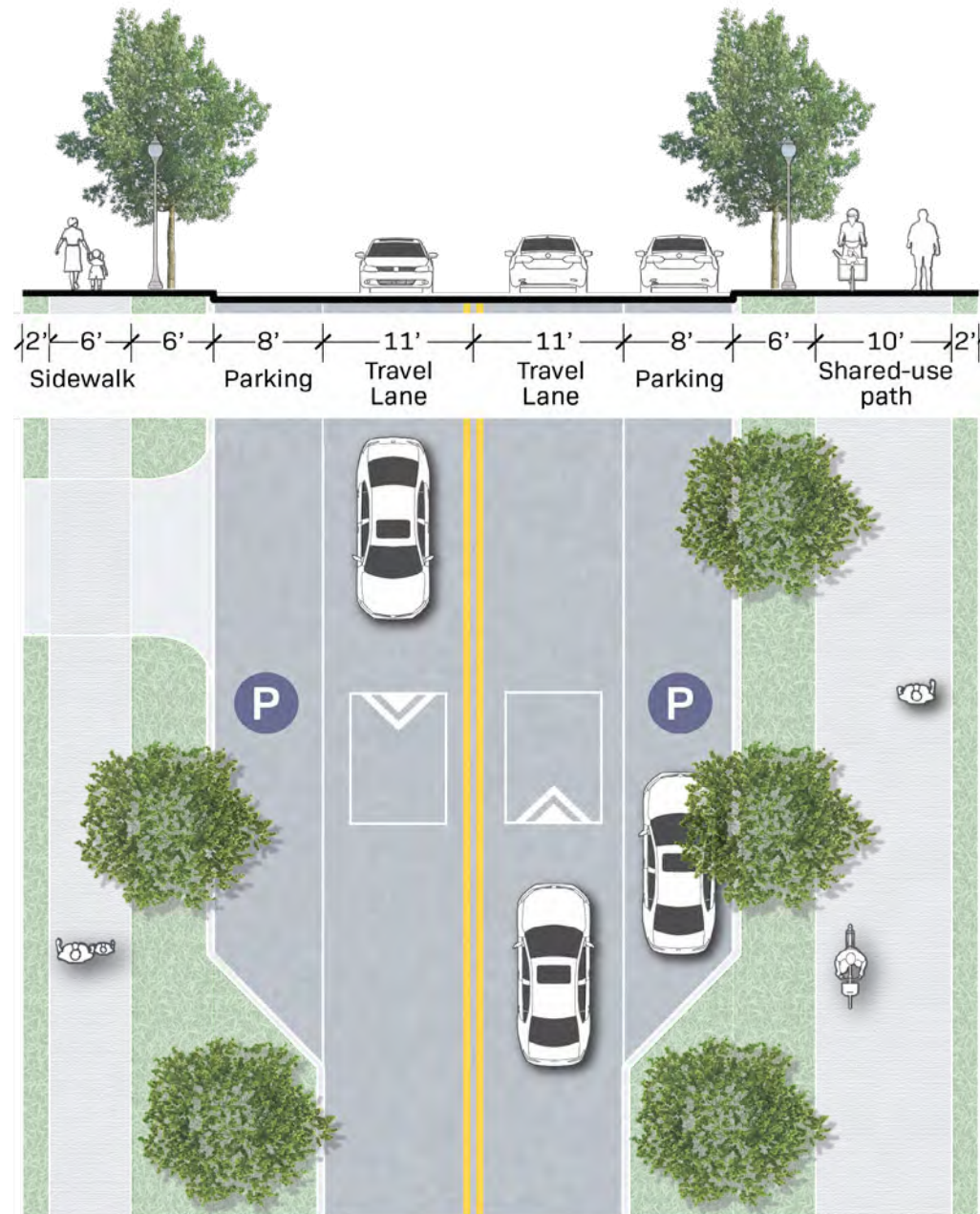


Figure 16. 14th St Reconstruction Shared-Use Path Example (Source: Team Analysis, 2026)

New Shared Use Path Blair to Studewood

The Blair to Studewood segment illustrates a more constrained portion of the corridor where maintaining neighborhood character is a priority. The visual shows a **two-lane street with on-street parking, continuous sidewalks, and street trees** that provide shade and buffer pedestrians from traffic.

High-visibility crosswalks and curb extensions are shown at intersections to shorten crossing distances and improve visibility for both pedestrians and drivers. These treatments help slow turning vehicles and make crossings safer for students walking to nearby schools and residents accessing neighborhood destinations. Where space allows, improvements may include a shared use path on one side of the street paired with a sidewalk on the opposite side to support both walking and biking in a constrained right-of-way.

This segment emphasizes sidewalk continuity, driveway conflict management, and pedestrian comfort, reinforcing 14th Street's role as a neighborhood street rather than a through route. West of Blair Street, the design intentionally emphasizes traffic management and shared use rather than dedicated bike lanes.

Implementation Considerations

This portion of the corridor includes wider right-of-way and serves higher pedestrian and bicycle demand, particularly near schools and trail connections. Implementation should emphasize continuous facilities and safer crossings. Key considerations include:

- **Shared use path opportunities:** Where right-of-way permits, a shared use path on one side of the street paired with a sidewalk on the opposite side can support both walking and biking in a constrained but flexible cross-section.
- **Intersection safety:** Crossings at Blair Street, Studewood Street, and other key intersections should include high-visibility crosswalks, curb extensions, and ADA-compliant ramps to shorten crossing distances and improve visibility.
- **School access:** Improvements should account for peak school arrival and dismissal periods, including enhanced crossings, traffic management, and clear pedestrian routes near schools.
- **Wayfinding and trail connections:** Signage and pavement markings should clearly direct people to the Heights Hike and Bike Trail and White Oak Bayou Greenway, strengthening east-west connectivity.
- **Stormwater and drainage:** Selective conversion of open ditches to curb and gutter may be required to support continuous sidewalks and accessible routes, particularly near trail access points.

Early implementation may focus on crossings, shared use path segments, and sidewalk repairs, with longer-term phases addressing more substantial reconstruction where additional width or drainage improvements are needed.

C6 Heights School Streets

Extents: Prince St - Arlington St

Extents: Yale St - Beauchamp

Extents: Main St - White Oak Dr

Length: 3.6 mi.

MTFP Class: Local

Project Description

Parents provided consistent feedback during the planning process that they want to live in neighborhoods with safe walking and biking access to schools. Schools like Love Elementary, Travis Elementary, and Hogg Middle were regularly mentioned as locations for potential safe street improvements.

The Heights School Streets project reconstructing or retrofitting four key corridors into Neighborhood Streets in the Houston Heights and Woodland Heights neighborhoods: 13th Street (Love ES), E 9th Street (Harvard ES), Bayland Avenue (Travis ES), and Beauchamp Street (Travis ES). All projects would also serve families attending Hogg MS and Heights HS, as well as several nearby private schools.

13th St Improvements (Dian St to Columbia St)

13th Street plays a key role connecting Love Elementary School and Love Park across major corridors in the Heights. This project provides an opportunity to add to MHRA's current investments in safe crossings along 13th Street by improving the intersections at Yale Street and Heights Boulevard. On its western end, a **short connection to Dian Street across an existing utility easement** would formalize a connection of several important green spaces from White Oak Bayou in the west, across the Shepherd-Durham Bikeway and Nicholson Trail to Heights Boulevard in the east. At the eastern end, the project includes a **raised intersection at 13th Street and Columbia Street for better visibility for Heights High School students.**

9th St & Bayland Ave Improvements (Yale St to Houston Ave)

Neighborhood Street improvements across 9th St and Bayland Ave would create a safe and enjoyable east-west corridor serving both Harvard ES and Travis ES. This project would **improve major crossings at Yale Street, Heights Boulevard, Studewood Street with a new mid-block crossing, and Houston Avenue.** This project would also improve access for residents to the Houston Avenue Commercial Corridor.

Beauchamp St Improvements (White Oak Dr to N Main St)

A Neighborhood Street project along Beauchamp Street would create north-south connections for Travis ES and its Spark Park. The project should include **focused improvements to major intersections such as Pecore Street, White Oak Drive, and N Main Street.** The White Oak Drive and N Main Street intersections provide opportunities for reducing the turning radii to shorten the crossing distance for students and people accessing the Bayou.

Details

- 13th St ROW: 50' - 70'
- 9th St ROW: 70'
- 13th St/9th St pavement width: 20' - 30'
- Bayland Ave ROW: 60'
- Beauchamp St ROW: 40' - 60'
- Bayland/Beauchamp St pavement width: 30'

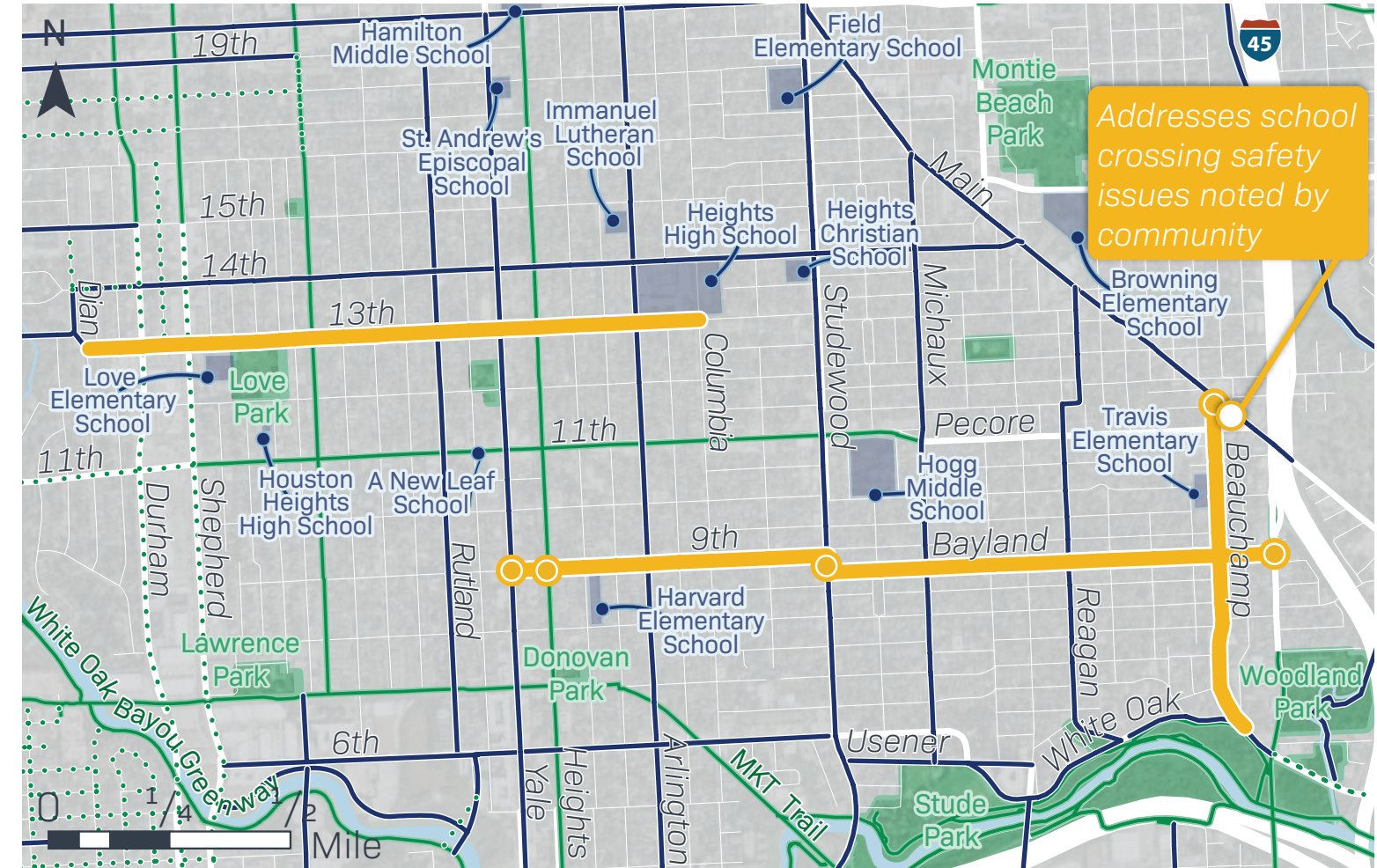


Figure 17. Heights School Streets Project Map (Source: Team Analysis, 2026)

Destinations

- Love Elementary School & Love Park
- Heights High School
- Hogg Middle School
- Harvard Elementary School
- Travis Elementary School
- Stude Park
- White Oak Bayou Greenway

LEGEND

Recommendation

- Project (Highlighted)
- Intersection/Crossing
- Project (Background)

Map

- ⋯ Programmed Project
- Bikeways and Trails
- ++ Railroads
- Water
- Parks and Greenspace
- Schools

C6 Heights School Streets

Design Considerations

Design of Heights School Streets should follow guidance from the **Neighborhood Streets section and its suite of recommendations (explained starting Page 96)**. As these streets serve vulnerable populations, their design should meet the highest level of improvements, described in the Gold Star Neighborhood Street guidelines. The figure below demonstrates an example of what potential Gold Star level improvements would look like on W 13th Street next to Love Elementary and Love Park.

Implementation Considerations

Neighborhood Street improvements will also include **installation of speed cushions along Prince Street within one block of 13th Street due to consistent reports of stop sign running and speeding along this section of Prince Street with blind curves.**

Crossings of Yale Street and Heights Boulevard will require extra attention to ensure the highest level of safety is provided for vulnerable populations, namely schoolchildren, crossing these streets. Coordination with future projects for Yale Street and existing safe infrastructure on Heights Boulevard should be considered.

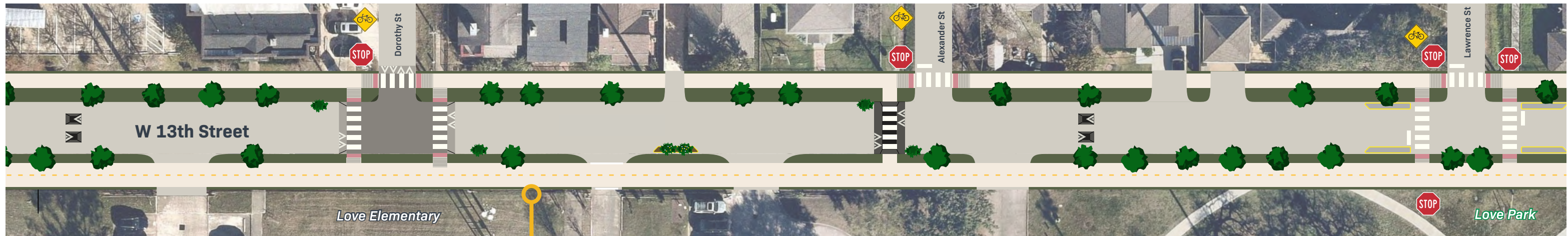


Figure 18. Heights School Streets Example Street (Source: Team Analysis, 2026)

Recommended improvements along 13th Street near Love Elementary School

C7 MKT Bird Sanctuary Loop

Extents: MKT Trail - White Oak Bayou, Shepherd Dr - Yale

MTFP Class: Local Streets & New Trails

Length: 1.7 mi.

Project Description

The M-K-T along the MKT Trail is one of Houston’s great economic development success stories in the last decade. The popularity of the local businesses alongside the MKT Trail and nearby green spaces like Lawrence Park and Nicholson Trail have created a virtuous cycle creating a true sense of space in the heart of the Heights.

New projects, including Phase 2 of the Shepherd-Durham Corridor, the Rutland Bridge over the Bayou, Cottage Grove sidewalk and drainage improvements, and the Swift Building promise to add to the growth and success of this area.

The MKT Bird Sanctuary Loop offers a set of projects that will improve connectivity to and within this growing commercial and recreation district.

New Trail Miles

A new trail on the north bank of White Oak Bayou would connect from Shepherd Drive and the programmed bikeway along that corridor to W 5th Street and the programmed Rutland Bridge that will cross White Oak Bayou in the east. This new trail will create new trailside access points for the W 6th Street Commercial Corridor and access points to the Houston Heights Bird Sanctuary. An added trail on the north bank can also include opportunities for public art and placemaking with views of the Downtown Houston skyline.

W 6th Street Improvements

W 6th Street is currently disconnected at TxDOT’s detention basin (the Houston Heights Bird Sanctuary). This project envisions closing that gap with a **bridge over the detention basin**. This bridge should include significant room for

pedestrians and bicyclists and a viewing platform to enjoy the scenery of the Bird Sanctuary. Additional improvements along W 6th Street should complete the sidewalk network, install pedestrian-scale lighting, and improve crosswalks at Waverly Street and parking lot entrances.

Bird Sanctuary

Local organizations should work with TxDOT to formally **designate their detention basin as a park and formalize the existing desire paths within the detention basin as official walking trails** with added seating, lighting, and other amenities. TxDOT’s Rutland Bridge project will also provide a shared-use path connection along the eastern boundary of the detention basin to connect the bridge with W 5th Street, W 6th Street and the MKT Trail. Additional environmental clearance may be required for any major work along these former industrial sites.

Patterson Bridge & M-K-T Connection

Patterson Street plays a unique role as a local street that connects across I-10, offering an important north-south link from the Heights south to the Washington Avenue corridor and connecting via local streets to Buffalo Bayou Park and ultimately Montrose. This unique quality makes Patterson Street a great candidate for a **bridge connection across White Oak Bayou to continue that north-south connectivity with links to the MKT and Nicholson Trails**. The Loop proposed a new Patterson Street Bridge over the Bayou with an additional new trail connection between the M-K-T and the Swift Building to tie directly into the MKT Trail.

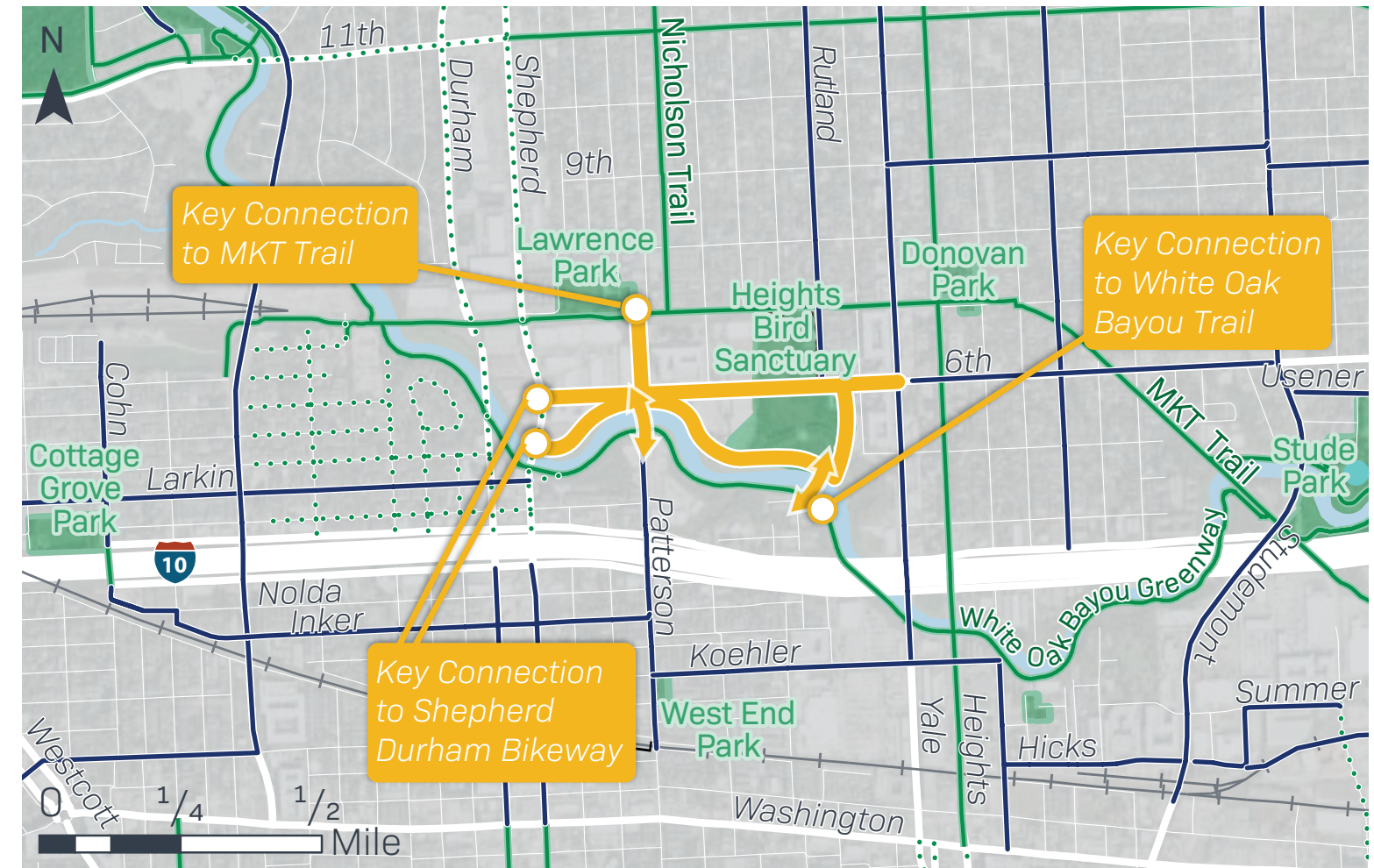


Figure 19. MKT Bird Sanctuary Loop Project Map (Source: Team Analysis, 2026)

Destinations

- M-K-T and The Swift
- Heights Bird Sanctuary
- Nicholson Street Trail
- Lawrence Park
- Stude Park
- White Oak Bayou Greenway

Details

- 6th St ROW: 70'
- 6th St pavement width: 20' - 35'

LEGEND

Recommendation

- Project (Highlighted)
- ↔ Bridge
- Project (Background)

Map

- ⋯ Programmed Project
- Bikeways and Trails
- ++ Railroads
- Schools
- Parks and Greenspace
- Water

C7 MKT Bird Sanctuary Loop



Figure 20. MKT Bird Sanctuary Loop Plan View (Source: Team Analysis, 2026)

- A North Bank Trail & MKT Link**
 Build a new trail on the north bank of White Oak Bayou between 5th St and Shepherd Dr with additional connections to MKT Trail along Shepherd Dr. These new trails will serve existing buildings, the Heights Bird Sanctuary, and the programmed Phase 2 Shepherd-Durham Project.
- B New Bridges**
 Build new bridges across White Oak Bayou at Patterson St and Rutland St to solidify north-south connections, creating safe ways to explore from the M-K-T and Bird Sanctuary to as far as Montrose.
- C W 6th Street Improvements**
 Improve existing crossings and complete sidewalks along W 6th St and build a new bridge to close the existing gap and alleviate congestion along 8th St. The bridge could include pedestrian overlooks of the Bird Sanctuary.
- D Bird Sanctuary Walking Trails**
 Formalize trails within the Bird Sanctuary, with street and trail connections.
- E MKT Trail Connection**
 Extend trail through the M-K-T development to the broader trail network.
- F Wayfinding & Placemaking**
 Install creative wayfinding at several gateways to improve usability of the trail network and M-K-T campus.

C8 Yale Street Improvements

Extents: 20th - Koehler

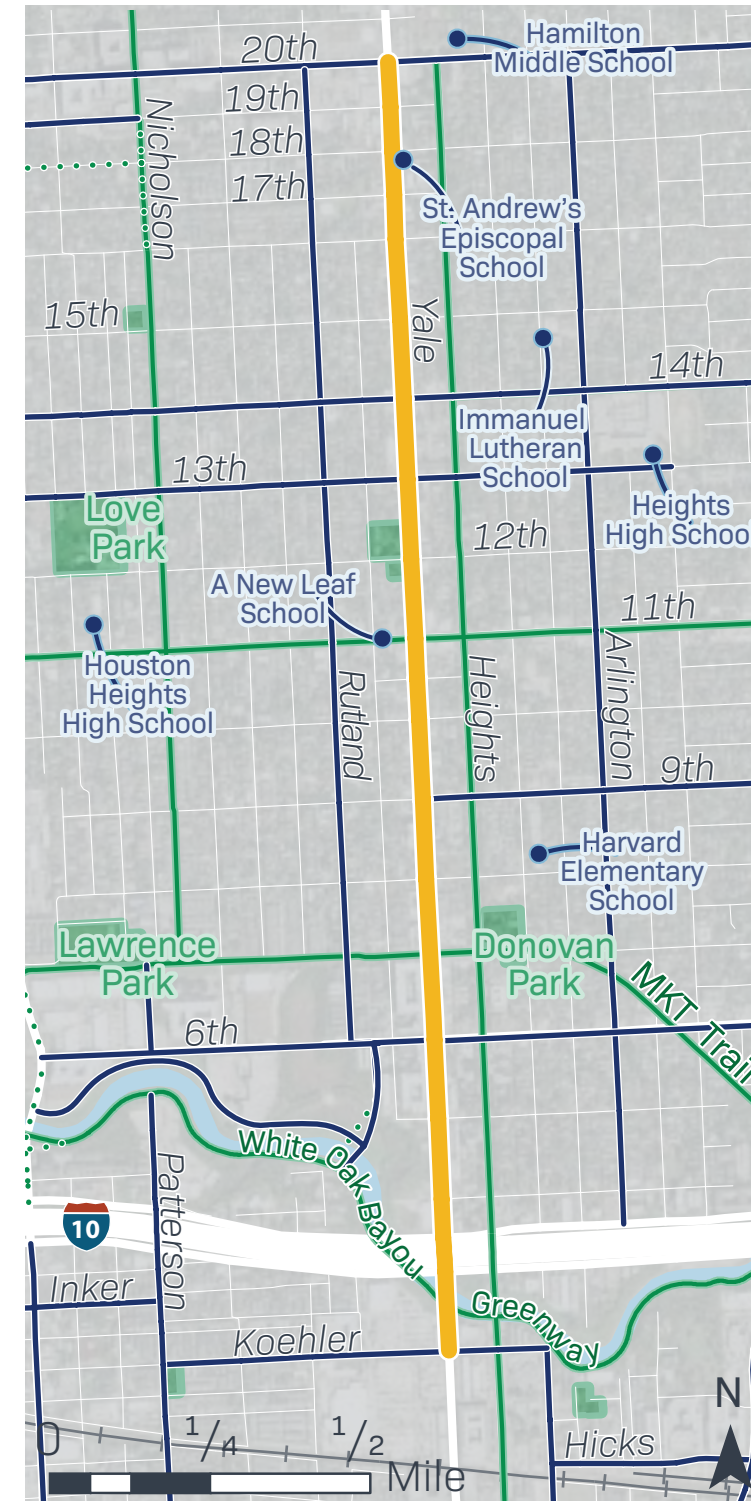
Length: 2 mi.

MTFP Class: Major Thoroughfare

Project Description

Yale Street is one of the most important north/south corridors in Greater Heights, but its current design creates challenges for people walking, biking, and accessing nearby trails. Long crossing distances, wide turning movements, and limited waiting space at intersections make it uncomfortable to cross, particularly where the White Oak Bayou Trail and neighborhood routes intersect the corridor. Additionally, the corridor is missing around 0.3 miles of sidewalks.

This project proposes a combination of near-term safety improvements and longer-term reconstruction concepts to improve crossings, reduce conflicts, and create a more welcoming public realm along Yale Street. Together, these strategies improve safety while setting a clear vision for future investment along this key corridor.



Destinations

- Hamilton Middle School
- Harvard Elementary School
- St. Andrew's Episcopal School
- A New Leaf School
- Milroy Park
- Connection to White Oak Bayou Trail
- Connection to MKT Trail
- Connection to dedicated bike lanes on 11th Street

Details

- Yale St ROW: 20th St - 11th St: 60' - 70'; 11th St - White Oak Dr: 70' - 75'; White Oak Dr - Koehler St: 80'
- Yale St pavement width: 42'

LEGEND

Recommendation

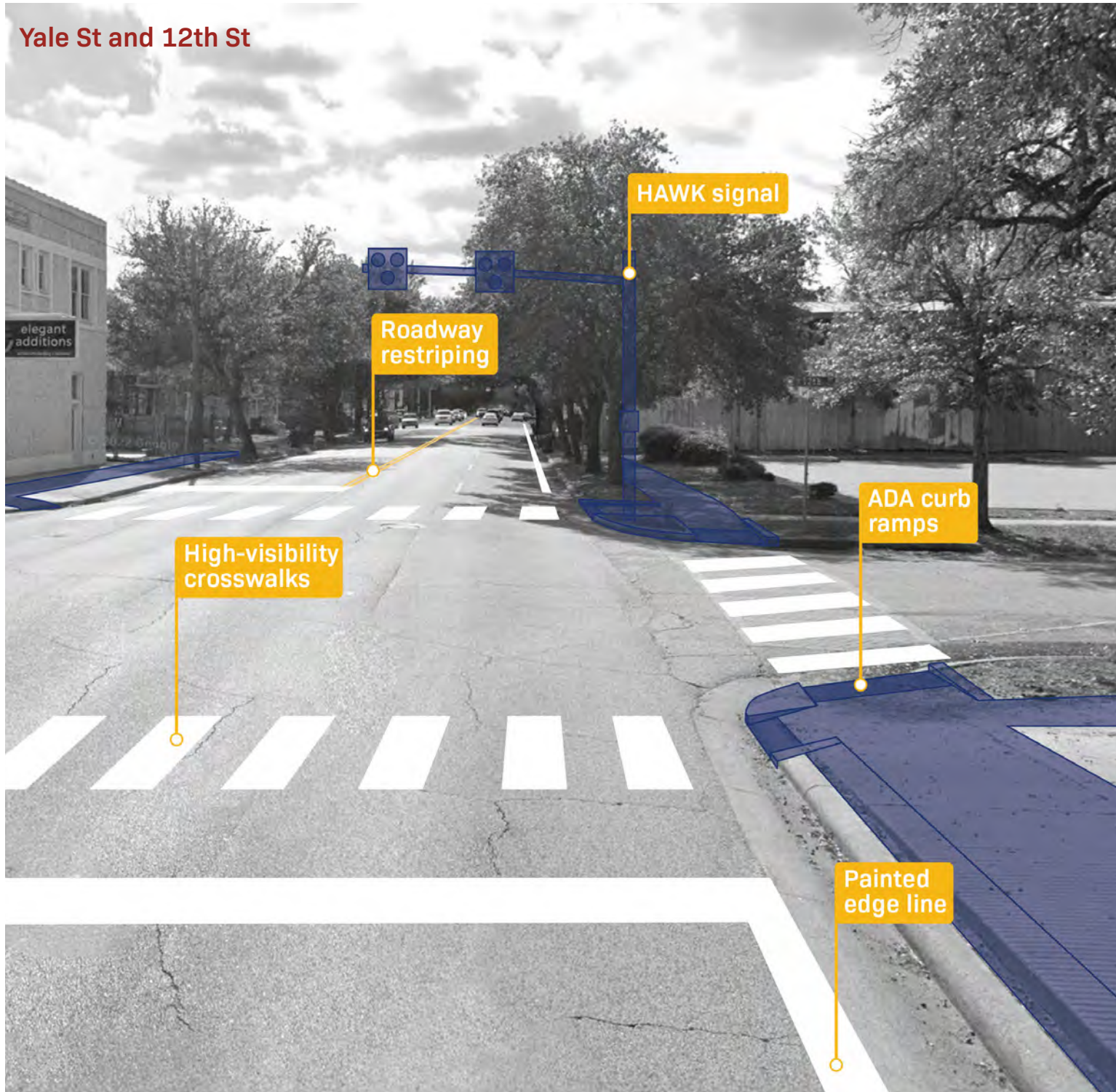
- Project (Highlighted)
- Project (Background)

Map

- Programmed Project
- Parks and Greenspace
- Bikeways and Trails
- ++ Railroads
- Schools
- Water

Figure 21. Yale Street Improvements Project Map (Source: Team Analysis, 2026)

c8 Yale Street Improvements *Short Term*



Project Description

Short-term improvements along the Yale Street corridor focus on improving pedestrian safety. These improvements are focused on **repairing and replacing all sidewalks to ensure continuous sidewalk connectivity with minimum 5 feet widths, as well as the addition of pedestrian-scale lighting along the corridor to improve visibility. Crossings along the corridor will prioritize pedestrian safety with the addition of high-visibility crosswalks, ADA curb ramps, and pedestrian signalization such as a HAWK signal to require motorists to stop for crossing pedestrians.** At intersections such as Yale Street and 12th Street, these improvements include high-visibility crosswalks, upgraded ADA-compliant curb ramps, and refreshed pavement markings to clarify pedestrian priority. At non-intersection locations with high crossing volumes, formal mid-block crossings with signalization will be installed.

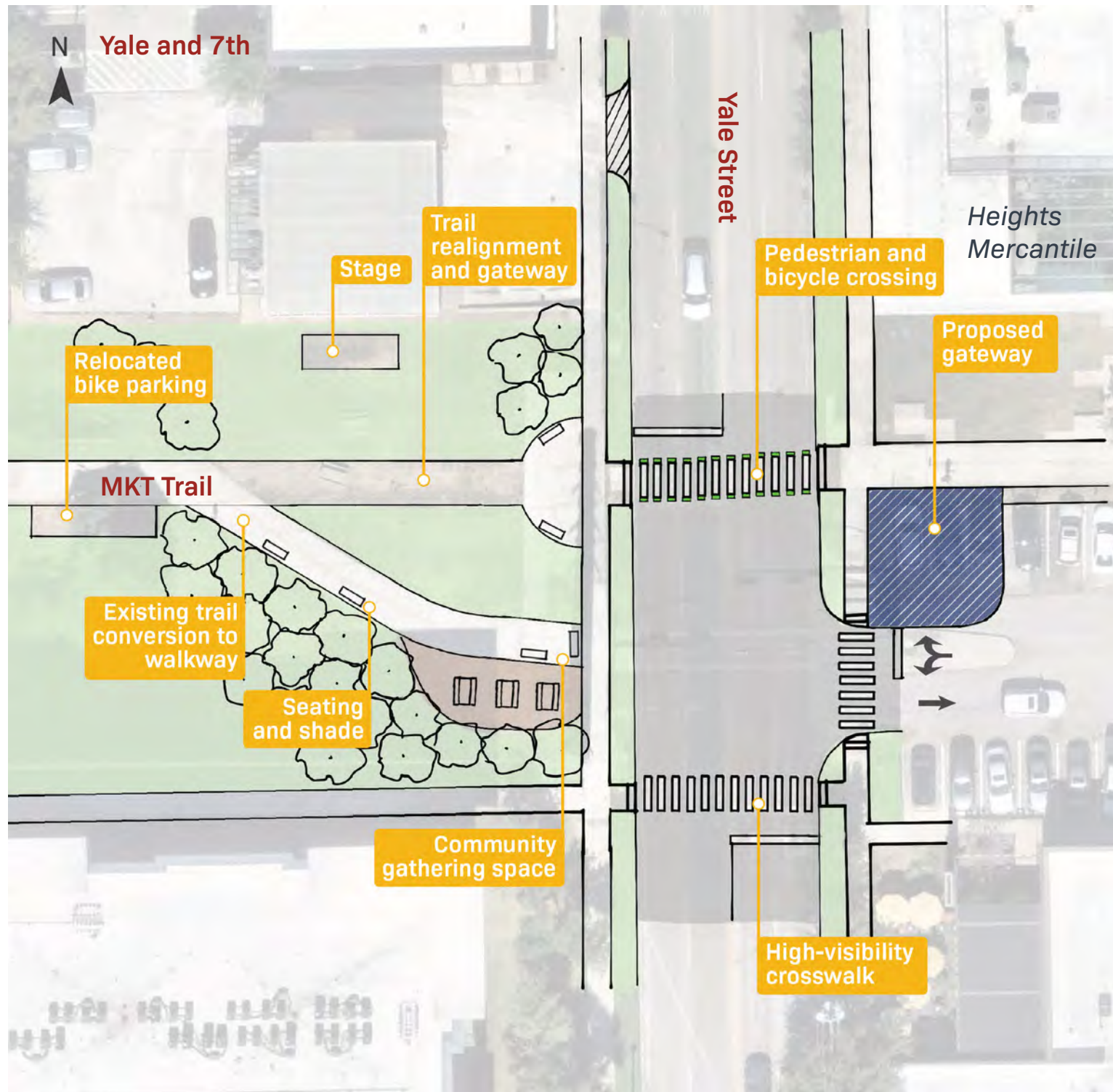
The proposed short-term improvements along Yale are intended to provide relief to the current challenging and often dangerous conditions for pedestrians. These investments will improve safety; however, they do not address the significant challenges with the current design of the roadway, which creates a dangerous environment for all users.

Implementation Considerations

- Short-term improvements can be implemented using striping, signage, and curb ramp upgrades to improve safety relatively quickly.

Figure 22. Yale Street Improvements Short Term Example (Source: Team Analysis, 2026)

C8 Yale Street Improvements *Short Term*



Project Description

The intersection of Yale Street and 7th Street provides a vital connection for the MKT Trail and support many key destinations, including local shops, restaurants, and residences. This project proposes a redesigned crossing that prioritizes safety, comfort, and gathering space.

Proposed intersection recommendations will improve conditions for people walking and riding bicycles along the MKT Trail or accessing local destinations. Currently, as the MKT Trail approaches Yale Street, people walking and riding bicycles are forced to turn onto a narrow sidewalk to cross Yale Street. Proposed crossing solutions include:

- **Realignment of the trail** to create a perpendicular crossing.
- **Upgrade of existing traffic signal to a full signal, including signalization for vehicles on Yale and 7th Sts**, as well as dedicated trail signal timing for people walking and riding bicycles across Yale Street.
- Removal of approximately 3 parking spaces to accommodate a **new neighborhood gateway and gathering point** to provide adequate space for people waiting to cross Yale St, or those entering 7th St or the MKT Trail after crossing Yale St.
- **Widening of the MKT Trail to 12-14 feet on both approaches of Yale St** to accommodate a transition zone for gathering and placemaking. The additional width also allows users to navigate conflict more comfortably especially during peak periods.
- On the west side of Yale St, the **existing shared-use path alignment is retained to preserve its familiar, sweeping character**. The path is narrowed to approximately seven to eight feet at the curb to allow space for benches and planters, creating a small plaza area where people can comfortably wait, gather, or orient themselves before crossing. This semi-circular plaza functions as a neighborhood gateway and aligns with guidance from the Parks Board Greenway Design Guidelines.

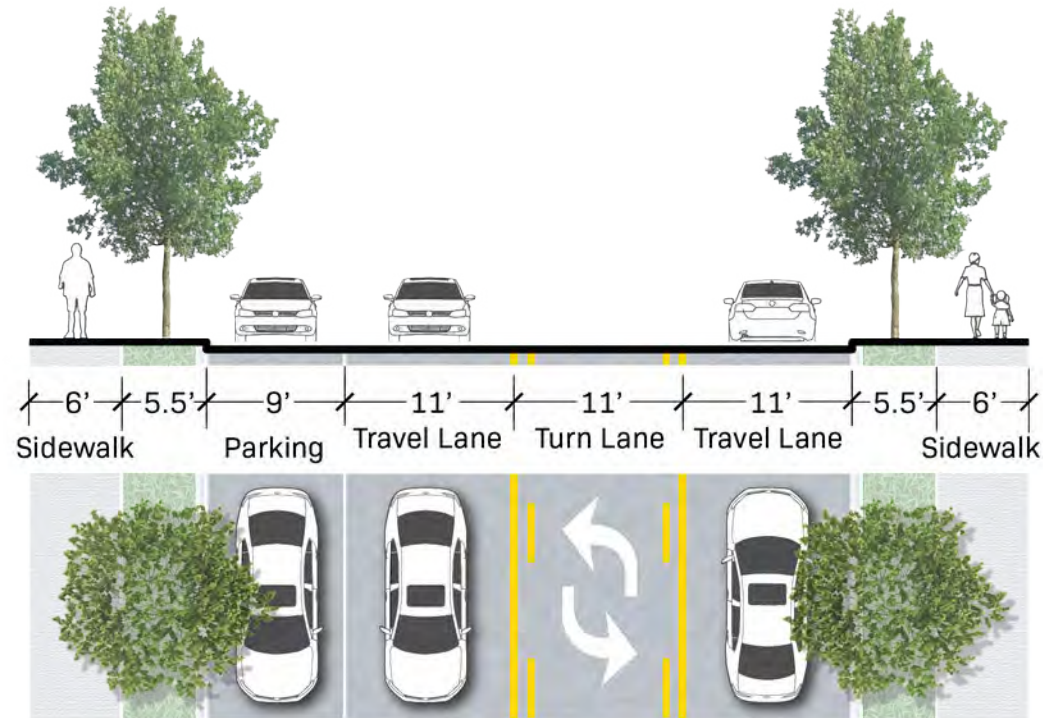
Implementation Considerations

- Trail crossing treatments at 7th Street should be coordinated with trail operators and city partners to ensure consistent design and operations.
- Public realm elements such as seating and planters should balance comfort, maintenance, and sight line needs at the intersection.

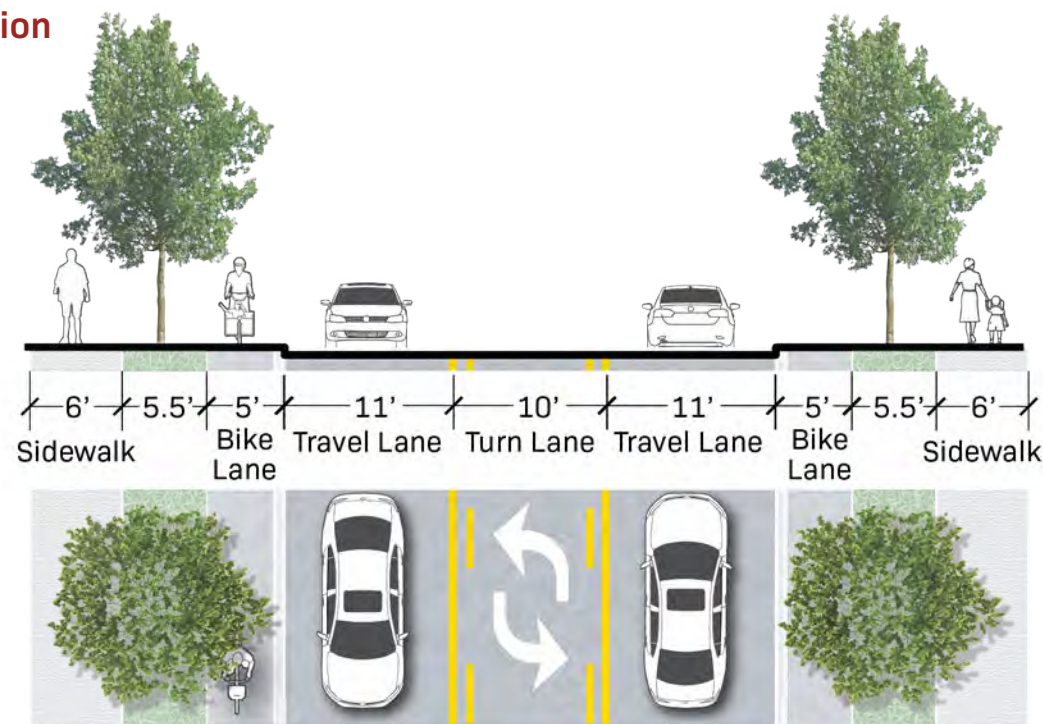
Figure 23. Yale Street at 7th Street Example Redesign (Source: Team Analysis, 2026)

C8 Yale Street Improvements *Long Term*

Retrofit



Reconstruction



Project Description

Long-term improvements illustrate how Yale Street could be reconstructed to better balance vehicle movement with the needs of people walking and biking. The **improvements include tightened curb lines, more clearly defined pedestrian space, and a reconfigured roadway that shortens crossings and improves visibility.** To support feasibility of implementation two options are provided below for long-term implementation for improving mobility on the Yale Street corridor: a retrofit and a full reconstruction of the roadway.

Retrofit: the retrofit solution includes a mill and overlay of the street surface and will maintain the existing pavement width. This solution will allow for re-striping along the corridor to include:

- 4 lanes to 3 lanes conversion
- Two, 11' vehicular travel lanes with one 11' dedicated center turn lane
- 9' parking lane
 - Parking lane will alternate sides of the street, block by block
- Maintained 5' planting strip and minimum 5' sidewalks on both sides of road
- All intersections will include:
 - High-visibility crosswalks
 - Pedestrian signalization
 - Curb ramps

Reconstruction: the reconstruction solution includes a four-lane to three-lane conversion with a continuous turn lane. The sidewalk on both sides of the street will be widened to 6', buffered by a 5.5' landscaped buffer with street trees. These improvements include a 5' bike path at sidewalk level on both sides of the street, adjacent to the vehicular lane. The separation will allow for a vertical landscape buffer to safely separate people riding bicycles and people walking, while also allowing for the preservation of existing, mature trees along the corridor. This solution also includes pedestrian safety improvements at all crossings, such as high-visibility crosswalks, curb ramps, pedestrian signalization, and pedestrian-scale lighting.

Implementation Considerations

- A conversion from 4 to 3 vehicular lanes with curb relocation provides ample space for wider sidewalks, while providing a dedicated lane for left-turning movements.
- Current AADT suggests that a conversion from 4 to 3 lanes can still accommodate the daily traffic seen on the corridor and could be beneficial in reducing left-turn conflicts.
- Long-term reconstruction will require coordination with drainage, utilities, and future capital projects.

Figure 24. Yale Street Improvements Long-Term Example (Source: Team Analysis, 2026)

20th Street Reconstruction

Extents: E TC Jester Blvd - I-45

Length: 3.2 mi.

MTFP Class: Major Thoroughfare

Project Description

20th Street is one of the Heights’s major commercial corridors, defined by a vibrant bar and restaurant scene west of Durham Drive and popular shopping and food destinations closer to Yale Street. **The roadway intersects major greenways** like White Oak Bayou, the Shepherd-Durham Corridor, Nicholson Street, and Heights Boulevard and is within a few blocks of five schools, including Hamilton Middle School.

The 20th Street Reconstruction project envisions a full reconstruction of the corridor in line with the redesign proposed in the TIRZ 5 Three Study Areas Mobility & Drainage Study, as shown in Figures 25 and 26. From Bevis Street to Harvard Street, reconstruction would provide **two vehicle travel lanes and a center turn lane with 6-8’ sidewalks**, supporting transit trips along the corridor and providing necessary space for people walking, biking, and rolling to businesses and the Bayou. **Where ROW allows, reconstruction should also include a 6-8’ median, allowing for crossings that include a median-protected pedestrian refuge.**

The full length of the project will include **updated curb ramps, high-visibility crosswalks, safe mid-block crossings at select locations, and a full conversion to curb-and-gutter drainage.** Additionally, about 0.6 miles of sidewalk can be added to fill gaps. Where possible, the project will add tree canopy and pedestrian-scale lighting. As of the completion of this report, MHRA is studying the feasibility of these designs, as well as feasible alternatives from Harvard Street to I-45.

Destinations

- White Oak Bayou Greenway, Shepherd-Durham Corridor, Nicholson Street Trail, & Heights Boulevard Trail
- Hamilton MS, Helms ES, Field ES, The Goddard School, and St. Andrew’s Episcopal School
- Transit Connections: 26 Long Point/Cavalcade, 27 Shepherd, 40 Telephone/Heights, 44 Acres Homes, 56 Airline/Montrose

Details

- 20th St ROW: 68’
- 20th St pavement width: 22’ - 26’ W of Shepherd Dr; 42’ Shepherd Dr - N Main St; 68’ E of N Main St

20th St Recommendations from TIRZ 5 Three Study Areas Mobility & Drainage Study

Figure 25. W of Durham Dr

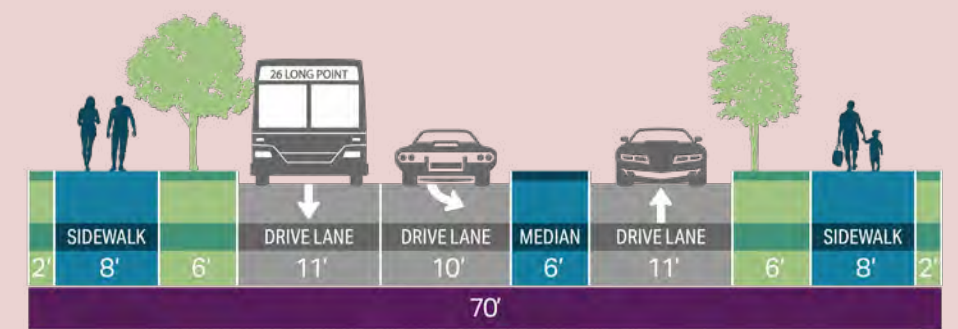


Figure 26. Shepherd Dr to Harvard St

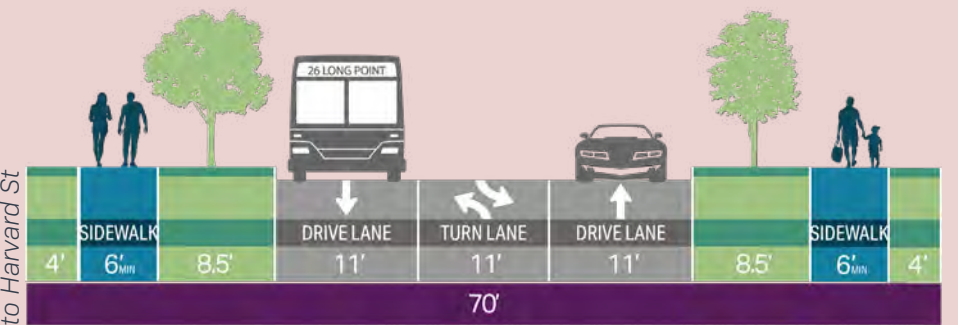


Figure 27. 20th Street Reconstruction Project Map (Source: Team Analysis, 2026)





Additional Projects

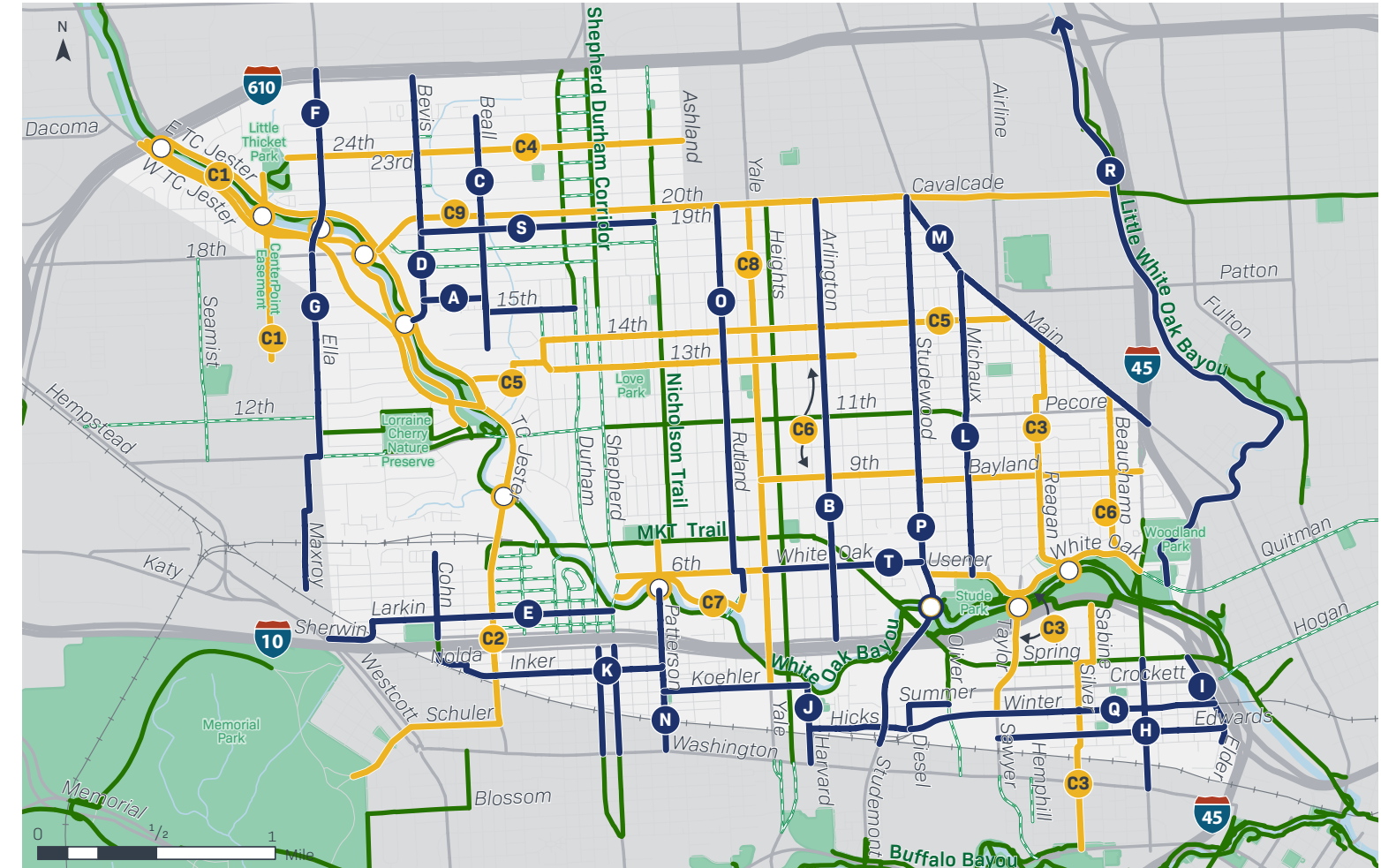


Figure 28. All Project Recommendations (Source: Team Analysis, 2026)

LEGEND					
	Catalyst Project		Active Streets & Trails		Water
	Other Project		Programmed Projects		Parks
	Key Bayou Crossing Improvement				Railroads

Figure 28 shows all Project Recommendations. The Catalyst Projects, featured on prior pages are shown in gold. Additional Projects are shown on the map in blue and represented by letters A through T.

The following pages detail the specific improvements recommended for each project, as well as descriptions of area-wide programs that do not appear on the map in Figure 28. These programs are denoted in the document as P1, P2, and P3.

Additional Projects

Project ID	Project Name	Pg.
A	W 15th/W 15 1/2 Streets	101
B	Arlington Street	102
C	Beall Street	102
D	Bevis Street	103
E	Cohn Street/Larkin/Sherwin	103
F	Ella Blvd (N)	104
G	Ella Blvd (S) & Maxroy Street	104
H	Edwards Street & Houston Avenue	105
I	Elder Street Trail	105
J	Harvard/Hicks/Diesel/Summer Streets	106
K	Shepherd/Durham & Inker/Koehler	107
L	Michaux Street	107
M	N Main Street	108
N	Patterson Street	109
O	Rutland Street	109
P	Studemont & Studewood Streets	110
Q	Winter Street Promenade	110
R	Little White Oak Bayou Extension	111
S	W 19th Street	111
T	White Oak Drive	112

A W 15th/W 15½ Streets

Extents: Bevis St - N Durham Dr

Length: 0.7 mi.

MTFP Class: Local Street

Project Description **Retrofit**

This project will create an east-west Neighborhood Street from White Oak Bayou (via Bevis Street) to the Shepherd-Durham Corridor to create safe multimodal access for residents living near the SPJST Lodge 88 and Timbergrove Park.

- **Repair broken sidewalks** and install new sidewalks (equaling 0.8 miles) along 15½ Street (MHRA and Congressional District 7 are currently installing new sidewalks along 15th Street).
- **Paint edge lines** to visibly define the travel lane and **paint curb extensions** to reduce turning radii and encourage safe travel speeds.
- Improve **curb ramps & high-visibility crosswalks** at all intersections.
- Create a safer pedestrian environment across the 15th Street Turkey Gully bridge. To improve safety, a **set of speed cushions** should be installed near the bridge on both sides to encourage slower speeds with a **printed sidewalk across the bridge** indicating the presence of people walking, biking, & rolling.
- Mill and overlay asphalt in locations with poor pavement condition.
- Add new Neighborhood Street **signage with wayfinding** to help people navigate to the Shepherd-Durham Corridor and the Bayou.



Figure 29. Example of a painted advisory sidewalk, an option for the 15th St bridge over Turkey Gully (Source: BBC)

Destinations

- Timbergrove Sports Association Fields
- SPJST Lodge 88 Community Center & Park
- Shepherd-Durham Commercial Corridor
- White Oak Bayou Greenway

Implementation Considerations

- The intersections at Beall Street and Bevis Street are critical connection points in the broader *People-First Network*. The project should direct more investment to safety at those crossings.
- 15 1/2 St ROW is 50' and 15th St ROW is 45'-70'. Pavement width is between 20' and 26' for both streets.

B Arlington Street

Extents: E 20th St - I-10

Length: 1.9 mi.

MTFP Class: Local Street

Project Description **Retrofit**

This project will create a north-south Neighborhood Street from E 20th Street to the I-10 frontage road, a nearly two-mile continuous stretch of roadway across Houston Heights and Sunset Heights. This project will also provide needed sidewalk and crossing improvements near Heights HS.

- **Repair broken sidewalks** & install of new 6' sidewalks where gaps exist & near major destinations like Heights HS & MKT Trail (equaling 0.2 miles of new sidewalk).
- **Paint edge lines** to visibly define the travel lane.
- Install **physical curb extensions** to reduce turning radii, encourage safe travel speeds, and provide better visibility of all roadway users at intersections.
- Install one **set of speed cushions** per block.
- Improve **curb ramps and paint high-visibility crosswalks** at all intersections.
- Install Neighborhood Street **signage with wayfinding** to help people navigate to the MKT Trail, 11th Street, & White Oak Bayou.
- Install mid-block crossings at key locations, especially for safe access to Heights HS.
- Install a **raised crossing at E 13th Street & Columbia Street** as a main pedestrian entrance to Heights HS.
- Install **direct sidewalk connections to the MKT Trail** to improve local neighborhood connectivity; this location is also ideal for additional **wayfinding, seating, and public art**.

Destinations

- Heights High School
- MKT Trail
- Immanuel Lutheran Church
- Transit Connections (26 Long Point/Cavalcade, 30 Clinton/Ella, 66 Quitman)
- 11th Street Bikeway
- 11th St, 20th St, & White Oak Dr Commercial Corridors

Implementation Considerations

- The street is lined with mature trees, which should be considered during sidewalk repair, widening, and installation.
- The existing pavement width and drainage type changes along the corridor.
- School drop off/pick up will impact the design between 13th & 15th Sts.
- Consider improvements to alley lighting
- Arlington St ROW is 70' and its pavement width is 20' to 34'

C Beall Street

Extents: W 26th St - W 14th St

Length: 1.0 mi.

MTFP Class: Local Street

Project Description **Reconstruct**

This project will retrofit Beall Street north of 20th Street and reconstruct Beall Street south of 20th Street. Together, these two segments will serve a critical role as one of the few major north-south connections for Shady Acres and the 19th and 20th Street Commercial Corridors. These improvements will build upon an existing MHRA project to fill sidewalk gaps along Beall Street.

Retrofit North of 20th Street

- **Repair, replace & add 6' sidewalks** north of 22nd Street, in total 1.2 miles (Congressional District 7 and MHRA are currently repairing sidewalks south of 22nd Street).
- **Paint edge lines, stop bars & high-visibility crosswalks** to visibly define the travel lane and encourage safe travel speeds.
- Add/repair **curb ramps** at all intersections.
- Mill & overlay of asphalt to improve pavement condition & **add speed cushions** at every block.
- Install **mini traffic circles** at W 23rd & W 25th Sts. (minimum of 45' pavement width at the intersection diagonal).

Destinations

- Timbergrove Sports Association Fields
- SPJST Lodge 88 Community Center & Park
- Wright-Bembry Park
- Transit Connections (26 Long Point/Cavalcade)
- W 20th & W 19th Sts Commercial Corridor

Reconstruct South of 20th Street

- Reconstruct Beall St. in alignment with the TIRZ 5 Three Study Areas Mobility & Drainage Study to include a 6' sidewalk on the east side of the street and a **10' shared-use path on the west side of the street**.
- Install **mini traffic circles** at W 18th St. & W 15^{1/2} St. (minimum of 45' pavement width at the intersection diagonal).
- Install **curb extensions** at all remaining intersections to reduce crossing distance for people walking and reduce the turning radii.
- **Add speed cushions** at every block.

Implementation Considerations

- Dense housing and narrow setbacks may require creative sidewalk strategies
- Open ditch drainage will need to be replaced with curb & gutter along much of the street
- Beall St ROW is 68' - 70' and its pavement width is 20'

D Bevis Street

Extents: I-610 - E TC Jester Blvd

Length: 1.0 mi.

MTFP Class: Minor Collector

Project Description **Reconstruct**

This project will improve a critical north-south connection for Shady Acres residents to White Oak Bayou and the W 20th Street and W 19th Street commercial corridors. This project would reconstruct Bevis Street from I-610 to W 26th and from W 20th to E TC Jester Boulevard. These improvements will build upon an existing MHRA project to fill sidewalk gaps along Bevis Street from TC Jester Blvd to W 20th Street and the City of Houston Turkey Gully project.

South of W 20th St

- Align with recommendations from the Three Study Areas Mobility & Drainage Study to add a **10' shared-use path on the west side** of Bevis St.
- Install **curb extensions** at each intersection & **inset parking** along the street, alternating sides to create chicanes that encourage people to drive at the posted speed limit.
- Replace **curb ramps** & add **high-visibility crosswalks** at each intersection to allow safe crossings for people walking.
- Add **pedestrian-scale lighting** matching the standard of the Shepherd-Durham Corridor.
- Install **speed cushions** every 500 feet.

North of W 26th St

- Reconstruct the street to match the City of Houston Turkey Gully project with **curb extensions & inset parking**.
- Add **high-visibility crosswalks**, especially at the intersection with I-610.
- Add **6' sidewalks** along the segment on both sides of the street.
- Install **speed cushions** every 500 feet.

Implementation Considerations

- The City of Houston is implementing the Turkey Gully drainage & roadway reconstruction project along Bevis St from W 26th St to W 20th St.
- Reconstruction should accommodate frequent driveways between W 26th and W 27th Sts.
- Bevis St ROW is 60'. North of 18th, its pavement width is 20'; South of 18th, 40'

Destinations

- White Oak Bayou Greenway
- Timbergrove Sports Association Fields
- Transit connections (26 Long Point /Cavalcade)
- W 20th & W 19th Sts Commercial Corridor
- Live Oak Friends Meeting

E Cohn/Larkin/Sherwin Streets

Larkin/Sherwin Extents: Shepherd Dr - Washington Ave)

Cohn St Extents: Egbert St - I-10)

Length: 1.7 mi.

MTFP Class: Local

Project Description **Reconstruct**

This project will establish a grid of safe Neighborhood Streets within Cottage Grove, linking the community to nearby trails and parks and creating safe crossings at TC Jester Boulevard. These recommendations are intended to be conducted in addition to, or in coordination with the City of Houston's Cottage Grove West drainage and sidewalk project, which will convert the streets to curb & gutter drainage and provide 5-foot sidewalks with inset parking.

- Install **high-visibility crosswalks** at every intersection along all three streets.
- Paint edge lines** to visibly define the travel lane and **build curb extensions** that shorten pedestrian crossing distances, reduce turning radii, and establish clear space near intersections to delineate space for on-street parking.
- Install Neighborhood Street **signage with wayfinding** to help people navigate to the Shepherd-Durham Corridor, MKT Trail, and Memorial Park.
- Place **pedestrian-scale lighting** along both corridors and **speed cushions** every 500 feet.
- Coordinate with the owner of the property north of Kansas St between Cohn St & TC Jester Blvd to **establish a trail connection to the western terminus of the MKT Trail**.
- Coordinate with TxDOT for a future **Cohn St pedestrian & bicycle bridge** over I-10 as part of their Inner Katy Project.
- Improve **crossings at Shepherd Dr & Durham Dr** to for people walking & biking to the White Oak Bayou Greenway.
- Establish a safe, dedicated **pedestrian & bicycle facility along Sherwin St** to and across Washington Avenue to add connectivity to Memorial Park.

Destinations

- Cottage Grove Park & Pedestrian Bridge over I-10
- Larkin Street Baptist Park
- White Oak Bayou Greenway
- Transit Connections (66 Quitman)

Implementation Considerations

- The White Oak to Memorial Park Connection recommendation (B) also recommends improvements to the intersection with Larkin St & TC Jester Blvd
- The ROW of these streets is 50' and their pavement is 20'

F Ella Boulevard (North)

Extents: I-610 - W 18th St

Length: 0.8 mi.

MTFP Class: Major Thoroughfare

Project Description **Reconstruct**

Ella Boulevard is a four-lane divided street with an existing turn lane & median along the corridor with four-foot standard bike lanes alongside the vehicle lanes. This section of Ella Blvd has a high crash rate and is on the City of Houston and Harris County Vision Zero High-Injury Network.

- **Move the existing curb** four feet to the current location of the painted bike lanes to **maintain the existing vehicle travel lanes** and create enough space behind the curb for an **8' sidewalk on both sides of the street** with opportunities to plant trees.
- Redesign all intersections to provide **directional curb ramps, high-visibility crosswalks, & smaller turning radii** to reduce the distance for pedestrian crossings.
- Where possible, **reconstruct median noses** to allow room for pedestrian refuge islands.
- Realign the curb ramp curb cuts to provide **directional ramps for the crosswalks** at the I-610 frontage road.
- In addition to the City of Houston District C intersection improvements at W 22nd St/W 23rd St, study the W 24th St and W 23rd St intersections to see if either warrants a signal & relocate the 30 Clinton/ Ella bus stops to the cross street that meet those warrants.
- Close unused driveways to **reduce turning conflicts** for people driving, walking, & biking.
- A **cantilevered trail bridge** along the existing Ella Blvd bridge over the Bayou and the intersections at E & W TC Jester Blvds and W 18th St can be improved as part of this project or the White Oak Connectivity Vision (Project C1). See page 65 for details on those intersections.

Destinations

- Memorial Hermann Greater Heights Hospital Emergency Center
- Little Thicket Park
- White Oak Bayou Greenway
- Ella Blvd & W 18th St Commercial Corridors
- Transit Connections: 30 Clinton/Ella

Implementation Considerations

- The White Oak Connectivity Vision (Project A) will also offer an opportunity to improve the Ella Blvd intersections with E & W TC Jester Blvds & W 18th St.
- Design should pay close attention to utility relocations & ensure minimal disruption to businesses along the corridor.
- Ella Blvd N ROW is approx. 75' and its pavement width is approx. 60'

G Ella Boulevard (South)

Extents: W 18th St - 11th St

Length: 1.3 mi.

MTFP Class: Major Thoroughfare

Project Description **Reconstruct**

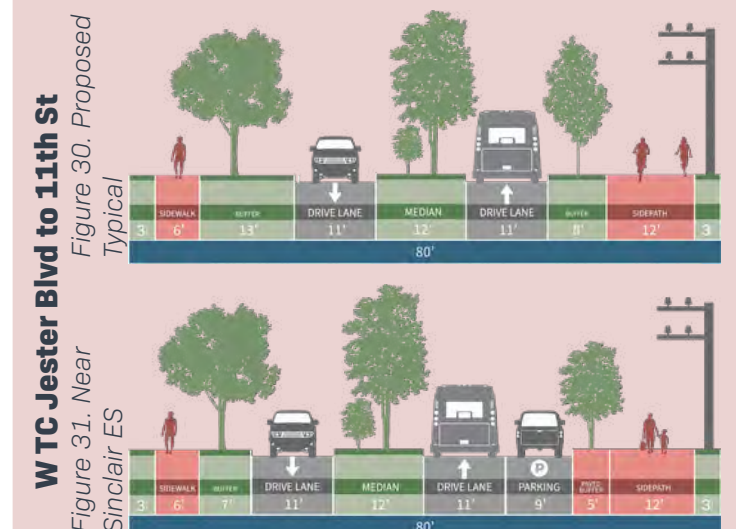
The southern segment of Ella Boulevard is currently a four-lane divided street with a median-divided turn lane and painted four-foot bike lanes. This segment of Ella Blvd. should be reconstructed as recommended in the TIRZ 12 Mobility Plan, which focuses on providing safe access for people walking and biking in Lazybrook & Timbergrove, particularly students of Sinclair Elementary.

- Reconstruct the street to have **two lanes with a median-divided center turn lane** to align with vehicle volumes & encourage drivers to adhere to the posted speed limit.
- Build a **6' sidewalk on the west side and a 12' shared-use path on the east side** of the street.
- Near Sinclair ES, include an **additional parking lane**, a longer **southbound left-turn lane**, & a **dedicated right-turn lane** onto Grovewood Ln to mitigate congestion during peak school hours.
- Redesign all intersections to provide directional **curb ramps, high-visibility crosswalks, smaller turning radii, & pedestrian refuges across the medians** to allow for safer pedestrian crossings.
- Modify the MTFP designation **from a Major Thoroughfare to a Minor or Major Collector**
- **Extend** improvements through **Maxroy Street** for **access to Memorial Park** via Cottage Grove.

Destinations

- Sinclair ES & Principrin School
- White Oak Bayou Greenway
- Ella Blvd & W 18th St Commercial Corridors
- Transit Connections: 30 Clinton/Ella

Ella Blvd Recommendations from City Park Mobility Plan (TIRZ 12)



Implementation Considerations

- Design should consider utility relocations and trees. There should be plenty of space behind the curb to move utilities and avoid trees for most of the corridor.
- Ella Blvd S ROW is approx. 75'; its pavement is approx. 60'

H Edwards Street & Houston Avenue

Extents: Sawyer St - Elder St

Length: 1.5 mi.

MTFP Class: Major Th-fare (Houston) Local St (Edwards)

Project Description Retrofit Reconstruct

This project will establish a safe and walkable east-west Neighborhood Street along Edwards Street and north-south street along Houston Avenue. Improving these two corridors will link key destinations like Sawyer Yards and provide key safe connections across Sawyer Street and Houston Avenue. All improvements should follow recommendations from the Old Sixth Ward Mobility Plans (Phases 1 & 2).

Edwards St

- Widen **sidewalks to 6'** along the corridor
- Formalize on-street parking with painted parking lanes** on both sides of the street and build **concrete curb extensions with vertical delineators** to shorten pedestrian crossing distances, keep intersections clear of parked vehicles, & reduce turning radii.
- Add **speed cushions** and **paint sharrows**
- Redesign the intersection of Edwards St & Houston Ave** to include crossings on all four sides, directional curb ramps, leading pedestrian intervals, & tighter turning radii. Conduct a signal warrant for the intersection with signal timing coordinated with the Crockett St signal along Houston Ave.

Houston Ave

- Convert roadway to two vehicle lanes in each direction with **center turn lane and medians to improve safety for turning vehicles**. This will create room for designated parallel parking lanes as well as **safe crossings at intersections with reduced turning radii and shortened crossing distances** for people walking.
- Add a **wide sidewalk for shared uses behind the curb or an on-street bikeway** depending on available ROW.
- Widen **sidewalks to 6'** along the corridor
- Redesign the rail underpass between Center St and Dart St with lighting and ADA-compliant ramps.

Destinations

- Edwards St & Houston Ave commercial corridors
- Sawyer Yards
- Brock Park & American Statesmanship Park
- Transit Connections: 44 Acres Homes

Implementation Considerations

- Once the Elder St Trail (Project I) is completed, Edwards St can connect directly into the trail and offer a viable connection to the Spring St Trail and the Bayou.
- Edwards St ROW is 50' E of Holly St, otherwise it is 70'. Pavement width is 35' to 50'

I Elder Street Trail

Extents: Spring St Trail - Dart St

Length: 0.4 mi.

Project Description New Trail

First Ward residents and businesses east of Houston Avenue have no existing north-south connection due to the lack of crossings of the Southern Pacific rail line. The Elder Street Trail project should be proposed as part of TxDOT's North Houston Highway Improvement Project (NHHIP). The project should include the construction of a **12' shared-use path along TxDOT right-of-way to connect the Spring St Trail to Dart St** with neighborhood connections along the length. The trail would cross under the existing rail bridge and design should consider **future connections into Downtown and along Crockett Street** as part of NHHIP.

This trail extension would provide a **safe connection** for the residents **to the Spring St Trail**, unlocking **additional links to the White Oak Bayou and MKT Trail**.

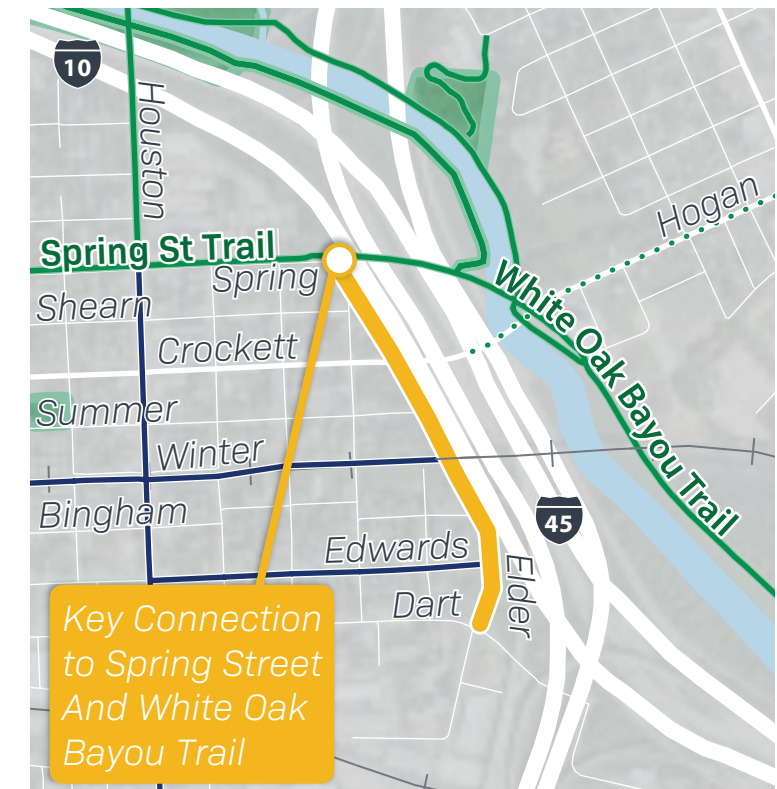


Figure 32. Elder Street Trail Project Map (Team Analysis, 2026)

Destinations

- Spring St Trail
- American Statesmanship Park
- White Oak Bayou Greenway
- Ecclesia Houston

Implementation Considerations

- Implementation offers an opportunity for new trail connections into Downtown via White Oak Bayou Greenway & the existing I-10 & I-45 alignment. Opportunities for connections outside of the Study Area should be reviewed during design
- The ROW for the proposed Elder St Trail is owned by TxDOT. There is no existing pavement.

Harvard/Hicks/Diesel/Summer Streets Corridor

Extents: Heights Blvd - Oliver St

Length: 1.0 mi.

MTFP Class: Local Streets

Project Description

Retrofit

Reconstruct

This project will create an east-west Neighborhood Street corridor by improving a series of streets that connect key commercial and residential destinations from Yale Street to Oliver Street. These streets provide direct connections to both the White Oak Bayou Greenway and Oliver Street Trail (connecting to the Spring Street Trail). Hicks Street also provides a low-traffic, grade separated crossing of Studemont Street as a safer alternative crossing for people walking and biking. Destinations served include multiple grocery stores, high-activity shopping centers, White Oak Bayou Greenway, and a growing number of new residential developments.

This project will:

- Fill **sidewalk gaps & repair existing sidewalks with a 6' minimum** width.
- **Paint edge lines** on Harvard, Hicks, and Diesel Sts to visibly define the travel lane.
- Formalize **on-street parking along Summer St** with paint.
- Improve all crossings with **high-visibility crosswalks, curb ramps, & curb extensions** to reduce crossing distances for pedestrians and provide a safer crossing opportunity, including at key intersections with Heights Blvd and Washington Ave.
- Add **mid-block crossings at key destinations** along Summer St.
- Install a set of **speed cushions** approximately every 500 feet.
- Add **pedestrian-scale lighting** along the corridor, especially near the Summer St & Oliver St intersection.
- Redesign the **Summer St & Oliver St intersection** to improve visibility for all road users and establish a more logical connection to the Oliver St trail.
- Install Neighborhood Street **signage with wayfinding** to help people navigate to White Oak Bayou & the MKT Trail via Oliver St.

Destinations

- Kroger
- HEB Buffalo Heights
- MKT/Spring Street Trail
- West End Multi-Service Center
- White Oak Bayou Greenway
- Olivewood Cemetery
- Transit Connections: 40 Telephone/Heights, 85 Antoine/Washington, 56 Airline/Montrose

Implementation Considerations

- Excess pavement around Studemont bridge (40 feet whereas adjacent approaches have 26 feet of pavement)
- Where excess ROW allows, consider designing for a wide sidewalk or shared-use path along segments of the corridor
- Consider crossing improvements at Washington Ave & Harvard St to connect to HEB and Spotts Park
- The ROW for these streets is 50' to 70', and their pavement width is 26' for Harvard & Hicks, 18' for Diesel, and 35' for Summer

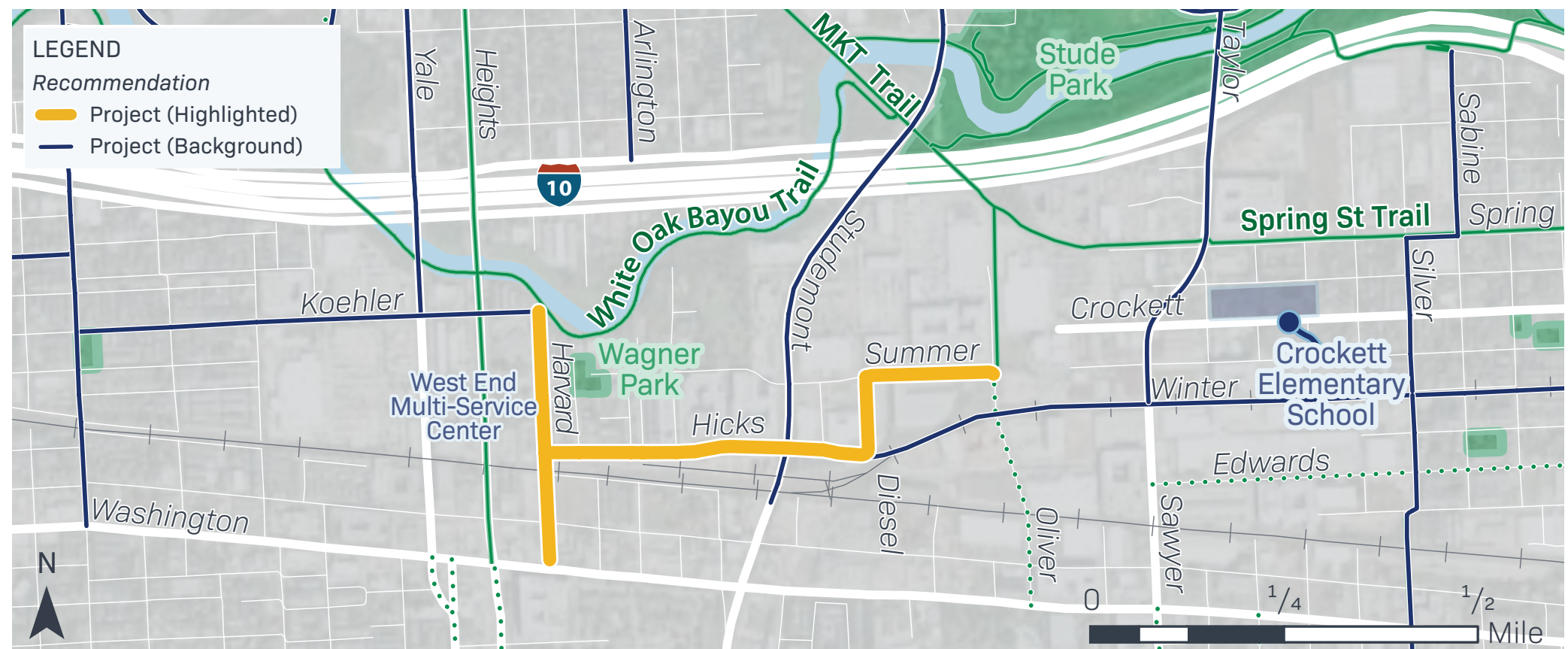


Figure 33. Harvard/Hicks/Diesel/Summer Streets Project Map (Source: Team Analysis, 2026)

K Shepherd/Durham Drives & Inker/Koehler Streets

Extents: I-10 - Harvard St

Length: 2.5 mi.

MTFP Class: Major Th.fare (Shep./Durh.) Local St (Others)

Project Description **Reconstruct**

This project will fill the gap in safe walking and biking infrastructure along Shepherd Drive and Durham Drive from I-10 to Washington Avenue. It will also improve the east-west connection along Nolda, Inker, and Koehler Streets linking Cottage Grove, the West End, and the Yale Street and Heights Boulevard corridors south of I-10. Shepherd Drive, Durham Drive, Inker Street and Koehler Street are all on the Harris County or City of Houston Vision Zero High-Injury Networks

Inker St, Koehler St, & Nolda St

- Replace and repair all **sidewalks to 6'**, **improve curb ramps**, and **add high-visibility crosswalks** along both corridors.
- **Install concrete curb extensions** that shorten pedestrian crossing distances & establish clear space near intersections.
- Install Neighborhood Street **signage with wayfinding** to help people navigate to the White Oak Bayou Greenway & Cottage Grove.
- Place **pedestrian-scale lighting** along both corridors and **speed cushions** every 500 feet.
- Coordinate with TxDOT to connect to the rebuilt Cohn St pedestrian & bicycle bridge over I-10.
- Redesign the **intersections at Shepherd Dr, Durham Dr, Yale St, & Heights Blvd** with safer, visible crossings for people walking and biking.

Shepherd Dr & Durham Dr

- Reconstruct Shepherd Dr and Durham Dr from I-10 to Washington Ave to **match the improvements north of I-10**, including a wide sidewalk and bikeway behind the curb, pedestrian-scale lighting, seating and shade improvements,
- Improve crossings to be **high-visibility with smaller turning radii and shorter crossing distances** for people walking.
- Coordinate with UPRR to improve the **at-grade rail crossing to include safe crossings** for people walking and biking.

Implementation Considerations

- TxDOT's Inner Katy Project will reconstruct the Cohn Street bridge over I-10
- May require reconstruction of street to replace open ditch drainage and provide appropriate space for facilities
- Design should consider the many driveways along both Inker & Koehler Sts.
- West of Bonner St, the ROW for these streets is 50' and pavement is 20'. East of Bonner St ROW is 60' and pavement is 40'.

Destinations

- Cottage Grove Park
- West End Park
- Shepherd/Durham Commercial Corridor
- White Oak Bayou Greenway
- West End Multi-Service Center

L Michaux Street

Extents: N Main St - Usener St

Length: 1.3 mi.

MTFP Class: Minor Collector

Project Description **Retrofit**

This project will affirm Michaux Street as a north-south Neighborhood Street with added improvements to create a safe and comfortable walking and biking corridor. This project would retrofit most of Michaux Street. This project (alongside Project C3) provides a great opportunity to redesign the intersection of Michaux Street and Usener Street to establish a safe & distinct neighborhood gateway into Stude Park.

- Replace & repair all **sidewalks to 6' width**, equaling 2.3 miles of new sidewalk.
- Redesign all crossings with **concrete curb extensions, high-visibility crosswalks, & directional curb ramps** to provide safe crossings.
- Give special attention to the **redesign of key intersections** at N Main St (transit connection), 14th St (*People-First Network*), Merrill St (Hogg MS), & Usener St (Stude Park).
- Where open ditch drainage exists, reconstruct to convert drainage to **curb-and-gutter** drainage & provide spot repairs where poor pavement quality exists.
- **Formalize on-street parking** with painted parking areas away from intersections.
- Install Neighborhood Street **signage with wayfinding** to help people navigate to Stude Park, the Bayou, and the 11th Street Bikeway.
- Install **pedestrian-scale lighting** along the corridor and **speed cushions** every 500 feet.

Destinations

- N Main St & 11th St Commercial Corridors
- Hogg MS
- Stude Park
- White Oak Bayou Greenway

Implementation Considerations

- Reconstruction to occur only where open ditch drainage exists
- Reconstruction should consider location of utilities & prevent removal of existing trees, where possible
- S of 14th St Michaux St ROW is 60', N of 14th it is 50'. Its pavement width is 26' to 32'

M N Main Street Reconstruction

Extents: W Cavalcade St - I-45

Length: 1.4 mi.

MTFP Class: Major Thoroughfare

Project Description **Reconstruct**

N Main Street is a major corridor serving several neighborhoods including Sunset Heights, Norhill, Brooke Smith, and Woodland Heights. The roadway’s current design within the Study Area makes it a barrier for these neighborhoods—it diagonally bisects the local street grid, creating misaligned and skewed intersections that are difficult for people driving, walking, and biking to navigate safely.

High vehicle speeds on the corridor exacerbate the issue; the 85th percentile speed exceeds 40 mph between I-610 and I-45, including in school zone segments. This project proposes realigning the corridor’s design with its existing vehicle volumes to better serve local residents and embrace its role as a key commercial and transit corridor.

- Convert N Main Street from a four-lane undivided roadway to one with **two vehicle lanes and a center turn lane**, giving people driving time and space to make safer turning movements without disrupting traffic flow.
- Reallocate the remaining space to **widen sidewalks** (8' minimum; preferred 10' width) to create a **safe path for people walking and biking, including students** who cross N Main Street to get to Browning, Field, and Travis Elementary Schools (equaling 0.5 miles of new sidewalks).
- Include **pedestrian-scale lighting, enhanced bus stops, shade trees, and designated on-street parking** where right-of-way allows to improve the pedestrian realm and support local businesses.
- Provide **improved curb ramps, high-visibility crosswalks, and reconfigured intersections** that shorten the distance needed to walk across N Main Street.
- Conduct **signal warrant analysis** for select intersections along the corridor, including locations near schools and parks such as Beauchamp St, Cottage St, Northwood St, 14th St, Michaux St, and Winston St.

The City of Houston has previously studied and developed designs for both short- and long-term improvements to N Main Street to address crossing safety and vehicle speeds. This project should consider this prior work by the City and incorporate its recommendations where feasible.

Destinations

- Browning ES, Field ES, & Travis ES
- Montie Beach Park & Proctor Plaza Park
- Transit Connections: 26 Long Point/Cavalcade, 44 Acres Homes, 56 Airline/Montrose

Implementation Considerations

- Design should consider access management challenges along the corridor & identify where closing some diagonal intersections would both improve local traffic operations and reduce the potential for crashes.
- ROW of N Main St is 60'; its pavement width is 44'

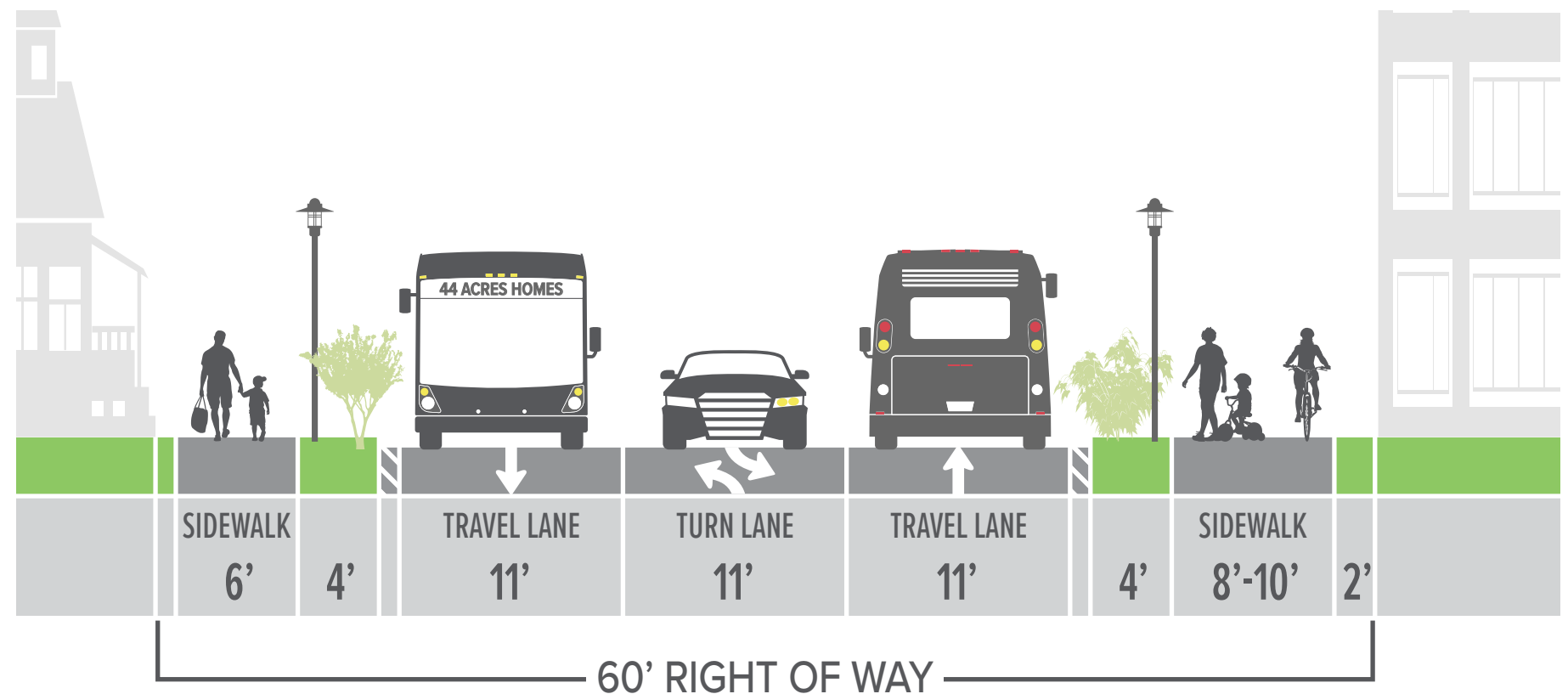


Figure 34. N Main Street Reconstruction Project Example Recommended Cross Section (Source: Team Analysis, 2026)

N Patterson Street

Extents: White Oak Bayou Trail - Washington Ave

MTFP Class: Local Street N of I-10, Major Collector S of I-10

Length: 0.6 mi.

Project Description **Retrofit**

This project will improve Patterson Street as a safe and walkable Neighborhood Street, therefore fulfilling its unique potential as a north-south corridor that connects the White Oak Bayou Trail and Washington Avenue. North of I-10, Patterson Street can be retrofitted to a Neighborhood Street.

- Replace & repair all **sidewalks to 6' width**, equaling 0.6 miles of new sidewalks.
- Redesign all crossings with **concrete curb extensions, high-visibility sidewalks, & directional curb ramps** to provide safe crossings, keep intersections clear of parked vehicles, & reduce turning radii.
- Provide spot repairs where poor pavement quality exists.
- **Formalize on-street parking** with painted parking areas away from intersections.
- Install Neighborhood Street **signage with wayfinding** to help people navigate to West End Park and the Bayou.
- Install **pedestrian-scale lighting** along the corridor and **speed cushions** every 500 feet.
- Study the feasibility of a pedestrian/ bike bridge over White Oak Bayou.

North of the I-10 Highway bridge, improvements would expand to include:

- **Mini traffic circle** at Cornish St to calm traffic coming off of the highway.
- Concrete **chicanes and access management** to protect walkers and bikers accessing the White Oak Bayou Trail.
- **Pedestrian lighting** to promote access to the Trail and improve the entrance.

Destinations

- White Oak Bayou Trail
- West End Park
- Crossing of I-10 and railroad
- Washington Avenue Commercial Corridor

Implementation Considerations

- Coordinate with TxDOT's I-10 Inner Katy Project, which plans to provide a 17-foot pedestrian & bicycle realm on the overpass.
- Implementation in conjunction with Project T will create a more robust local network, as they align for approximately 530 feet.
- The ROW of Patterson St is 50' N of I-10 and 55' S of I-10. Its pavement width is 35'

O Rutland Street

Extents: W 20th St - W 6th St

Length: 1.6 mi.

MTFP Class: Local Street

Project Description **Retrofit**

This project will create a 1.56-mile north-south Neighborhood Street along Rutland Street from W 20th Street to the MKT Trail and W 6th Street, serving the Houston Heights neighborhood.

- **Repair broken sidewalks** and install new 6'-wide sidewalks where gaps exist (equaling 1.9 miles of new sidewalks).
- South of 11th St, **paint edge lines** to visibly define the travel lane.
- North of 11th St, **paint parking rows** to visibly define the travel lane. Install **curb extensions** to reduce turning radii, encourage safe travel speeds, & provide better visibility of all roadway users at intersections.
- Improve & install **curb ramps, stop bars, & high-visibility crosswalks** at all intersections.
- Install a **set of speed cushions** every block.
- Add Neighborhood Street **signage with wayfinding** to help people navigate to the MKT Trail, 11th Street, and White Oak Bayou.

Destinations

- White Oak Bayou Greenway & MKT Trail
- Houston Heights Bird Sanctuary
- Milroy Park
- W 20th St & W 19th St, W 29th St, W 11th St, and M-K-T Commercial Corridors
- Future: Rutland Street bridge over White Oak Bayou

Implementation Considerations

- The street is lined with mature trees, which should be considered during sidewalk repair, widening, and installation.
- Coordination with TxDOT and its plans for the Rutland Street bridge over White Oak Bayou for a seamless transition within the multimodal network.
- Rutland St ROW is 70' and its pavement width is 35' N of 11th, 20' S of 11th

P Studewood/Studemont Street | Q Winter Street Promenade

Extents: N Main St - UPRR

Length: 2.4 mi.

MTFP Class: Major Thoroughfare

Project Description **Retrofit**

North of I-10, Studewood Street is a critical commercial corridor with recent crossing improvements for safe transit and school connectivity. South of I-10, Studemont Street connects to large commercial and residential developments. This project focuses on a retrofit of existing crossings with wider sidewalks or shared use paths behind the curb.

- **Add a 10' shared-use path behind the curb on one or both sides** of the street north of I-10.
- South of I-10, **widen existing sidewalks to 6'** preferred, in total equaling 0.4 miles of new sidewalks.
- Add **mid-block crossings at key locations** to match the existing mid-block crossings to improve school crossing safety & support transit connectivity at 7th St, 8th St, Bayland Ave, Highland St, Cottage St, 14th St, between Le Green St & Peddie St, 18th St, and 19th St.
- **Upgrade curb ramps & crosswalks at all intersections**, especially at White Oak Dr, 11th St, & 14th St.
- Study options for redesigning the intersection of E 20th St, N Main St, & Studewood St as part of MHRA's current 20th Street project.
- **Access management** to consolidate driveways and remove sidewalk conflicts where feasible.

Destinations

- Field ES, Hogg MS
- Stude Park & White Oak Bayou Greenway
- Studewood St, Studemont St, N Main St, 14th St, 11th St, & White Oak Dr Commercial Corridors

Implementation Considerations

- Recent improvements by METRO BOOST project created improved bus stops, better sidewalks/curb ramps, & new mid-block crossings.
- Parked vehicles often block the sidewalks along Studewood St; wider sidewalks or a new shared-use path should consider placement to reduce the possibility of these conflicts.
- Studewood and Studemont St ROW is 50' with the exception of one block of 70'; its pavement width is 35'

Extents: Diesel St - Elder St

Length: 1.3 mi.

MTFP Class: Local Street

Project Description **Reconstruct**

The UPRR line along Winter Street is a major north-south barrier for the First Ward neighborhood. Prior studies recommend a **trail along one side of the rail line or an alley-like shared street** to improve local access for residents. This plan reinforces these recommendations as short term investment to create a safer, more accessible local street.

Long term, this plan recommends "triple tracking" the freight rail line. Triple tracking would relocate all freight traffic from Winter Street to the main sub line running north of Center Street. This would allow for several new opportunities:

- **Reconnect the disrupted local street grid**, including at Sabine, Colorado, Johnson, Hickory, Goliad, and Holly Streets.
- **Remove at-grade rail crossing conflicts** at existing crossing locations like Houston Ave, Silver, Henderson, Oliver, and Sawyer Streets.
- Create **space for the Winter Street Promenade: a new trail & local street connection from Hicks St to Holly St** in the east, similar to the existing design along Spring Street.
- When combined with other recommendations in this plan, including the Elder Street Trail, **establish a premier neighborhood loop of trails** to benefit local residents and businesses.

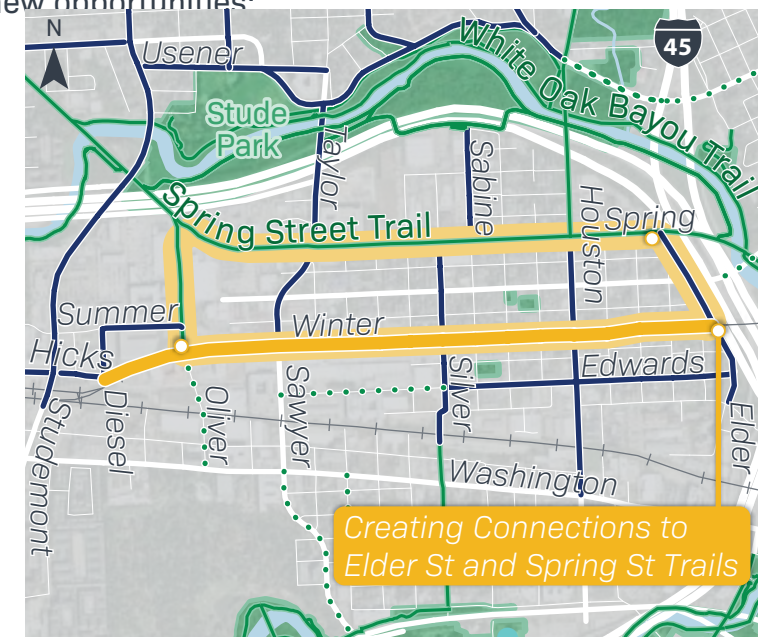


Figure 35. Winter Street Trail Loop (Team Analysis, 2026)

Destinations

- Brock Park, Summer Street Park
- Sawyer St, Studemont St, Edwards St, and Houston Ave Commercial Corridors
- Transit Connections: 30 Clinton/ Ella, 44 Acres Homes

Implementation Considerations

- Implementation would require coordination with UPRR & could be part of a larger grade separation project along the corridor.
- Winter St ROW is 70', and its pavement is variable from 0 - 11'

R Little White Oak Bayou Trail

Extents: White Oak Dr - Moody Park

Length: 1.3 mi.

Project Description **New Trail**

Little White Oak Bayou, also known as Hollywood Bayou, is a north-south branch of White Oak Bayou that runs for over 6 miles from Yale Street at W Tidwell Road in north Houston to Freed Art & Nature Park. Near Greater Heights, Little White Oak runs along important community destinations, including Moody Park, White Oak Music Hall, and Woodland Park. This project proposes an expansion of the Bayou Greenways by incorporating Little White Oak Bayou into the regional trail network and providing safe walking and biking access to the North Main Street commercial corridor.

- **Add a 12' Shared Use Path on the bank** of Little White Oak Bayou.
- Build **underpasses**, where feasible, to provide continuous, uninterrupted trail access beneath major corridors.
- Add **mid-block crossings at key locations** to support access to community destinations from either side of the trail.
- **Upgrade or build sidewalks** connecting to trail access points.
- Study the feasibility of extending the Little White Oak Bayou Greenway north beyond Moody Park.

Destinations

- Woodland Park, Freed Art & Nature Park
- Moody Park & Community Center
- White Oak Music Hall
- North Main/White Oak Commercial District
- METRORail Red Line (via Moody Park station)

Implementation Considerations

- Coordination with TxDOT and plans for NHHIP are critical as the Bayou is currently channelized beneath the North Freeway
- Coordination with North Main Street project to create safe mobility at junction with Little White Oak Bayou
- Little White Oak Bayou is not well-maintained; partnerships to clean and enhance the Bayou should be considered
- HCFCD owns the ROW for this area

S W 19th Street Reconstruction

Extents: W 20th St - Nicholson St

Length: 1.1 mi.

MTFP Class: Minor Collector

Project Description **Reconstruct**

W 19th Street is a critical commercial corridor for the Shady Acres neighborhood, home to popular restaurants and bars that serve the greater Houston area. This project supports the recommendations of the TIRZ 5 Three Study Areas Mobility & Drainage Study to reconstruct W 19th Street as a signature link from White Oak Bayou to the walkable “Main Street” storefronts east of Nicholson Street.

- Where ROW is wide, construct a **median-running, tree-lined 12' shared-use path**, similar to Heights Blvd. Where ROW narrows, the **shared-use path should narrow to 10' & move behind the curb** on one side.
- Along both segments, **provide two vehicle lanes in each direction with inset parking, upgraded curb ramps & shortened pedestrian crossings** to improve pedestrian visibility.
- Use **enhanced pavement markings** to communicate shared spaces between people driving & people outside of cars.
- Add **mid-block crossings** to increase access to local businesses (ex: Beall St to Durham Dr).
- **Upgrade or add sidewalks** (1.2 miles) & improve connections to local bikeways and active streets, including through **wayfinding** that directs people to Nicholson Street, Heights Boulevard, and the White Oak Bayou Greenway.

Destinations

- Nicholson Street Trail
- 19th Street Commercial Corridor
- Heights Boulevard active street

19th St Recommendations from TIRZ 5 Three Study Areas Mobility & Drainage Study

Figure 36. Bevis St to 904 W 19th St

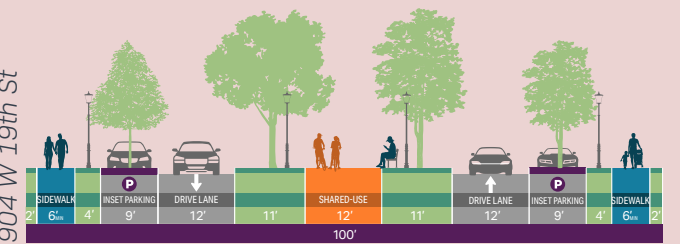
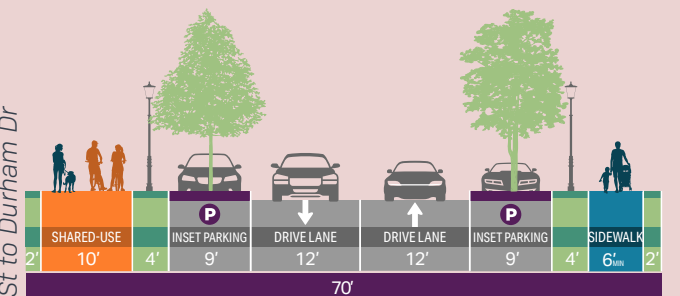


Figure 37. 904 W 19th St to Durham Dr



Implementation Considerations

- Consider projects for Yale Street and West 20th Street to coordinate safe access and mobility for all users
- 19th St ROW is 70' or more; pavement width is 22' west of Durham Dr, 38' east of Shepherd Dr.

T White Oak Drive

Extents: Yale St - Studewood St

Length: 0.7 mi.

MTFP Class: Minor Collector

Project Description **Retrofit**

White Oak Drive is one of the major commercial corridors in the Heights’s southern neighborhoods and home to many local businesses. Despite the street’s connections to the MKT Trail and Heights Boulevard, the four-lane undivided street does not often feel comfortable to walk along or cross on foot. A retrofit that focuses on crossing improvements and placemaking would support local businesses could affirm this corridor as a destination within the Study Area.

- **Upgrade all sidewalks (6’ width preferred)** & replace missing or poor-condition curb ramps, equaling 0.4 miles of new sidewalks in total.
- Improve the existing **mid-block crossing at the MKT Trail** by converting the existing painted curb extensions to concrete.
- Improve the existing crosswalks at **Threlkeld St & Granberry St to raised crossings** to improve access to local businesses.
- Install **curb extensions & high-visibility crosswalks**, particularly at key intersections such as Yale St, Heights Blvd, Arlington St, Harvard St, and Oxford St.
- Invest in placemaking such as **seating, pedestrian-scale lighting, & shade** where feasible to add to the sense of place.

Destinations

- Heights Blvd Trail & MKT Trail
- White Oak Drive Commercial Corridor
- Transit Connections: 40 Telephone/Heights, 56 Airline/Montrose

Implementation Considerations

- Consider ways to use placemaking elements to improve the existing crossing at the MKT Trail to encourage trail users to use the improved crossing
- The ROW of White Oak Dr is 70’, E of Granberry St it is 60’. Its pavement width is 44’

P1 Greenway Placemaking & Wayfinding Program

Program Description

The goal of this project is to create a more accessible walking and biking experience for residents and visitors of the White Oak Bayou Greenway. This project clarifies the locations and connections along the growing trail network on White Oak Bayou, the MKT Trail, the Shepherd-Durham Corridor, the Spring Street Trail, Heights Boulevard, and Nicholson Street.

- **Focus signage on MKT Trail/Cottage Grove area.**
- Create visual cues and wayfinding posts to define directions at the **MKT Trail/White Oak Bayou Greenway junction.**
- Identify future areas of implementation to assist trail users.

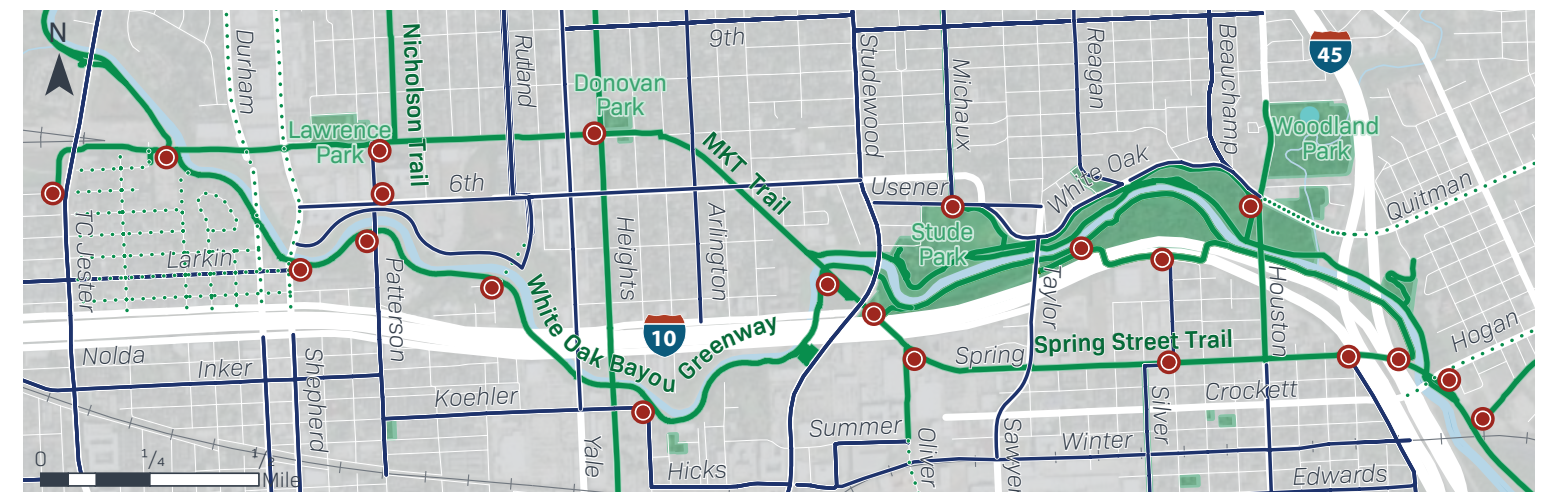


Figure 38. Wayfinding Example Locations (Source: Team Analysis, 2026)

Destinations

- Lawrence Park, Stude Park, Woodland Park, Donovan Park
- MKT Trail, White Oak Bayou Greenway, Spring Street Trail

Implementation Considerations

- Consideration of Stude Park Master Plan and TxDOT Interstate 10 Project to understand changing shared use path access and corridors

P2 Safe Routes to School Program

Program Description

Community members consistently highlighted **safe streets near schools as a major priority**, reflected in the plan's School Streets Goal. That goal is to "Create an environment in which students feel safe walking or biking to school by implementing safe street infrastructure initially within a 1/4-mile radius of all schools."

The roster of Project Recommendations takes this goal seriously, recommending projects within 1/4 mile of every school in the study area. Some projects, such as C6 Heights School Streets, G Ella Boulevard (S), M N Main Street, and P Studemont and Studewood Streets were selected in part for specific opportunities to improve safety for students at nearby schools.

Figure 39 lists each project within 1/4 mile of every public school in the study area. At a minimum, each school has two projects that provide direct impacts for students. Some schools, such as Heights High School, have four or more nearby projects. As school-focused funding opportunities arise, the TIRZ has several options available to improve safety near all schools in the Greater Heights.

Each project offers multiple opportunities to create safer crossing conditions, wider sidewalks, better lighting and shade, and other design options that encourage drivers to drive at or below the posted speed limit. Figure 40 shows an example of what some of these projects could look like along 13th Street near Love Elementary School.

Public School	Recommended Projects within ~1350 Feet
Browning Elementary	C3 - Heights to Buffalo Bayou Corridor, C6 - Heights School Streets, L - Michaux St, M - N Main St
Crockett Elementary	C3 - Heights to Buffalo Bayou Corridor, H - Edwards St, Q - Winter St Promenade
Field Elementary	C9 - 20th St Improvements, M - N Main St, P - Studemont and Studewood Sts
Hamilton Middle	C8 - Yale St Improvements, C9 - 20th St Improvements, B - Arlington St, O - Rutland St
Harvard Elementary	C6 - Heights School Streets, C8 - Yale St Improvements, B - Arlington St, T - White Oak D
Heights High	C5 - 14th St Reconstruction, C6 - Heights School Streets, C8 - Yale St Improvements, B - Arlington St, P - Studemont and Studewood Sts
Helms Elementary	C4 - Shady Acres Greenway, C9 - 20th St Improvements, O - Rutland St, S - W 19th St
Hogg Middle	C6 - Heights School Streets, L - Michaux St, P - Studemont and Studewood Sts
Love Elementary	C5 - 14th St Reconstruction, C6 - Heights School Streets
Sinclair Elementary	G - Ella Blvd (S) and Maxroy St, C1 - White Oak Connectivity Vision
Travis Elementary	C3 - Heights to Buffalo Bayou Corridor, C6 - Heights School Streets, M - N Main St

Figure 39. Projects Within 1/4 mile of a School



Figure 40. Recommended School Street Improvements along W 13th Street (Team Analysis, 2026)

P3 Safe Routes to Transit Program

Program Description

An important feature of the Greater Heights area is access to reliable and safe transit. Figure 41 and Figure 42 showcase the various bus routes that run through Greater Heights and the miles of roadway within a quarter mile of the bus routes. Figure 41 includes data on the miles of **existing sidewalks withing ½ mile of the various bus routes and the miles of sidewalks needed to make the sidewalk network complete** within ½ mile of the bus routes. Understanding the proximity and condition of sidewalks to bus routes allows for the opportunity to implement sidewalk repair and construction alongside METRO improvements to transit lines.

Bus Route Name	Miles of Roadway Within 1/2 Mile of Route	Miles of Existing Sidewalks Within 1/2 Mile of Route	Needed sidewalks for Complete Network Within 1/2 Mile of Route
44 Acres Home	40	52	28
56 Airline/Montrose	44	65	23
30 Ella Clinton	101	139	63
26 Long Point Cavalcade	49	62	36
66 Quitman	76	100	52
27 Shepherd	63	72	54
40 Telephone Heights	50	78	22
Greater Heights Total	182	241	123

Figure 41. Transit Lines in Study Area

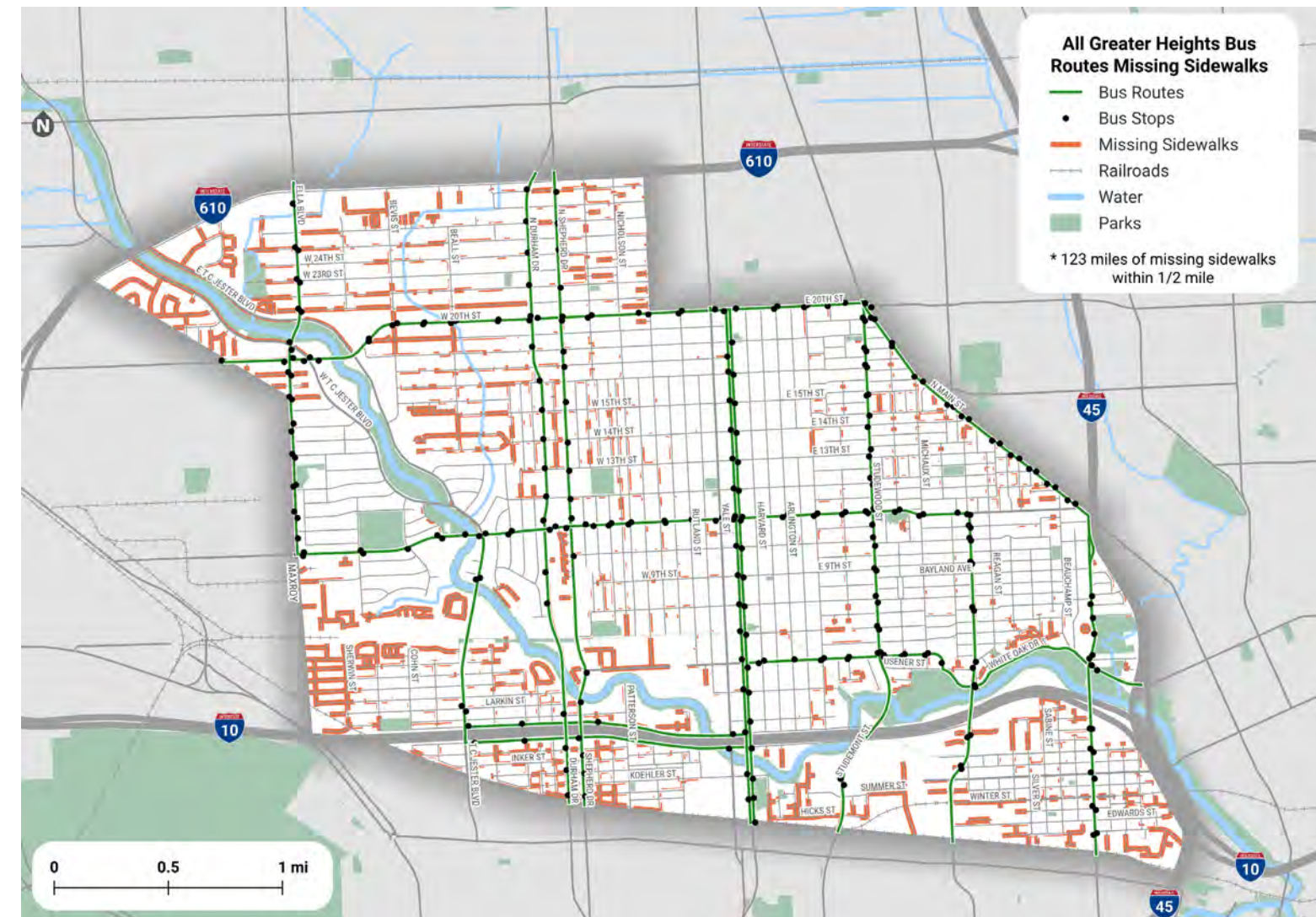


Figure 42. Transit Routes and Sidewalks Map (Source: Team Analysis, 2026)

Neighborhood Streets Framework

Street Typology

Neighborhood Streets are lower vehicle-volume streets designed so that people using all modes can share the street comfortably and safely. These streets are the basis of a neighborhood: they are the transitory stage between a front door and a shared community space. They are the stages on which neighbors greet each other, kids make friends and play in yards, and families and friends stroll together. Due to their comfort and safety, Neighborhood Streets are a place, not just a throughway for travel.

The *Neighborhood Streets Framework* is a tool for determining the appropriate projects an agency can implement to create Neighborhood Streets. The Framework identifies the big ideas of what Neighborhood Streets should be, taken from community conversations and goals. Then, the Framework filters big ideas through three ingredients and their on-street tools with a proven record of achieving those goals. Finally, all tools are sorted into three levels of investment to ensure that, even with constrained budgets, any agency can create a safe street for all.

Ingredients of a Neighborhood Street

Designing a shared street involves being clear, intentional, and safe. Neighborhood streets are places where vehicle traffic is slow, people walking and biking are visible, and the design of both the street and landscaping makes residents proud to live on their block.

Make it Safe

Designing for appropriate vehicle speeds creates conditions for all road users to feel safe and comfortable on their Neighborhood Street. Preventing unpredictable and unexpected movements on the road allows kids, families, and all residents to enjoy their space together. Safety strategies for Neighborhood Streets should consider including:

- Speed cushions
- Raised crossings & intersections
- Chicanes
- Access Management
- Curb extensions

Make it a Place

Neighborhood streets are an important community gathering space. Whether used for after-school family walks, a game of pick-up basketball, or a stroll to see Lights in the Heights with neighbors, the street is the stage for the show that is living in the Heights. Creating a sense of place on the street lends space for expression, care, and stewardship. The following features facilitate this strategy:

- Planters in curb extensions & chicanes
- Places to sit or gather
- Native vegetation plantings
- Mini-medians
- Public art

Make it Clear & Visible

Community feedback indicates a willingness to change the look and feel of streetscapes for the better, so long as the changes are easy to understand. Letting all users, especially those driving a car, know where to maneuver safely and where to yield increases safety and security for everyone in the neighborhood. To achieve this, Neighborhood Streets should include:

- Marked crosswalks
- Themed wayfinding
- Pedestrian level lighting
- Pavement markings
- Visible curb paint

Neighborhood Streets should...

- Be natural
- Be accessible
- Be a destination
- Be welcoming
- Be intentional
- Put people first



Rue Berri, Montréal, QC



St. Paul, MN via streets.mn



Wyandotte Street, Kansas City, MO via KCUR

Make it Safe Toolbox

Safe Streets Tools

When considering a Neighborhood Street, a wide variety of street interventions can be made to improve that street's safety by encouraging drivers to not go over the speed limit. Appropriate speeds give drivers time to see people walking, biking, and rolling and reduce the consequences of any potential conflict. The following are the key tools to use to achieve this condition.

This toolbox presents countermeasures allowable in the City of Houston's Infrastructure Design Manual (IDM). Each corridor will need further assessment for the best allowable and appropriate tool/countermeasure based on context and traffic. Corridors must go through the Neighborhood Traffic Management Program (NTMP) process before implementation (per IDM Section 15.2.11.B.2).

Chicanes

Mid-block deviations in the curb or travel lane (chicanes) narrow the street to encourage careful driving. The reclaimed space behind the curb returns street space for community use, such as for trees or gardens.



Bloomington, IN via Google Earth

Curb Extensions

Paint, bollards, posts, or concrete curbs extending into the roadway shorten crossing distances for people walking at intersections or at mid-block crossings, increasing visibility in areas where on-street parking is already prohibited.



Cincinnati, OH via WVXU

Raised Crossings

Similar to a speed cushion, raised crossings create a vertical obstacle to caution drivers of people crossing the street.



Houston Heights, TX via Heights MKT

Access Management

Access limits encourage safe speeds and direct through vehicle traffic to collectors and thoroughfares instead of speeding through local streets. Access to people walking and biking can be maintained with selectively permeable designs.



Québec City, QC via Google Earth

Speed Cushions

Speed cushions calm general traffic speeds and raise driver awareness. These can be designed with openings that allow the wheelbases of emergency vehicles and bicycles to pass uninterrupted.



Houston, TX via Houston Chronicle

Raised Intersections

The same idea of a raised crossing can be applied to an entire stop-controlled intersection in areas of high crossing volumes, such as near schools.



Woodhead Street, Houston, TX

Mini Traffic Circles

Mini traffic circles encourage vehicles to approach and cross intersections at appropriate speeds. They also create a space for community gardens, plantings, or monuments.



Vancouver, BC via PBIC

Speed Limits

Modifying local residential street speed limits is a well-understood manner to indicate calmer streets for drivers. Speeds below 30 mph can help reduce the chance of severe or fatal injury in cases of a crash.



Columbus, OH via Google Earth

Make it a Place Toolbox

Placemaking Tools

Pavement designs and patterns to either delineate driving space or just encourage careful driving give the community opportunities to take ownership and feel pride on its own street. With higher levels of investment, streets can go beyond paint to plantings and features that make the street a destination instead of just the path to a destination.

Pavement markings are represented in the City of Houston IDM and standards are outlined in that document.

Community Art

In all streetscape adjustments, the opportunity for neighbors to contribute to the aesthetic improvements of the space just beyond their front yard should be considered.



S Troy St, Chicago, IL via ANF Chicago

Planters

Plants enhance the appearance of the street in areas dominated by concrete or asphalt. New trees, shrubs, and garden beds with native plantings beautify the neighborhood while encouraging people to move slowly and deliberately.

Any design including box planters would need to be approved on a case-by-case basis as no existing standards exist in the City of Houston IDM for those planter types.



E 18th St, Kansas City, MO

Medians

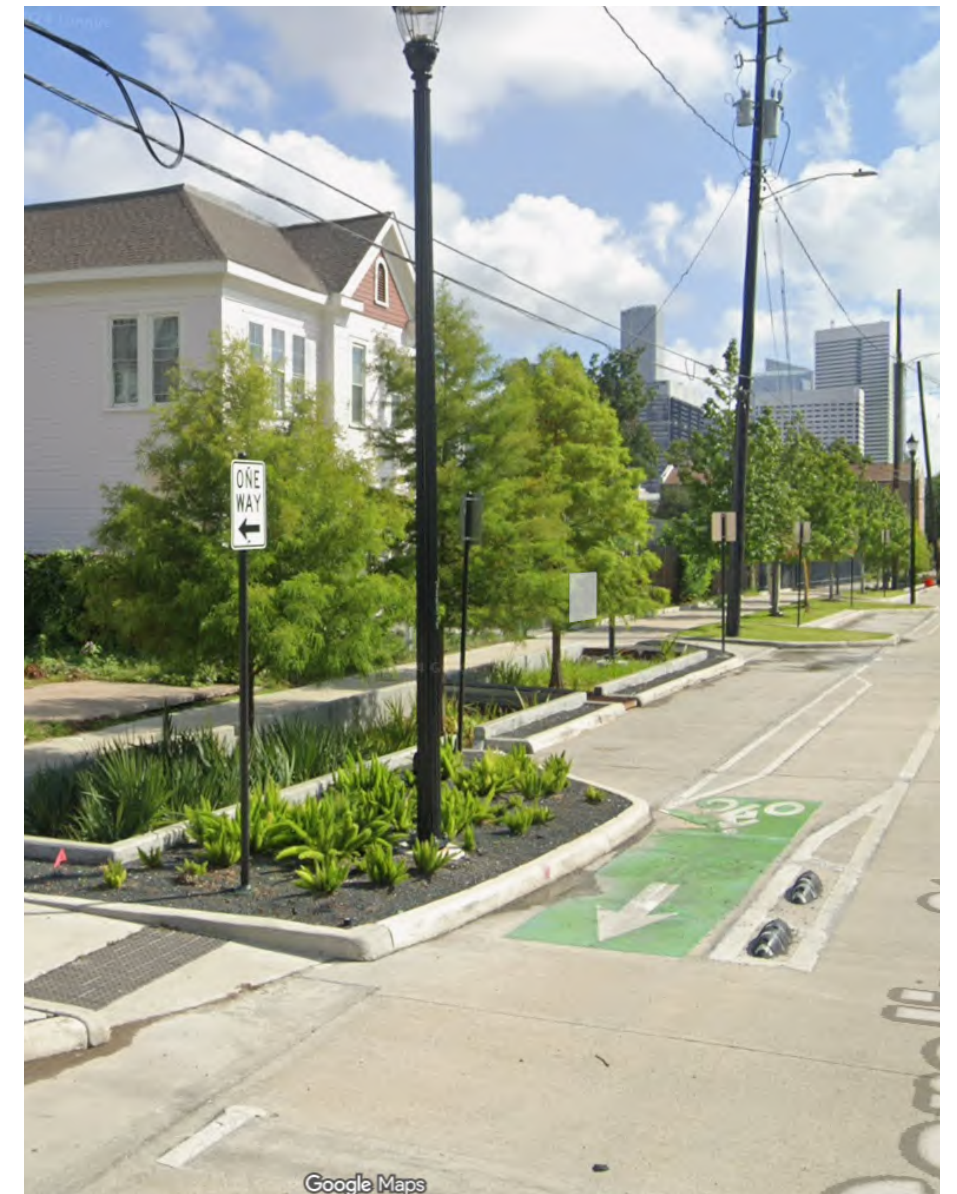
At entrances to the street from thoroughfares or collectors, small medians can channel vehicles from fast to appropriate speeds. Medians can serve as a gateway that add to a sense of place, announcing the community to its surroundings.



Salford St, Houston via Google Earth

Drainage Swales

Investment to upgrade and maintain open ditch drainage channels can create natural greenspaces that more efficiently collect runoff and improve the street's appearance year-round.



Google Maps

Caroline St, Houston via Google Earth

Make it Clear & Visible Toolbox

Safe Streets are Clear & Visible

When modifying streetscapes, it's important for all users to know what those changes are and what they mean for daily mobility. Clearly marking uses, potential conflicts, and rules of the road help remind everyone that these are shared spaces for everyone to enjoy. Community feedback has directly shown appreciation for clearly marked and painted upgrades.

On-Street Parking Boundaries

Delineating parking spaces helps to set expectations of all road users where to anticipate parked vehicles or people.



Oak Park, IL via Google Earth

Themed Wayfinding

Signs that orient people outside of cars to the active street network help assure them that they are on an improved street. Where permissible, alternate street signs can help designate these streets as improved spaces for all users.



Portland, OR via Bike Portland

Marked Crosswalks

Continental crosswalks with appropriately distanced stop bars are the most visible, the best practice, and the only recommendation for all pedestrian crossings.



Harvard St, Houston via Nearmap

Pavement Markings

High-visibility pavement markings that ascertain street use (such as sharrows) and permitted movements (such as on-street parking delineation and stop bars) encourage people to travel on the street responsibly.



Woodhead St, Houston via Nearmap

Sharrows

Permissive pavement markings indicate to all road users that people on bikes will be present. These markings assure people on bikes that they are on an appropriate bike route.



Salford Street, Houston via Google Earth

Pedestrian Lighting

Human-scale lighting increases visibility and the perception of safety for people walking and biking on active streets.



Rutland Street, Houston via Google Earth

Investment Levels

While budget priorities and timelines may not make every street a candidate to receive every improvement, there is a minimum set of upgrades that should be made for a local street to truly become a Neighborhood Street. Three Levels of Investment are proposed, where Benchmark streets contain the bare minimum to truly be considered Neighborhood Streets, while higher stages provide more safety and community benefits at higher investment levels.

★ Benchmark Neighborhood Streets

- A** Sidewalks on both sides (5' min. width, 6' preferred)
- B** Curb ramp improvements
- C** Speed cushions
- D** Shared lane pavement markings
- E** Neighborhood Street signage
- F** Active street-specific signs (R4-11, W11-1)
- G** High-visibility crosswalk markings along the entire corridor
 - Median refuge islands (not shown)
- I** Painted curb extensions, parking delineation, and chicanes
- J** Street trees where missing
 - Street lighting evaluation and improvement (not shown)

★★ Enhanced Neighborhood Streets

- All of *Benchmark* features, plus:
- Raised crosswalks (mid-block) at key locations
 - Concrete curb extensions
 - Stop sign optimization
 - Potted or box planters
 - Medians at neighborhood entrances
 - Open-ditch drainage improvements or conversion to vegetated swales

★★★ Gold Star Neighborhood Streets

- All of *Benchmark and Enhanced* features, plus:
- Median optimization for safer turning movements and to avoid turning conflicts
 - Access management
 - Mini traffic circles
 - Raised intersections
 - Concrete chicanes
 - Pedestrian-scale lighting

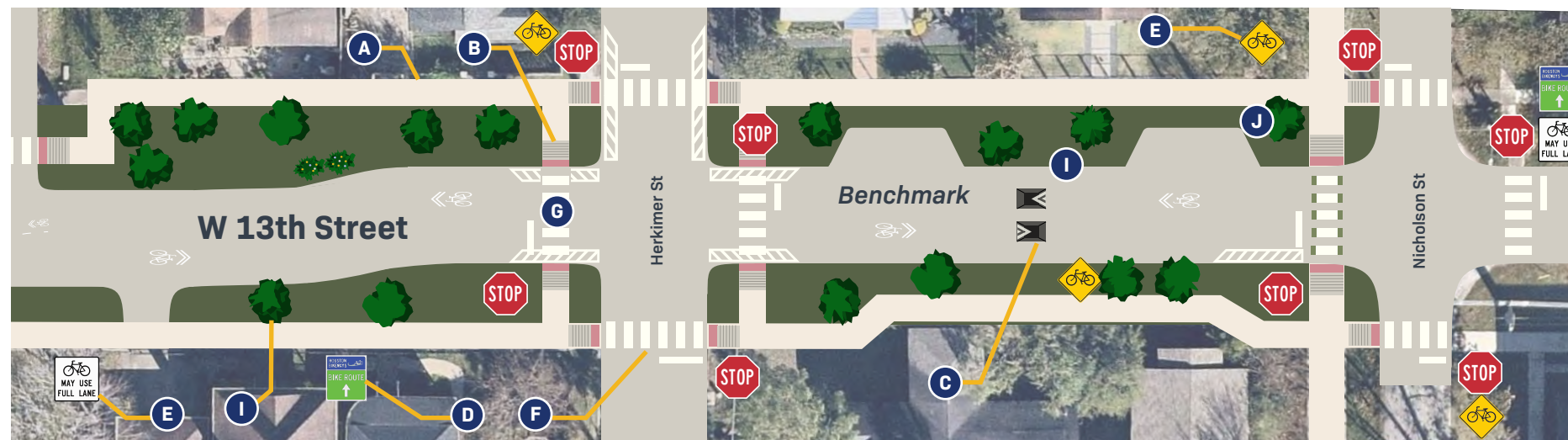


Figure 43. Example Benchmark Neighborhood Street - Blair Street to Nicholson Street (Source: Team Analysis, 2026)

Investment Levels

While budget priorities and timelines may not make every street a candidate to receive every improvement, there is a minimum set of upgrades that should be made for a local street to truly become a Neighborhood Street. Three Levels of Investment are proposed, where Benchmark streets contain the bare minimum to truly be considered Neighborhood Streets, while higher stages provide more safety and community benefits at higher investment levels.

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- High-visibility crosswalk markings along the entire corridor
- Median refuge islands
- Painted curb extensions, parking delineation, and chicanes
- Street trees where missing
- Street lighting evaluation and improvement

★★ Enhanced Neighborhood Streets

All of *Benchmark* features, plus:

- Ⓛ Raised crosswalks (mid-block) at key locations
- Ⓜ Concrete curb extensions
- Ⓝ Stop sign optimization
- Ⓞ Potted or box planters
 - Medians at neighborhood entrances (not shown)
- Ⓠ Open-ditch drainage improvements or conversion to vegetated swales

★★★ Gold Star Neighborhood Streets

All of *Benchmark and Enhanced* features, plus:

- Median optimization for safer turning movements and to avoid turning conflicts
- Access management
- Mini traffic circles
- Raised intersections
- Concrete chicanes
- Pedestrian-scale lighting

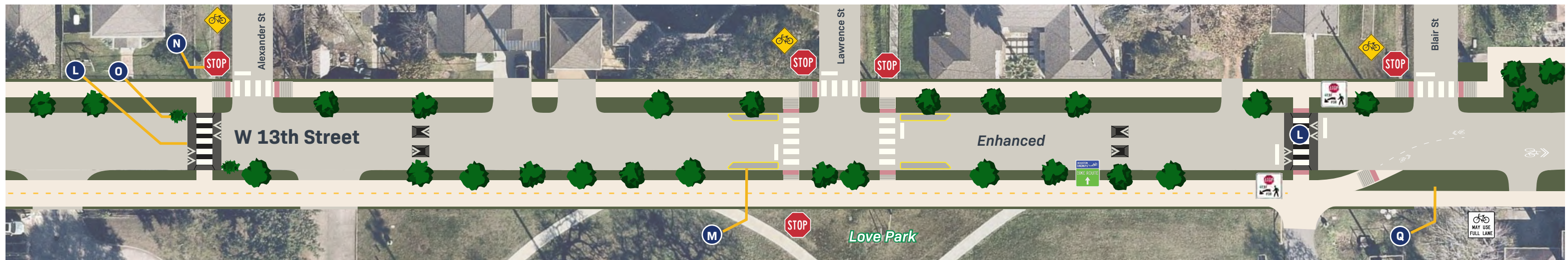


Figure 44. Example Enhanced Neighborhood Street - Alexander Street to Blair Street (Source: Team Analysis, 2026)

Investment Levels

While budget priorities and timelines may not make every street a candidate to receive every improvement, there is a minimum set of upgrades that should be made for a local street to truly become a Neighborhood Street. Three Levels of Investment are proposed, where Benchmark streets contain the bare minimum to truly be considered Neighborhood Streets, while higher stages provide more safety and community benefits at higher investment levels.

★ Benchmark Neighborhood Streets

- Sidewalks on both sides (5' min. width, 6' preferred)
- Curb ramp improvements
- Speed cushions
- Shared lane pavement markings
- Neighborhood Street signage
- Active street-specific signs (R4-11, W11-1)
- High-visibility crosswalk markings along the entire corridor
- Median refuge islands
- Painted curb extensions, parking delineation, and chicanes
- Street trees where missing
- Street lighting evaluation and improvement

★★ Enhanced Neighborhood Streets

All of *Benchmark* features, plus:

- Raised crosswalks (mid-block) at key locations
- Concrete curb extensions
- Stop sign optimization
- Potted or box planters
- Medians at neighborhood entrances
- Open-ditch drainage improvements or conversion to vegetated swales

★★★ Gold Star Neighborhood Streets

All of *Benchmark and Enhanced* features, plus:

- Median optimization for safer turning movements and to avoid turning conflicts (not shown)
- Access management (not shown)
- Ⓣ Mini traffic circles
- Raised intersections (not shown)
- Ⓥ Concrete chicanes
- Pedestrian-scale lighting (not shown)



Figure 45. Example Gold Star Neighborhood Street - N Shepherd Drive to Alexander Street (Source: Team Analysis, 2026)

Implementation Strategy

Chapter Five

The Greater Heights Strategic Connections Plan



MEMORIAL HEIGHTS
REDEVELOPMENT AUTHORITY

tei Planning
+ Design

TOOLE
DESIGN

Implementation Strategy

Outlining an Implementation Approach

This chapter outlines a practical implementation approach for funding, designing, and constructing the project recommendations identified as part of this Greater Heights Strategic Connections Plan. The information in this chapter is structured in four sections and intended to be used as a playbook and **reference guide for any implementing agency working on mobility safety and connectivity in the study area.**

Section 1: Glossary & Jurisdictions

The first section provides key context for the terms used in this chapter. It includes a glossary of terms and acronyms that are shown later in the chapter, as well as maps of (1) existing TIRZ boundaries, (2) City of Houston Council District boundaries, and (3) Harris County Commissioner Precinct boundaries.

Section 2: Implementation Table

The second section provides key implementation details for each project, including the project name, recommended phasing, project type, opportunities for quick-win early actions, implementation considerations, key partners and stakeholders, potential funding sources, and a planning-level cost estimate.

Section 3: White Oak Bayou Projects

The third section outlines a multi-pronged approach to implementing a set of projects specific to White Oak Bayou connectivity. When combined, these projects represent a reimagination of the bayou as a regional destination. This greenway-focused implementation strategy offers additional details to key community needs identified throughout the course of the planning effort.

Section 4: Safe Routes to Transit

The final section details an analysis showing the miles of missing sidewalks within 1/2 mile of each transit stop within the study area. This analysis can support future implementation of sidewalk and crossing improvements throughout the study area by METRO and others who use FTA funding.

Key Implementation Strategy Notes:

Together, these Project Recommendations represent a generational level of investment for projects that span an area well outside of the existing MHRA boundaries. As a result, **full implementation of these projects will take collaboration among multiple agencies and several years.** Each agency listed in this Implementation Strategy chapter is operating within the context of available funds and agency/jurisdiction priorities. This plan acts as a playbook for all agencies to identify and advance community-vetted projects that focus on safety and mobility in the Greater Heights. These projects are intended to complement, not supersede previous planning efforts such as the City of Houston Bike Plan.

Cost estimates are included as part of this chapter and **represent a planning-level estimate using the best available data from TxDOT and similar recent projects** at the time the plan was published. Estimates include an industry-standard 20% contingency.



In addition to implementing agencies that are able to fund and maintain projects, each of these projects has a range of key stakeholders that will be consulted during implementation such as Super Neighborhoods, civic associations, non-profits, and other community groups.

Section 1: Glossary & Jurisdictions

Agency & Funding Glossary

Various funding sources, agencies, and organizations are involved in plan implementation. The acronyms below are used throughout this chapter to identify these groups. Because this is a planning study, the organizations listed are not necessarily the confirmed funders of any specific project — rather, they represent parties who should be informed and engaged as projects move toward implementation.

Agencies & Organizations

- CPE - CenterPoint Energy
- COH - City of Houston
- COH Dist C - City of Houston Council District C
- COH Dist H - City of Houston Council District H
- GNMD - Greater Northside Management District
- HCFCD - Harris County Flood Control District
- HC Pct. 1 - Harris County Precinct 1, Office of Commissioner Rodney Ellis
- HC Pct. 2 - Harris County Precinct 2
- HC Pct. 4 - Harris County Precinct 4
- H-GAC Houston-Galveston Area Council
- HISD - Houston Independent School District
- HPB - Houston Parks Board
- METRO - METRO Houston
- MHRA - Memorial Heights Redevelopment Authority (TIRZ 5)
- SN - Super Neighborhood
- TIRZ - Tax Increment Reinvestment Zone
- TX-07 - 7th Congressional District
- TX-18 - 18th Congressional District
- TX-29 - 29th Congressional District
- TX-38 - 38th Congressional District
- TxDOT - Texas Department of Transportation
- UPRR - Union Pacific Railroad

Funding Sources

- BUILD - Better Utilizing Investments to Leverage Development (previously RAISE, TIGER)
- COH CIP - City of Houston Capital Improvement Plan
- CPF - Community Project Funding for appropriation of available Congressional funds
- CRISI - Consolidated Rail Infrastructure and Safety Improvements
- FTA 5310 - Federal Transit Agency Enhanced Mobility for Seniors and Individuals with Disabilities
- SRTS - Safe Routes to School
- SS4A - Safe Streets 4 All
- H-GAC TASA - H-GAC Transportation Alternatives Set Aside Funds
- H-GAC Safety Funding - Additional mobility safety funding from H-GAC
- H-GAC STBG/CMAQ - Surface Transportation Block Grant and Congestion Management/Air Quality funding from H-GAC for roadway projects
- HSIP - Highway Safety Improvement Program
- TPWD RTP - Texas Parks & Wildlife Recreational Trails Program
- TxDOT TA - TxDOT Transportation Alternatives funds



Section 1: Glossary & Jurisdictions

TIRZ Boundaries

Figure 1 displays the jurisdictions of the various TIRZs within the study area and the recommended projects of the plan.

- TIRZ 3**
Main Street/Market Square
- TIRZ 5**
Memorial Heights
- TIRZ 12**
City Park
- TIRZ 13**
Old Sixth Ward
- TIRZ 16**
Uptown
- TIRZ 21**
Hardy/Near Northside

LEGEND

 Catalyst Project	 Active Streets & Trails	 Water
 Other Project	 Programmed Projects	 Parks
 Key Bayou Crossing Improvement	 Railroads	

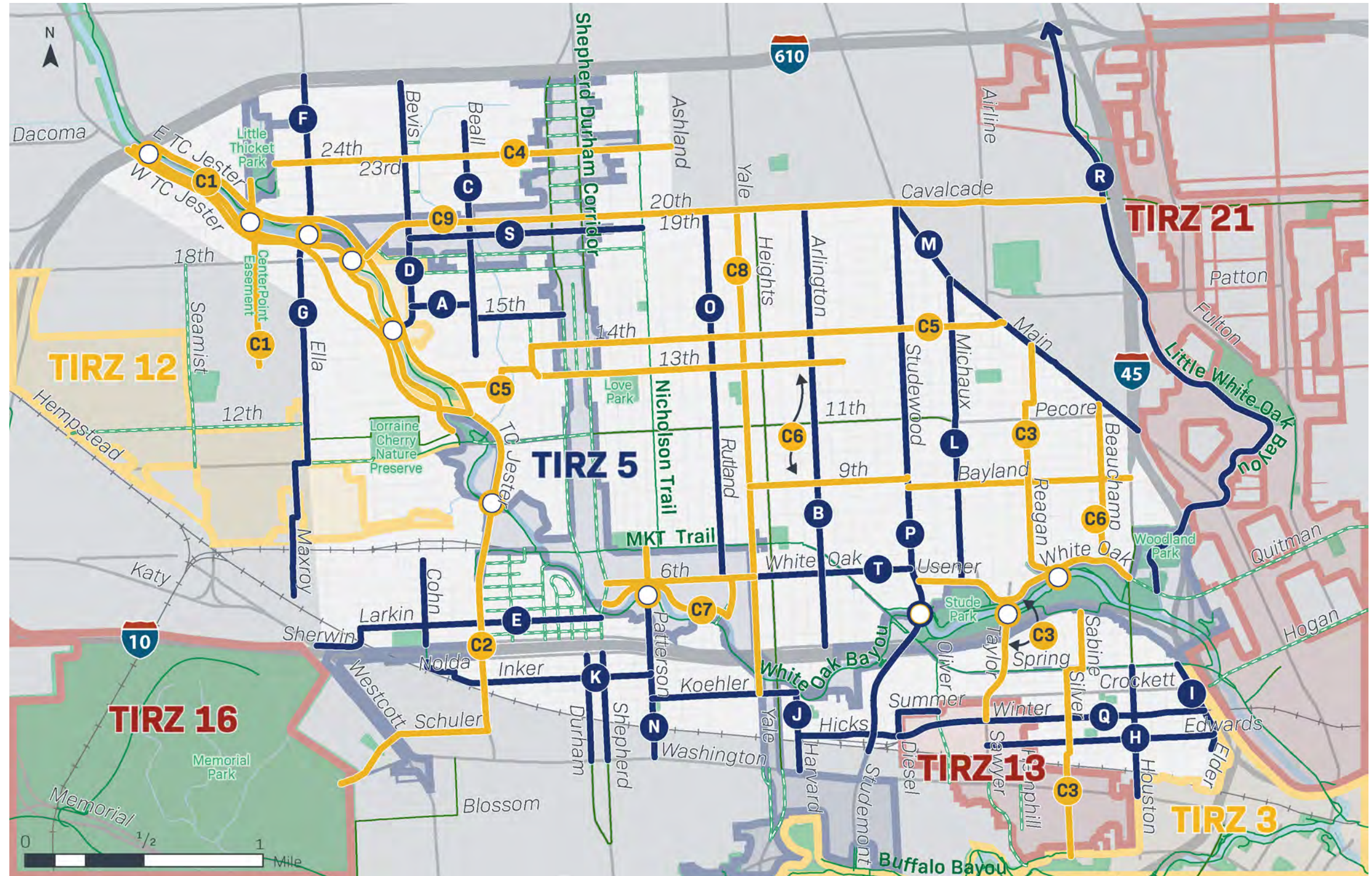


Figure 1. TIRZ Boundaries Overlapping the Study Area & Recommended Projects (Source: Team Analysis, 2026)

Section 1: Glossary & Jurisdictions

City of Houston Council Districts

Figure 2 displays the boundaries of the two City of Houston Council Districts that overlap the study area, District C and District H.

LEGEND

- Catalyst Project
- Other Project
- Key Bayou Crossing Improvement
- Active Streets & Trails
- Programmed Projects
- Water
- Parks
- + Railroads

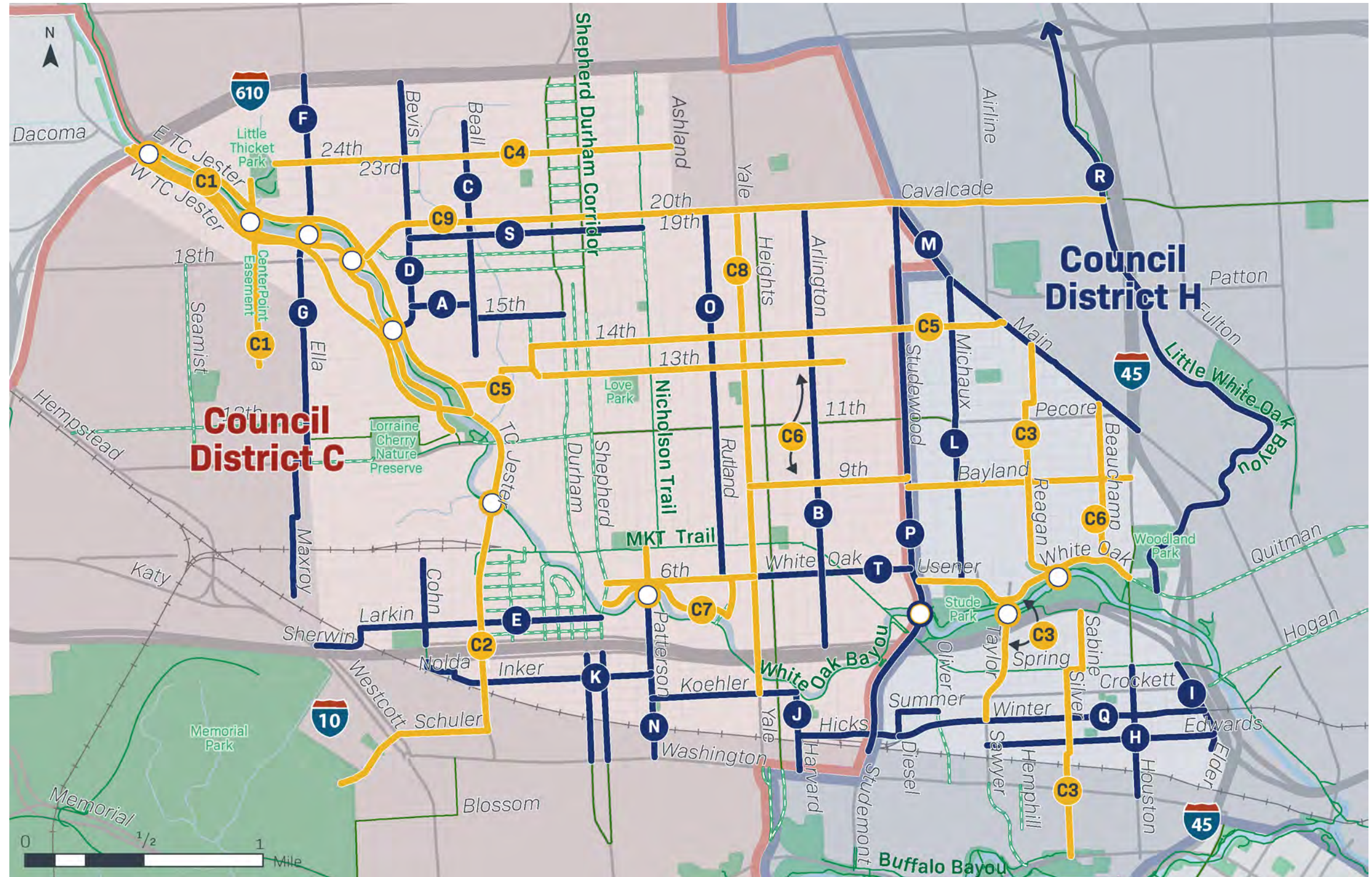


Figure 2. City of Houston Council Districts Overlapping the Study Area & Recommended Projects (Source: Team Analysis, 2026)

Section 1: Glossary & Jurisdictions

Harris County Commissioner Precincts

Figure 3 displays the boundaries of the three Harris County Commissioner Precincts that overlap the study area, Precincts 1, 2, and 4.

LEGEND

- Catalyst Project
- Other Project
- O Key Bayou Crossing Improvement
- Active Streets & Trails
- Programmed Projects
- Water
- Parks
- Railroads

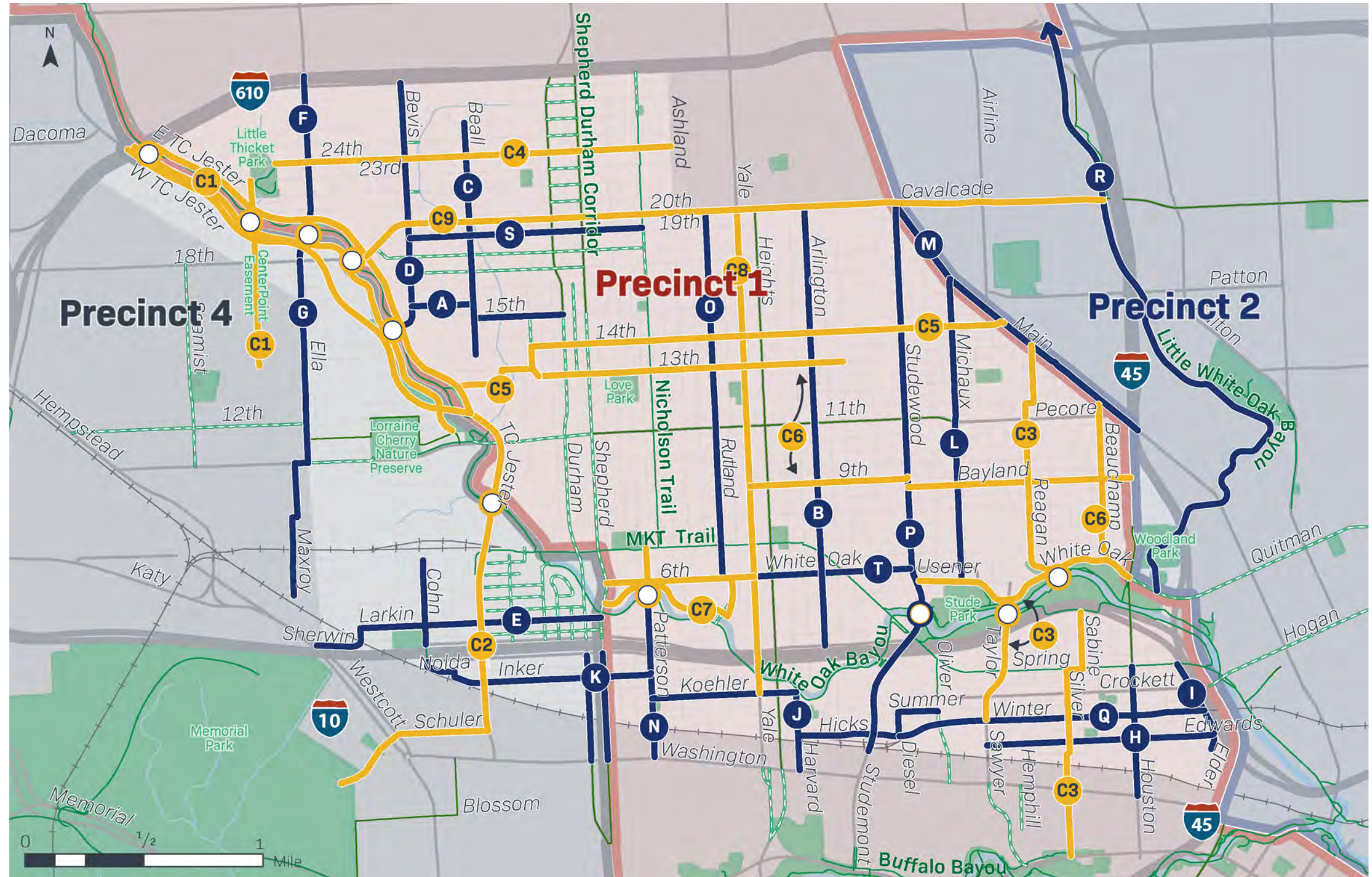


Figure 3. Harris County Precincts Overlapping the Study Area & Recommended Projects (Source: Team Analysis, 2026)

Section 1: Glossary & Jurisdictions

United States Congressional Districts

Figure 4 displays the boundaries of the three United States Congressional Districts overlapping the study area: Texas' 7th, 18th, 29th, and 38th Congressional Districts.

LEGEND

- Catalyst Project
- Other Project
- C Key Bayou Crossing Improvement
- Active Streets & Trails
- Programmed Projects
- Water
- Parks
- + Railroads

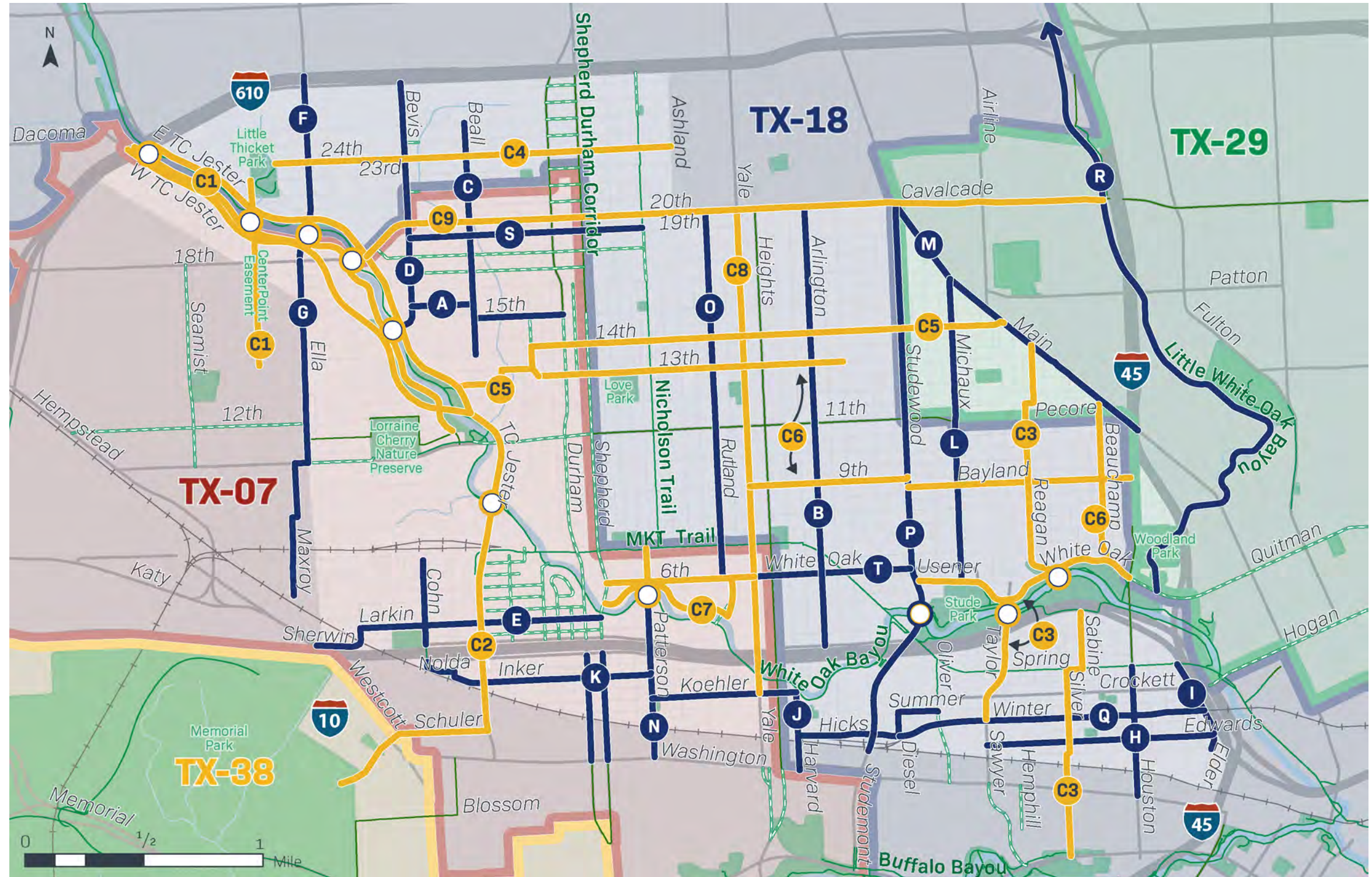


Figure 4. U.S. Congressional Districts Overlapping the Study Area & Recommended Projects (Source: Team Analysis, 2026)

Section 2: Implementation Table

This table describes sample details that can be included in each cell for the Implementation Strategy table on the following pages. The information below is provided for each recommended project.

ID	Project Name	Phase	Project Type	Early Action Opportunities	Implementation Considerations	Potential Implementing Agencies <i>ROW owner = Bold</i>	Potential Funding	Cost Estimate <i>Planning-Level (2026 dollars)</i>		
								<i>Design</i>	<i>Construct</i>	<i>Total</i>
ID #	Full project name, including with any noted phasing	One of 3 Possible Phases: Near Term/ Early Action (0-3 years) Mid-Term Capital Dependent (3-7 years) Long Term/ Partnership Dependent (7+ years)	Could include: > Striping, signage, curb ramp upgrades > Sidewalk repair, replace, install (without drainage conversion) > Defining travel lanes > Trail connections > Corridor reconstructions > Drainage conversion > Intersection & crossing improvements > TxDOT coordination > Bridges	Ideas for potential quick win type investments, often with lower level of investments and shorter potential timelines These Early Action Opportunities may help MHRA and other key players advance critical safety and connectivity improvements in the near term while working toward full implementation of each project.	Specific parameters to consider, including: > Drainage conversion (from open ditch to curb-and-gutter) > Other drainage considerations > ROW constraints > TxDOT coordination > Rail coordination > School operations	List of key agencies who could play some role in funding, designing, and implementing these projects. The bold names in each list represent the agency who owns the right-of-way of the project.	A list of potential local, regional, state, and federal funding sources	<i>Design</i>	<i>Construct</i>	<i>Total</i>

Key notes about the cost estimates:

- All costs are planning-level estimates using 2026 dollars at the time of plan publishing.
- Costs are estimated using TxDOT’s publicly available bid item averages for recent projects as well as TEI’s documentation of design and construction costs for recent, local projects in Harris County with similar conditions and recommended improvements.
- Construct = Base Construction Total + (Base Construction Total x 10% Mobilization Factor) where Base Construction Total = Construction Estimates x 20% Contingency Factor
- Design = Base Construction Total x 20% Design Factor
- Any discrepancy between the sum of the Design and Construct cost estimate columns and the Total cost estimate column should be attributed to rounding error.
- The Grand Total of all projects includes multiple Yale Street projects options for both the short and long term (Striping, Retrofit, and Reconstruction). Each of these options were priced individually, not cumulatively.
- Cohn/Larkin/Sherwin Streets will be reconstructed as part of the existing City of Houston Cottage Grove East and Cottage Grove West projects. The cost estimates shown here assume roadway, sidewalk, and drainage work will be conducted as part of these City of Houston projects. Project costs will include Neighborhood Street elements, signage, and striping.
- The full scale and extent of Little White Oak Bayou improvements will be determined in more detail as part of future planning efforts to identify specific plans for the greenway. The cost estimate for Little White Oak Bayou Extension only includes trail pavement and underpasses from White Oak Drive to Cavalcade Street. The final cost estimate for this project will depend on many additional factors determined through additional planning and pre-engineering. These will likely include costs for erosion and sediment control, updates to stormwater utilities, landscaping and placemaking, and others. These additional considerations will likely increase the final cost estimate for this project.

Section 2: Implementation Table

ID	Project Name	Phase	Project Type	Early Action Opportunities	Implementation Considerations	Potential Implementing Agencies <i>ROW owner = Bold</i>	Potential Funding	Cost Estimate <i>Planning-Level (2026 dollars)</i>		
								Design	Construct	Total
C1	White Oak Connectivity Vision - Phase 1 (Road Crossings & Bridge Feasibility) (Pg. 79)	Near Term/ Early Action	> Striping, signage, curb ramp upgrades > Sidewalk repair, replace, install (without drainage conversion) > Trail connections > Bridge feasibility studies	> High visibility crosswalks, curb extensions, and median refuges (example intersection: W TC Jester Boulevard and Brooktree Drive) > Formalize desire paths along bayou	> Drainage considerations when operating in HCFCD ROW	HPB, MHRA, TIRZ 12, HC Pct 1, HC Pct 4, CPE, HCFCD, COH , TxDOT, METRO	TIRZ 5, TIRZ 12, federal BUILD grants, HSIP, County Partnership (P1, P4), TxDOT TA, TPWD RTP	Design \$4.9M	Construct \$27.0M	Total \$31.9M
	White Oak Connectivity Vision - Phase 2 (New Greenways & Improved Bridges)	Mid-Term Capital Dependent	> Intersection & crossing improvements > Little Thicket Park connection study	> Intersection & crossing improvements (Ella/18th/W TC Jester) > Trail connections (CPE, west bank of bayou)	> TxDOT coordination > Rail coordination > School operations					
	White Oak Connectivity Vision - Phase 3 (New Bridges & Roadway Redesign)	Long Term/ Partnership Dependent	> New bridge construction > Cantilever existing bridges > Little Thicket park connection		> Drainage conversion > TxDOT coordination > Rail coordination > Bridge costs > Coordination with CenterPoint easements > Potential floodplain constraints					
C2	White Oak to Memorial Park Connection - Phase 1 (Schuler Street Improvements & Targeted Intersection Improvements) (Pg. 82)	Near Term/ Early Action	> Striping, signage, curb ramp upgrades	> Improvements on Schuler Street (upgrade to a Neighborhood Street) > Schuler and Westcott crossing improvements (underway) > High visibility crosswalks, curb extensions, and median refuges (example intersection: TC Jester and Larkin, TC Jester and Petty; connectivity improvements to Cottage Grove Park)	> School operations	MHRA, HC Pct 4, COH, TxDOT , Memorial Park Conservancy, UPRR, HPB, TX-07, TX-39	TIRZ 5, County Partnership (P4), TxDOT TA, Parks bonds, H-GAC TASA, CPF	Design \$2.4M	Construct \$13.4M	Total \$15.9M
	White Oak to Memorial Park Connection - Phase 2 (TC Jester Blvd shared-use path/wide sidewalk & remaining Improvements)	Mid-Term Capital Dependent	> Intersection and crossing improvements > Bridge cantilevering feasibility study	> Intersection & crossing improvements (curb extensions, close slip lanes) > Sidewalk widening and shared-use path construction to connect to MKT Trail > Vertical separation for people walking and bicycling on bridges	> Drainage conversion > TxDOT coordination > Rail coordination > School operations > Bridge costs					

ID	Project Name	Phase	Project Type	Early Action Opportunities	Implementation Considerations	Potential Implementing Agencies <i>ROW owner = Bold</i>	Potential Funding	Cost Estimate <i>Planning-Level (2026 dollars)</i>		
								Design	Construct	Total
C3	Heights - Buffalo Bayou Corridor - Phase 1 (Reagan St, Sabine St, Silver St, Taylor St, & White Oak Dr improvements) (Pg. 84)	Near Term/ Early Action	> Striping, signage, curb ramp upgrades > Trail connections	> High visibility crosswalks, striping, curb extensions, and curb ramps at key intersections/ crossing along White Oak Drive (Sabine, Silver, Reagan Streets from I-10 to the UPRR rail line >>(existing grant application) > Upgrade Reagan Street to a Neighborhood Street > Taylor Street shared-use path from White Oak Bayou to Spring Street Trail > Wayfinding for bayou trail(s), Spring Street Trail, and Stude Park	> TxDOT coordination > Drainage conversion > Community support > Bridge connection over White Oak Bayou	HPB, MHRA (White Oak Dr), TIRZ 13, TxDOT, COH, HCFCD , METRO, HC Pct 1, GNMD	Parks bonds, COH CIP, County Partnership (P1), FTA 5310, TIRZ 13 (Silver St, Sabine St, Taylor St), TIRZ 5 (White Oak Dr & Usener St)	Design \$3.9M	Construct \$21.6M	Total \$25.5M
	Heights - Buffalo Bayou Corridor - Phase 2 (Usener St & Bridges)	Mid-Term Capital Dependent	> Bridge cantilevering feasibility study > Corridor reconstruction	> Reconstruction at Usener Street	> Drainage conversion > ROW constraints > TxDOT coordination > Rail coordination					
C4	Shady Acres Greenway (Pg. 86)	Near Term/ Early Action	> Striping, signage, curb ramp upgrades > Intersection and crossing improvements > Corridor reconstruction (site-specific) > Drainage conversion	> COH Dist C funds identified for HAWK/RRFB at 22nd/23rd > Striping, signage, curb ramps (Ashland to Durham) > Drainage conversion (Durham to Ella and Ella to Little Thicket Park) > Corridor redesign to balance parking, shared use path installation, and corridor traffic management	> Drainage conversion from open ditch to curb and gutter > Utility relocation and coordination > Project may move to 22nd or 23rd (depending on project funds allocated to ped signalization)	MHRA, COH , COH Dist C, HC Pct 1, HCFCD, METRO	TIRZ 5, SS4A, TxDOT TA, FTA 5310, H-GAC Safety Funding	Design \$4.4M	Construct \$24.4M	Total \$28.9M
C5	14th Street Reconstruction (Pg. 88)	Mid-Term/ Capital Dependent	> Intersection and crossing improvements > Corridor reconstruction > Shared use path construction > Sidewalk replace and repair > Drainage conversion	> Wayfinding to White Oak Bayou & Nicholson Street > High visibility crosswalks and curb extensions at key intersections (ex: Blair and Studewood Streets) > School access improvements to include traffic management, improved crossings and formalization of parking to clarify drop off/pick up operations	> Drainage conversion in select locations > Potential utility coordination and relocation > School operations	MHRA, HC Pct 1, COH , GNMD, METRO, HPB	SS4A, SRTS, TIRZ 5, TxDOT TA, FTA 5310, HSIP, STBG/ CMAQ	Design \$6.0M	Construct \$33.1M	Total \$39.1M
C6	Heights School Streets (Pg. 91)	Near Term/ Early Action	> Striping/signage > Reconstruction > School access improvements	> Improved crossings and increased access/ connectivity near Love Elementary School, Travis ES, Harvard ES, Heights HS, and Hogg MS (Bayland at Norhill) > Utility easement crossing at the west end of 13th >Speed cushions along Prince St near 13th > Construct shared-use path along 13th from Shepherd to Herkimer/Nicholson	> Striping, signage, curb ramps, utility easement > Requires coordination with several schools; potential pilot corridor treatments	TX-07, TX-18, TX-29, HC Pct 1, COH , HISD, TxDOT, METRO	SRTS, SS4A, County Partnership (P1), CPF, FTA 5310, H-GAC Safety Funding, TIRZ 5 (13th St)	Design \$2.4M	Construct \$13.3M	Total \$15.7M

ID	Project Name	Phase	Project Type	Early Action Opportunities	Implementation Considerations	Potential Implementing Agencies <i>ROW owner = Bold</i>	Potential Funding	Cost Estimate <i>Planning-Level (2026 dollars)</i>		
								Design	Construct	Total
C7	MKT Bird Sanctuary Loop (Pg. 93)	Mid-Term/ Capital Dependent	> Trail expansion and loop connectivity > Bridge feasibility/construction	> Wayfinding and signage, public art and other placemaking efforts > Rutland trail connection > North bank bayou trail (whole or part) > Connect from MKT Trail to 6th St along M-K-T	> TxDOT coordination, community support > Environmental considerations within bayou corridor; potential bridge connections	HPB, MHRA, TxDOT , HCFC , COH , METRO, HC Pct 1	TPWD RTP, Park bonds, TIRZ 5, County Partnership (P1), TxDOT TA	Design \$2.8M	Construct \$15.6M	Total \$18.4M
C8	Yale Street Improvements (Yale & 7th Intersection) (Pg. 97)	Near Term/ Early Action	> Intersection and crossing improvement	> Trail realignment > Relocated bike parking > Gateway and placemaking opportunities	Coordination with trail operators and local community organizations for design and programming	MHRA, HC Pct 1, COH	TIRZ 5, County Partnership (P1), COH District funds, CPF	Design \$1.5M	Construct \$8.5M	Total \$10.0M
	Yale Street Improvements (Striping) (Pg. 96)	Near Term/ Early Action	> Striping, signage, and curb ramp upgrades	> Striping, high-visibility crosswalks > Curb ramp upgrades > Defining travel lanes > Pedestrian crossing signals (ex: HAWK at Yale and 12th Street)		HC Pct 1, COH , TxDOT, METRO	County Partnership (P1), HSIP, SS4A, FTA 5310, BUILD, H-GAC Safety Funding, STBG/CMAQ, CPF			
	Yale Street Improvements (Retrofit) (Pg. 98)	Mid-Term/ Capital Dependent	> Corridor retrofit	> Define travel lanes	> Potential utility coordination and relocation			Design \$1.5M	Construct \$8.5M	Total \$10.0M
	Yale Street Improvements (Reconstruction) (Pg. 98)	Long Term/ Partnership Dependent	> Corridor reconstruction		> Drainage, potential utility relocation and ROW acquisition, community support > Major reconstruction with traffic operations, streetscape, and business access considerations > High-crash corridor > Reconstruction may require phased approach (near and mid-term improvements before reconstruction) > Traffic operations considerations during reconstruction			Design \$6.5M	Construct \$35.8M	Total \$42.3M
C9	20th Street Reconstruction (Pg. 99)	Near Term/ Early Action	> Commercial corridor reconstruction	> Design currently underway		METRO, COH , HC Pct 1, HC Pct 2, TxDOT, MHRA	TIRZ 5, County Partnership (P2), H-GAC TIP, STBG/CMAQ	Design \$10.3M	Construct \$56.8M	Total \$67.1M
A	W 15th/W 15 1/2 Streets (Pg. 101)	Near Term/ Early Action	> Striping, signage, curb ramp upgrades (including advisory sidewalk over the Turkey Gully bridge) > Sidewalk repair, replace, and install (without conversion to curb & gutter drainage)	> High-visibility crosswalks (Intersection at Beall St and Bevis St) > Painting/striping, including of the Turkey Gully bridge > Shared-use path along the SPJST Ball Fields on the south side of W 15th St	> Low-cost retrofit opportunity > A fully ADA-compliant sidewalk across the Turkey Gully bridge would likely require reconstruction of the bridge	MHRA, COH , HCFC, TIRZ 12, HC Pct. 1	TIRZ 5, COH District funds, CPF	Design \$393K	Construct \$2.2M	Total \$2.6M

ID	Project Name	Phase	Project Type	Early Action Opportunities	Implementation Considerations	Potential Implementing Agencies <i>ROW owner = Bold</i>	Potential Funding	Cost Estimate <i>Planning-Level (2026 dollars)</i>		
								Design	Construct	Total
B	Arlington Street (Pg. 102)	Near Term/ Early Action	<ul style="list-style-type: none"> > Striping, signage, curb ramp upgrades (including defining travel lanes) > Sidewalk repair, replace, and install (without conversion to curb & gutter drainage) > Intersection & crossing improvements (curb extensions & mid-block crossings near Heights HS) 	<ul style="list-style-type: none"> > Signage and wayfinding near MKT > MKT @ Arlington sidewalk connections > School crossings near Heights HS 	<ul style="list-style-type: none"> > Existing drainage may impact what is possible - the drainage type changes along the corridor > School operations at Heights HS and Immanuel Lutheran School > Corridor contains a high number of established trees and existing parking encroachments 	COH , HC Pct 1, TxDOT, HISD, METRO	SS4A, SRTS, COH CIP, FTA 5310, H-GAC Safety Funding, CPF	Design \$443K	Construct \$2.4M	Total \$2.9M
C	Beall Street (Pg. 102)	Mid-Term Capital Dependent	<p>N of 20th:</p> <ul style="list-style-type: none"> > Striping, signage, and curb ramp upgrades > Sidewalk repair > Replace and install > Define travel lanes > Mill & overlay <p>S of 20th:</p> <ul style="list-style-type: none"> > Corridor reconstruction (including shared-use path, intersection/crossing improvements) > Drainage conversion 	<ul style="list-style-type: none"> > Painting/striping, including defining travel lanes & curb extensions > Sidewalk repair and curb ramp upgrades > Shared-use path along the SPJST Ball Fields on the east side of Beall St 	<ul style="list-style-type: none"> > Drainage conversion to curb & gutter > Potential ROW constraints 	MHRA, COH , HCFCD, HC Pct 1, METRO	TIRZ 5, CPF	Design \$1.4M	Construct \$7.7M	Total \$9.2M
D	Bevis Street (Pg. 103)	Mid-Term Capital Dependent	<p>N of 26th:</p> <ul style="list-style-type: none"> > Corridor reconstruction (including intersection/crossing improvements) > Drainage conversion <p>S of 20th:</p> <ul style="list-style-type: none"> > Corridor reconstruction (including shared-use path, intersection/crossing improvements) > Drainage conversion > Other (pedestrian-scale lighting) 	<p>N of 26th:</p> <ul style="list-style-type: none"> > Painting & striping, including defining travel lanes, curb extensions, and high-visibility crosswalks (around intersection with I-610) > Curb extensions <p>S of 20th</p> <ul style="list-style-type: none"> > Sidewalk and curb ramp repairs 	<ul style="list-style-type: none"> > Drainage conversion to curb & gutter N of 26th St > White Oak Bayou Greenway connection potential 	TIRZ 12, MHRA, COH , HC Pct 1, HPB, HCFCD, METRO	TIRZ 12, TIRZ 5, TxDOT TA, FTA 5310, STBG/CMAQ	Design \$2.4M	Construct \$13.4M	Total \$15.9M
E	Cohn/Larkin/ Sherwin Streets (Pg. 103)	Near Term/ Early Action	<ul style="list-style-type: none"> > Signage, striping > Intersection & crossing improvements (particularly at TC Jester Blvd and Shepherd Dr/ Durham Dr) > Other (pedestrian-scale lighting) 	<ul style="list-style-type: none"> > Trail connection from MKT Trail terminus to Cohn St > Painting & striping, including high-visibility crosswalks along Cohn St and Larkin St 	<ul style="list-style-type: none"> > TxDOT and COH coordination with existing projects > Drainage conversion to curb & gutter > Supports Memorial Park connection 	COH , TxDOT, HC Pct 4, UPRR, Fenway Development, Inc. , METRO	COH CIP, TxDOT Inner Katy Project, CPF	Design \$104K	Construct \$571K	Total* \$675K

*Cohn/Larkin/Sherwin Streets will be reconstructed as part of the existing City of Houston Cottage Grove East and Cottage Grove West projects. The cost estimates shown here assume roadway, sidewalk, and drainage work will be conducted as part of these City of Houston projects. Project costs will include Neighborhood Street elements, signage, and striping.

ID	Project Name	Phase	Project Type	Early Action Opportunities	Implementation Considerations	Potential Implementing Agencies <i>ROW owner = Bold</i>	Potential Funding	Cost Estimate <i>Planning-Level (2026 dollars)</i>		
								Design	Construct	Total
F	Ella Blvd (N) (Pg. 104)	Long Term/ Partnership Dependent	> Major reconstruction (including shared-use path, intersection/crossing improvements) > TxDOT coordination > Bridge cantilevering (also part of Project C1)	> High-visibility crosswalks, including at White Oak Bayou Greenway connection at E TC Jester intersection > COH Dist C crossing improvements at 22nd/23rd > Wayfinding/signage to Little Thicket Park and White Oak Bayou Greenway	> Drainage considerations to White Oak Bayou > Reconstruction may require utility relocation > 18th St intersection also part of Project C1	MHRA, HC Pct 1, COH , TxDOT, HPB, METRO	TIRZ 5, County Partnership (P1), METRO General Fund, FTA 5310, STBG/CMAQ	Design \$2.9M	Construct \$16.2M	Total \$19.1M
G	Ella Blvd (S) & Maxroy Street (Pg. 104)	Long Term/ Partnership Dependent	> Major reconstruction (including shared-use path, intersection/crossing improvements) > UPRR coordination	> High-visibility crosswalks (all intersections), especially Grovewood at Ella > Improvements at Ella/11th - short term striping, curb ramps, crosswalk improvements > Improvements for connection along W 11th St from Maxroy St to Ella Blvd	> Reconstruction would likely require utility relocation > School operations at Sinclair ES > TIRZ 12 coordination > Rail coordination	TIRZ 12, COH , HC Pct 4, METRO, UPRR	TIRZ 12, SRTS, FTA 5310, STBG/CMAQ	Design \$4.1M	Construct \$22.6M	Total \$26.7M
H	Edwards Street & Houston Avenue (Pg. 105)	Long-Term/ Partnership Dependent	>Major Reconstruction (Houston Avenue) > Striping, signage, curb ramp upgrades (Edwards Street) > Sidewalk repair, replace, install (without drainage conversion) > Intersection & crossing improvements > Define travel lanes	> Signal warrant analysis for Edwards St at Houston Ave > Improved striping at Edwards St at Houston Ave > Mid-block crossing improvements along Edwards St > Formalize pedestrian refuge at Edwards St and Sawyer St > Sidewalks, striping at Edwards/Silver intersection > Access management, back-in angle parking	> Align with prior improvements and plans for the corridor	TIRZ 13, METRO, HC Pct 1, COH	TIRZ 13, H-GAC TASA, FTA 5310	Design \$2.5M	Construct \$14.0M	Total \$16.5M
I	Elder Street Trail (Pg. 105)	Mid-Term Capital Dependent	> Trail connections > TxDOT coordination > UPRR coordination	> If constructed near term: Signage and wayfinding at Bayou and MKT Trail connection points	> TxDOT coordination as part of NHHIP project > Rail coordination	TxDOT, COH , TIRZ 13, HC Pct 1, TIRZ 3	TxDOT NHHIP Project, TPWD RTP	Design \$325K	Construct \$1.8M	Total \$2.1M
J	Harvard/Hicks/Diesel/Summer Streets (Pg. 106)	Mid-Term Capital Dependent	> Striping, signage, curb ramp upgrades > Sidewalk repair, replace, install (without drainage conversion) > Intersection & crossing improvements > Define travel lanes > Other (pedestrian-scale lighting)	> Sidewalks along Hicks St (east of Studemont St) and Diesel St > Curb extensions and high-visibility crosswalks at (1) Diesel St at Summer St (2) Summer St at Oliver St, and (3) Harvard St at Washington Ave > Pedestrian-scale lighting (near Summer St and Oliver St intersection) > Wayfinding to White Oak Bayou Greenway	> Drainage conversion to curb & gutter along Diesel St and part of Hicks St	TIRZ 13, COH, Festival Properties Inc. , HC Pct 1, HPB, UPRR	TIRZ 13	Design \$195K	Construct \$1.1M	Total \$1.3M
K	Shepherd/Durham & Inker/Koehler (Pg. 107)	Long-Term/ Partnership Dependent	>Major reconstruction > Striping, signage, curb ramp upgrades > Sidewalk repair, replace, install (without drainage conversion) > Intersection & crossing improvements (particularly at Shepherd Dr, Durham Dr, Yale St, and Heights Blvd) > Define travel lanes > Other (pedestrian-scale lighting)	> Sidewalk improvements, curb ramp improvements, and lighting at Cohn St bridge entrance > Sidewalk and curb ramp upgrades > High-visibility crosswalks at key intersections like Shepherd Dr, Durham Dr, Yale St, TC Jester, and Heights Blvd > Signage and wayfinding to connect Cottage Grove with WOB/Heights Blvd > Intersection improvements at Patterson and Inker/Koehler, including speed cushions along Patterson St	> Community support, potential drainage > East-west connectivity south of I-10	HC Pct 4, TxDOT, METRO, COH	FTA 5310, County Partnership (P4), SS4A, H-GAC Safety Funding	Design \$6.4M	Construct \$35.4M	Total \$41.9M

ID	Project Name	Phase	Project Type	Early Action Opportunities	Implementation Considerations	Potential Implementing Agencies <i>ROW owner = Bold</i>	Potential Funding	Cost Estimate <i>Planning-Level (2026 dollars)</i>		
								Design	Construct	Total
L	Michaux Street (Pg. 107)	Mid-Term Capital Dependent	<ul style="list-style-type: none"> > Striping, signage, curb ramp upgrades > Sidewalk repair, replace, install > Minor drainage conversion > Intersection & crossing improvements > Define travel lanes > Other (pedestrian-scale lighting) 	<ul style="list-style-type: none"> > Stude Park entrance gateway improvements at Usener St at Michaux St, including curb extensions, high-visibility sidewalks, signage and wayfinding, and lighting > Sidewalk and curb ramp improvements along the corridor, including new sidewalk from White Oak Dr to Usener St > High-visibility crosswalks and curb ramp improvements at key intersections like N Main St, 14th St, Merrill St, and Usener St 	<ul style="list-style-type: none"> > Drainage conversion to curb & gutter 	COH , HC Pct 1, GNMD, HPB, METRO	TIRZ 5, COH CIP, FTA 5310	Design \$830K	Construct \$4.6M	Total \$5.4M
M	N Main Street (Pg. 108)	Long Term/ Partnership Dependent	<ul style="list-style-type: none"> > Major reconstruction (including shared-use path, intersection realignment and redesign) > TxDOT coordination 	<ul style="list-style-type: none"> > High-visibility crosswalks, shade, enhanced bus stops > Signal warrant analysis for multiple intersections along the corridor 	<ul style="list-style-type: none"> > Reconstruction may require utility relocation > METRO coordination > TxDOT coordination > School operations for Browning ES, Travis ES, and Field ES 	TxDOT, GNMD, METRO, COH , HC Pct 1, HC Pct 2, HISD	NHHIP Project, COH District funds, TxDOT TA, METRO General Fund, STBG/CMAQ, SRTS, SS4A, H-GAC Safety Funding	Design \$5.4M	Construct \$29.5M	Total \$34.9M
N	Patterson Street (Pg. 109)	Mid-Term Capital Dependent	<ul style="list-style-type: none"> > Striping, signage, curb ramp upgrades > Sidewalk repair, replace, install (without drainage conversion) > Intersection & crossing improvements > Define travel lanes > Other (pedestrian-scale lighting) 	<ul style="list-style-type: none"> > Signage and wayfinding (to White Oak Bayou Greenway) > High-visibility crosswalks at all intersections and curb extensions to improve visibility > Intersection improvements at Inker/Koehler, including speed cushions along Patterson St 	<ul style="list-style-type: none"> > TxDOT coordination > Rail coordination 	TxDOT , COH , HPB, METRO, HC Pct 1, Bike Houston, MHRA (advisory)	TIRZ 5, TxDOT TA, FTA 5310	Design \$455K	Construct \$2.5M	Total \$3.0M
O	Rutland Street (Pg. 109)	Long Term/ Partnership Dependent	<ul style="list-style-type: none"> > Striping, signage, curb ramp upgrades > Sidewalk repair, replace, install (without drainage conversion) > Intersection & crossing improvements > Define travel lanes 	<ul style="list-style-type: none"> > Painting/striping and curb extensions at key intersections (20th, 14th, 11th, MKT Trail) > Signage and wayfinding at MKT Trail and White Oak Bayou access points 	<ul style="list-style-type: none"> > TxDOT coordination (for link to TxDOT detention basin at Heights Bird Sanctuary) 	COH , MHRA, TxDOT, METRO, HC Pct 1	FTA 5310, COH CIP	Design \$900K	Construct \$4.9M	Total \$5.8M
P	Studemont & Studewood Streets (Pg. 110)	Long Term/ Partnership Dependent	<ul style="list-style-type: none"> > Striping, signage, curb ramp upgrades > Shared-use path installation > Intersection & crossing improvements (including school-focused mid-block crossings) 	<ul style="list-style-type: none"> > Improved crosswalks at key intersections such as White Oak Dr, 11th St, and 14th St, Washington Ave > Keep the existing White Oak Bayou Greenway connection south of I-10 (currently scheduled for removal) > Signage & wayfinding (for key White Oak Bayou Greenway connections) 	<ul style="list-style-type: none"> > METRO coordination > TxDOT coordination > School operations for Field ES, Hogg MS, Heights HS 	METRO, HC Pct 1, COH , HISD, TxDOT, MHRA (advisory)	SS4A, SRTS, FTA 5310, TxDOT TA, H-GAC Safety Funding	Design \$2.5M	Construct \$13.6M	Total \$16.1M

ID	Project Name	Phase	Project Type	Early Action Opportunities	Implementation Considerations	Potential Implementing Agencies <i>ROW owner = Bold</i>	Potential Funding	Cost Estimate <i>Planning-Level (2026 dollars)</i>		
								Design	Construct	Total
Q	Winter Street Promenade (Pg. 110)	Long Term/ Partnership Dependent	> Trail connection > Rail coordination > Drainage considerations	> Extend pervious pavement along Winter St west to Sawyer St	> Rail & City of Houston coordination	COH , HC Pct 1, TIRZ 13, METRO, UPRR , TxDOT, HPB	TIRZ 13, UPRR, TxDOT TASA, HSIP, CRISI, H-GAC Safety Funding	<i>Design</i> \$3.4M	<i>Construct</i> \$18.4M	<i>Total</i> \$21.8M
R	Little White Oak Bayou Extension (Pg. 111)	Long Term/ Partnership Dependent	> Trail connection > TxDOT coordination > Bridges > Drainage considerations	NA	> TxDOT coordination > Underpass crossings required	TxDOT , HC Pct 2, HPB, TIRZ 21, COH, GNMD, HC Pct 1, HCFC	TPWD RTP, NHHIP Project, HSIP, H-GAC Safety Funding, County Partnership (P2)	<i>Design</i> \$1.0M	<i>Construct</i> \$5.6M	<i>Total*</i> \$6.6M
S	W 19th Street (Pg. 111)	Near Term/ Early Action	> Major reconstruction (including shared-use path, intersection/crossing improvements)	> Add bollards at each intersection for the existing shared-use path on 19th; add striping along the existing shared-use path similar to Spring St trail > Provide high-visibility crosswalks at key intersections, including Beall St and Bevis St.	> Potential drainage and utility relocation > Business access considerations	MHRA, COH , HC Pct 1, METRO	TIRZ 5, FTA 5310, County Partnership (P1), STBG/CMAQ	<i>Design</i> \$5.2M	<i>Construct</i> \$28.6M	<i>Total</i> \$33.8M
T	White Oak Drive (Pg. 112)	Mid-Term Capital Dependent	> Signage, striping, curb ramp upgrades > Intersection & crossing improvements (including mid-block crossings) > Other (pedestrian-scale lighting and placemaking)	> Add curb extensions and high-visibility sidewalks (Intersections: Yale St, Heights Blvd, Arlington St, Harvard St, and Oxford St), pedestrian-scale lighting > Formalize mid-block crossings at Threlkeld St and Granberry St with curb extensions to improve visibility for pedestrians	> Community support > Crossing safety + placemaking	METRO, COH , HC Pct 1, METRO, MHRA (advisory)	TIRZ 5, COH District funds, SS4A, HSIP, H-GAC Safety Funding	<i>Design</i> \$802K	<i>Construct</i> \$4.4M	<i>Total</i> \$5.2M
								Total Design \$88.6M	Total Construct \$487.5M	Grand Total \$576.1M

Key notes about the cost estimates:

- All costs are planning-level estimates using 2026 dollars at the time of plan publishing.
- Costs are estimated using TxDOT’s publicly available bid item averages for recent projects as well as TEI’s documentation of design and construction costs for recent, local projects in Harris County with similar conditions and recommended improvements.
- Construct = Base Construction Total + (Base Construction Total x 10% Mobilization Factor) where Base Construction Total = Construction Estimates x 20% Contingency Factor
- Design = Base Construction Total x 20% Design Factor
- Any discrepancy between the sum of the Design and Construct cost estimate columns and the Total cost estimate column should be attributed to rounding error.
- The Grand Total of all projects includes multiple Yale Street projects options for both the short and long term (Striping, Retrofit, and Reconstruction). Each of these options were priced individually, not cumulatively.
- Cohn/Larkin/Sherwin Streets will be reconstructed as part of the existing City of Houston Cottage Grove East and Cottage Grove West projects. The cost estimates shown here assume roadway, sidewalk, and drainage work will be conducted as part of these City of Houston projects. Project costs will include Neighborhood Street elements, signage, and striping.
- *The full scale and extent of Little White Oak Bayou improvements will be determined in more detail as part of future planning efforts to identify specific plans for the greenway. The cost estimate for Little White Oak Bayou Extension only includes trail pavement and underpasses from White Oak Drive to Cavalcade Street. The final cost estimate for this project will depend on many additional factors determined through additional planning and pre-engineering. These will likely include costs for erosion and sediment control, updates to stormwater utilities, landscaping and placemaking, and others. These additional considerations will likely increase the final cost estimate for this project.

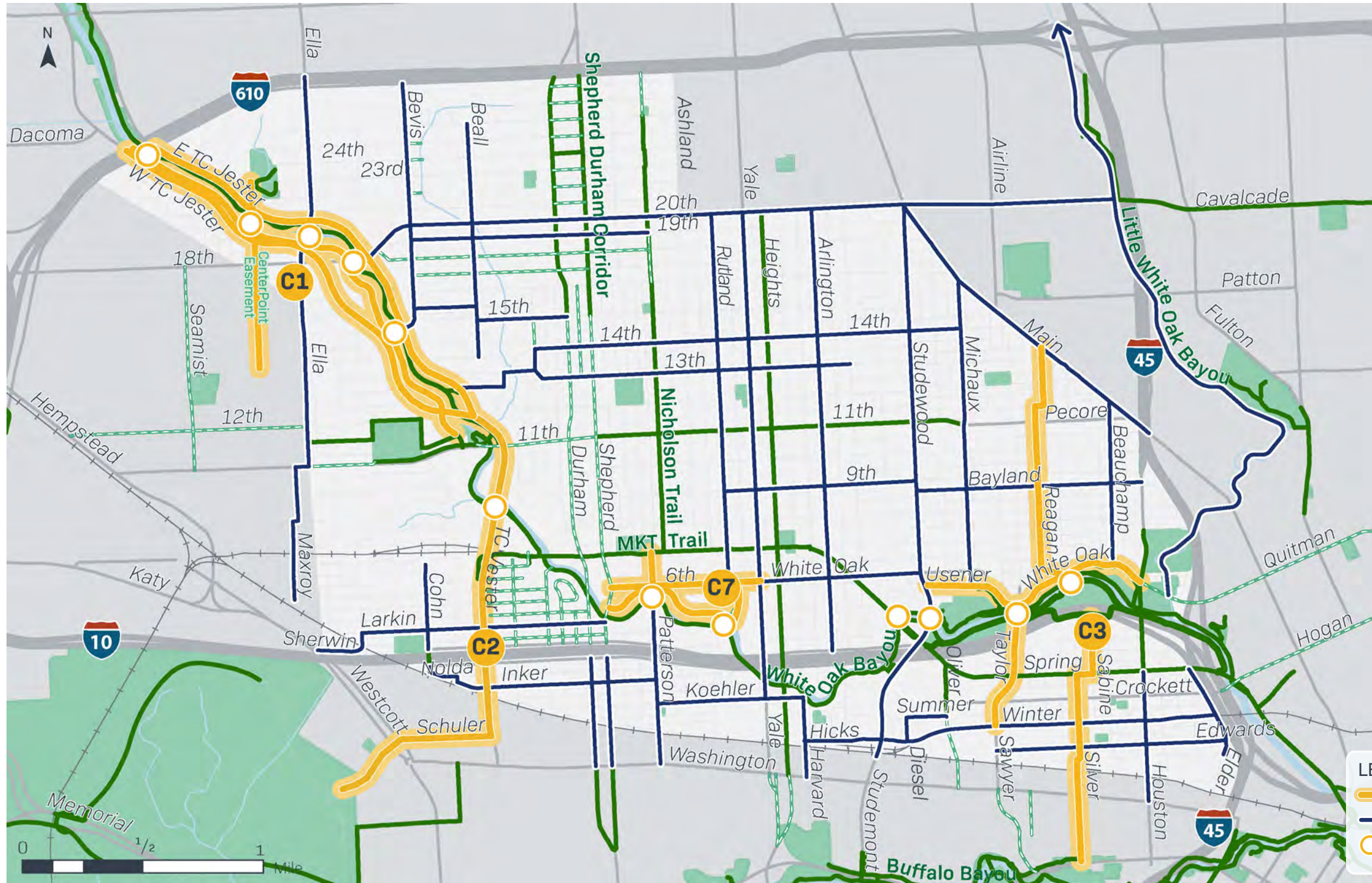
Section 3: A Generational White Oak Investment

Direct Investments in the Greenway

More than 32 miles of recommended projects connect to and across the White Oak Bayou Greenway. Four of these projects, shown on the map on the next page, are specifically designed to redefine the way that White Oak Bayou connects the neighborhoods, businesses, and green spaces of the Greater Heights. The following pages outline how these projects can be implemented in phases to establish this stretch of the Bayou as a signature regional destination for residents and visitors to relax, exercise, and travel.



Section 3: A Generational White Oak Investment



Four Signature Bayou Projects

The four Catalyst Projects shown on this map represent a set of projects intended to increase the safety and accessibility of the Bayou by proposing new connections over existing barriers. These projects also expand existing green space opportunities and reach into nearby neighborhoods to make the Bayou feel closer.

- C1** White Oak Connectivity Vision
- C2** White Oak to Memorial Park Connection
- C3** Heights to Buffalo Bayou Corridor
- C7** MKT/Bird Sanctuary Loop

Figure 5. Four Projects with Direct Impact on White Oak Bayou Access (Source: Team Analysis, 2026)

Section 3: A Generational White Oak Investment

C1 White Oak Connectivity Vision

C2 White Oak to Memorial Park Connection

C3 Heights to Buffalo Bayou Corridor

C7 MKT Bird Sanctuary Loop

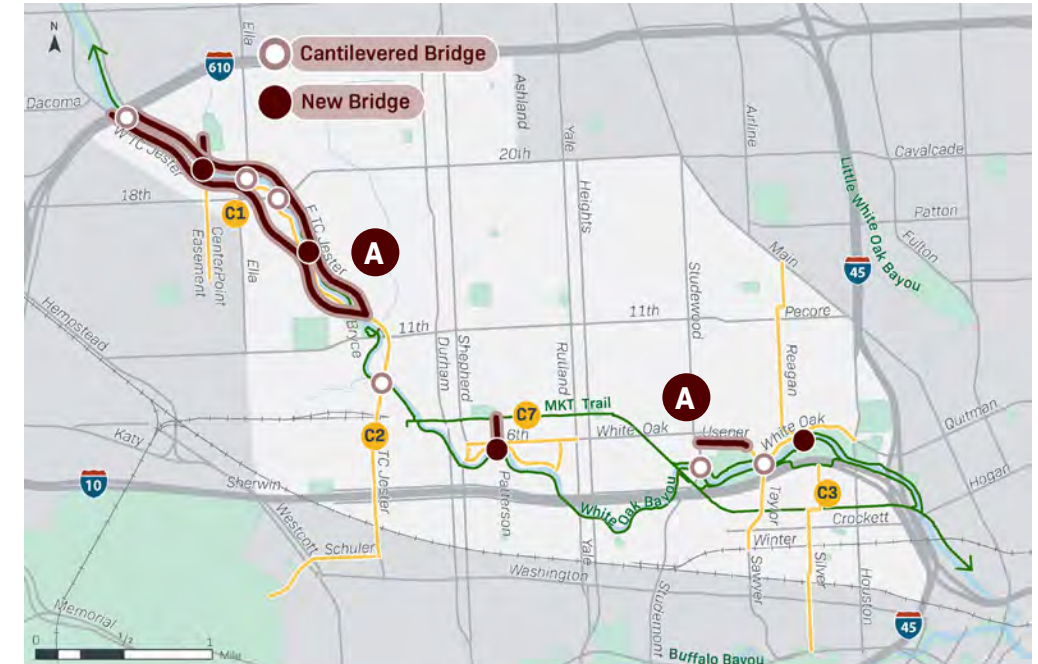
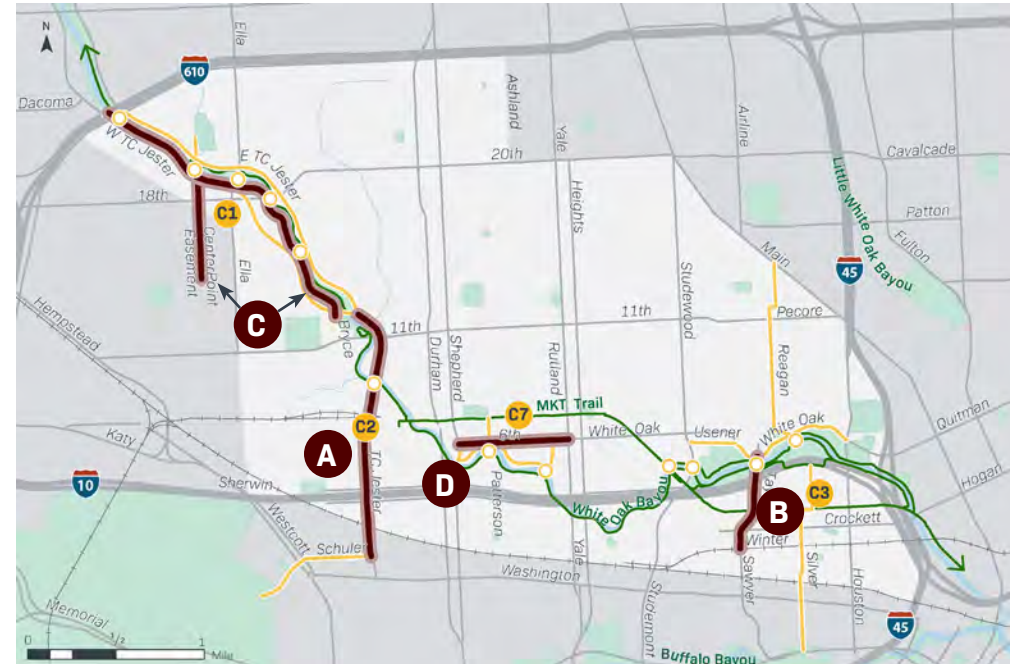
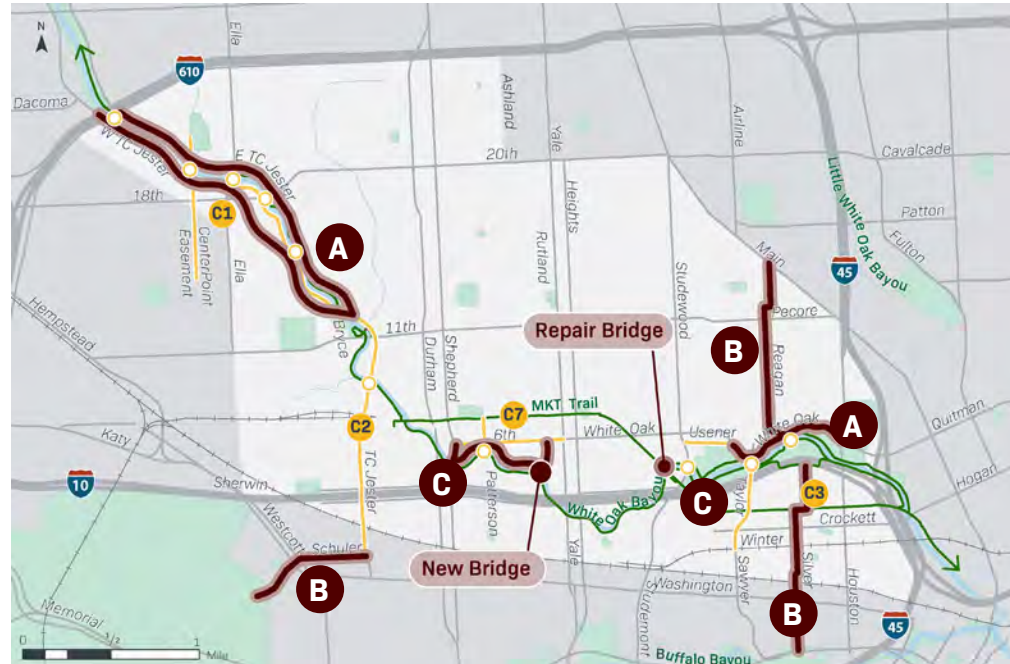


Figure 6. Map of Potential Phasing for White Oak Bayou-Adjacent Projects (Source: Team Analysis, 2026)

Phase 1 Safe Crossings, Local Streets

Improve Major Corridor Crossings **A**

- Install safe, high-visibility crossings across E TC Jester Blvd, W TC Jester Blvd, and White Oak Dr to improve access to the greenway

Retrofit/Reconstruct Key Corridors **B**

- Reconstruct Sabine St and retrofit Silver St to fill in missing sidewalks and construct safe crossings along the corridor
- Retrofit White Oak Dr and Reagan St to create safer, more logical crossings to Stude Park and the Bayou
- Retrofit Schuler St to improve safe access from TC Jester Blvd to Memorial Park

Greenspace Investments & Existing Projects **C**

- Add a trail on the north side of the Bayou from Shepherd Dr to Rutland St.
- Implement Stude Park improvements in partnership with the City of Houston.
- Build the Rutland St bridge over the Bayou and repair the existing MKT bridge (TxDOT)

Phase 2 Greenspace Connections

Memorial Park to White Oak Bayou **A**

- Build a shared-use path behind the curb on both sides of TC Jester Blvd to connect the Bayou and MKT Trail with Memorial Park. Long term, this connection can be improved by a future grade separation at the TC Jester Blvd railroad crossing.

Taylor St Connection to Stude Park **B**

- Construct the shared-use path along Taylor St to link the Spring St Trail to the Bayou.

New Greenway Trails **C**

- Add new trail on the Bayou's west bank from I-610 to Bryce St.
- Add a trail through the CenterPoint Energy (CPE) easement from Cindy Ln to W TC Jester Blvd.

Street Connections for M-K-T **D**

- Retrofit 6th St and work with TxDOT and local organizations to build trails and formalize a new park within the Rutland detention basin/Heights Bird Sanctuary.

Phase 3 Bridging the Bayou

Redesign TC Jester Blvd & Usener St **A**

- Retrofit TC Jester Blvd with a painted edge line to visibly define the travel lane. Reconstruct the "triangle" of unsafe intersections where Ella Blvd, TC Jester Blvd, W 18th St, and W 20th St meet.
- Reconstruct Usener St to improve connections to Stude Park

Cantilever Existing Bridges **Cantilever Bridge**

- Conduct feasibility studies of the existing bridges over the Bayou at I-610, Ella Blvd, W 18th St, TC Jester Blvd Studewood St, and Taylor St. Cantilever a trail bridge on the downstream side along any of the bridges that can structurally support it.

Build New Greenway Bridges **New Bridge**

- Build new bridges over the Bayou near the CenterPoint easement at Little Thicket Park, at Bevis St, Patterson St, and near Stude Park or White Oak Park.
- Extend a trails along the M-K-T at the base of the Patterson St Bridge and along the CenterPoint easement north of E TC Jester Blvd to connect into Little Thicket Park.

Section 4: Safe Routes to Transit: Sidewalk Installation, Repair & Replacement Data

Safe Sidewalks for Better Transit Service

Federal Transit Agency funding can be allocated toward sidewalk and crossing improvements within 1/2 mile of any existing transit stop or station. The following pages detail the miles of existing missing sidewalks that would be replaced (1) along each of the recommended projects, and (2) within 1/2 mile of bus stops along each of the seven bus routes within the study area. Cumulatively, these missing sidewalks total 123 miles. Installing new sidewalks would go a long way toward creating better access for transit users and nearby residents.



Section 4: Safe Routes to Transit: Sidewalk Installation, Repair & Replacement Data

Proposed Recommendations Sidewalk Data

Figure 6 highlights the corridors that are recommended for projects to increase access for pedestrians and bicyclists in Greater Heights. The table also shows the length of each corridor, how many miles of sidewalks currently exist, and how many miles of sidewalks would be needed to make the sidewalk network complete along the corridor. It is important to note that the Miles of Existing Sidewalks column simply indicates that a sidewalk is present but does not mean that the sidewalk is in proper condition. Sidewalks that are broken, cracked, or have widths that are too small will still need to be replaced. This table only shows how many miles of new sidewalks will be installed where there was previously no sidewalk.

Corridor Name	Miles of Roadway	Miles of Existing Sidewalks	Needed Sidewalks for Complete Network
9 th Street	0.67	1.34*	0
13 th Street	1.15	1.77	0.53
14 th Street	2.47	3.51	1.43
15 th Street	0.64	0.46	0.82
19 th Street	1.11	1.07	1.15
20 th Street	3.2	5.76	0.64
24 th Street	1.7	2.05	1.35
Arlington Street	1.86	3.52	0.2
Bayland Avenue	0.94	1.75*	0
Beall Street	0.097	0.76	1.18
Beauchamp Street	0.72	1.344	0.1
Bevis Street	1.05	1.03	1.07
Cohn/Inker Street	1.36	1.05	1.67
E TC Jester Boulevard	1.92	3.25	0.59
Edwards Street	0.96	1.31	0.61
Ella/Maxroy Boulevard	2.3	3.25	1.35
Harvard Street	0.19	0.04	0.34
Hicks/Summer Street	0.69	0.09	1.29
Koehler Street	0.61	0.87	0.35
Larkin Street	1.27	1.63	0.91
Little White Oak Bayou	2.4	No Data**	N/A
Michaux Street	1.97	1.62	2.32
N Main Street	1.39	2.33	0.45
Patterson Street	0.76	0.89	0.63
Reagan Street	1.05	1.22	0.88
Rutland Street	2.36	2.82**	1.9
Sabine/Silver Street	1.1	0.98	1.22
Studemont Street	0.69	0.75	0.63
Studewood Street	1.7	2.97	0.43
TC Jester Boulevard	2.12	3.02	1.22
Usener Street	0.4	0.58	0.22
W TC Jester Boulevard	1.95	2.78	1.12
White Oak Drive	0.73	1.09	0.37
Winter Street	1.52	0.07	2.97
Yale Street	2.04	3.74	0.34

Figure 7. Sidewalk data for recommended projects in Greater Heights (Source: Team Analysis, 2026)

*Indicates that entire corridor has sidewalks

**Has trail that does not have sidewalk data

Section 4: Safe Routes to Transit: Sidewalk Installation, Repair & Replacement Data

Proposed Recommendations Sidewalk Data

Figure 7 showcases the various bus routes that run through Greater Heights and the miles of roadway within a quarter mile of the bus routes. The table also includes data on the miles of existing sidewalks within ½ mile of the various bus routes and the miles of sidewalks needed to make the sidewalk network complete within ½ mile of the bus routes. Similar to Figure 6, the “Miles of Existing Sidewalks” column highlights that a sidewalk exists but does not indicate that the sidewalk is in good condition. Figure 8 through Figure 16 also show the location of missing sidewalks within a ½ mile along each bus route in Greater Heights.

Bus Route Name	Miles of Roadway Within ½ Mile of Route	Miles of Existing Sidewalks Within ½ Mile of Route	Needed Sidewalks for Complete Network Within ½ Mile of Route
44 Acres Homes	40	52	28
56 Airline/Montrose	44	65	23
30 Ella Clinton	101	139	63
26 Long Point Cavalcade	49	62	36
66 Quitman	76	100	52
27 Shepherd	63	72	54
40 Telephone Heights	50	78	22
Greater Heights Total	182	241	123

Figure 8. Sidewalk data for roads within 1/2 mile of bus routes in Greater Heights

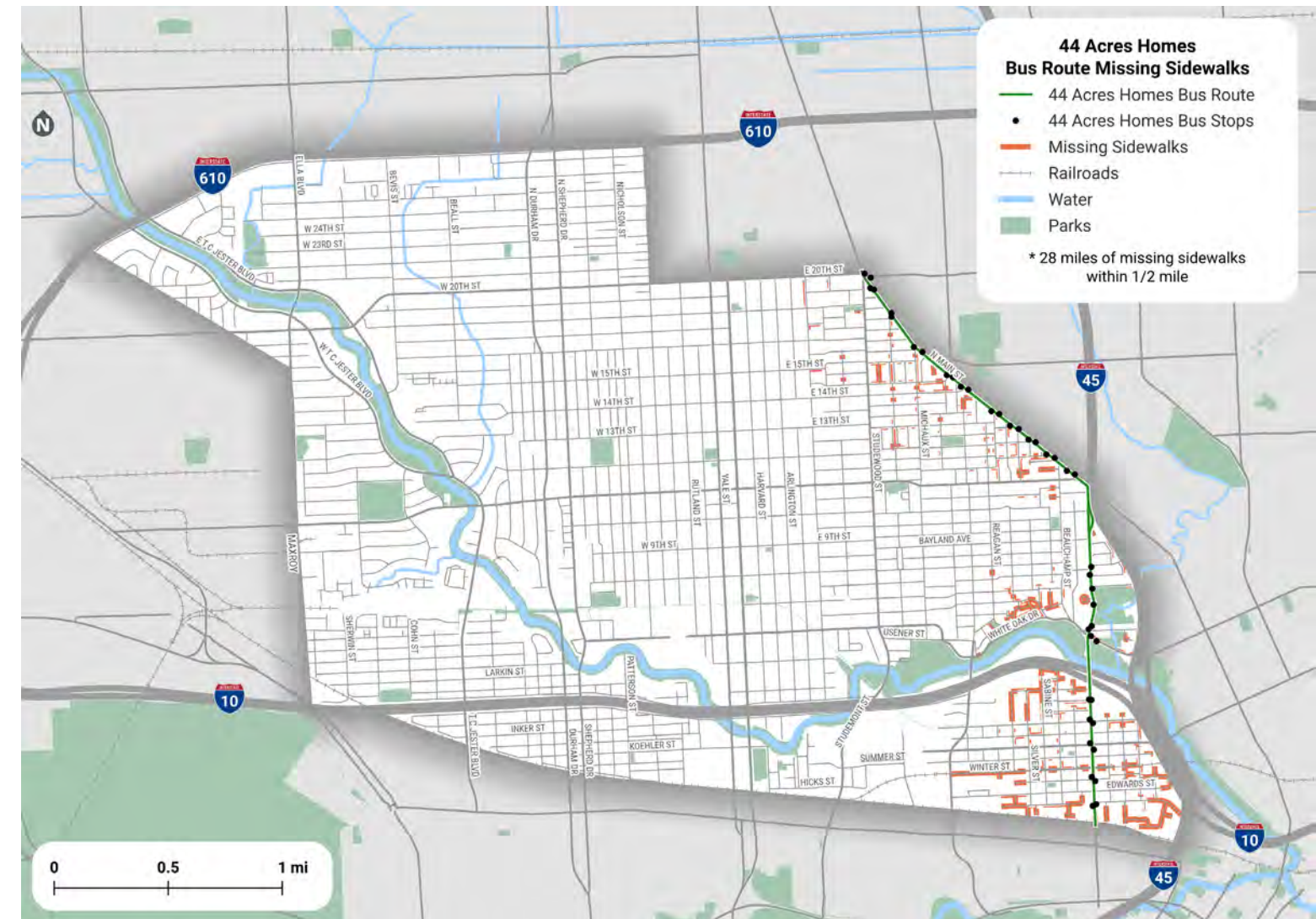


Figure 9. Missing sidewalk data within 1/2 mile of Route 44 Acres Homes (Source: Team Analysis, 2026)

Section 4: Safe Routes to Transit: Sidewalk Installation, Repair & Replacement Data

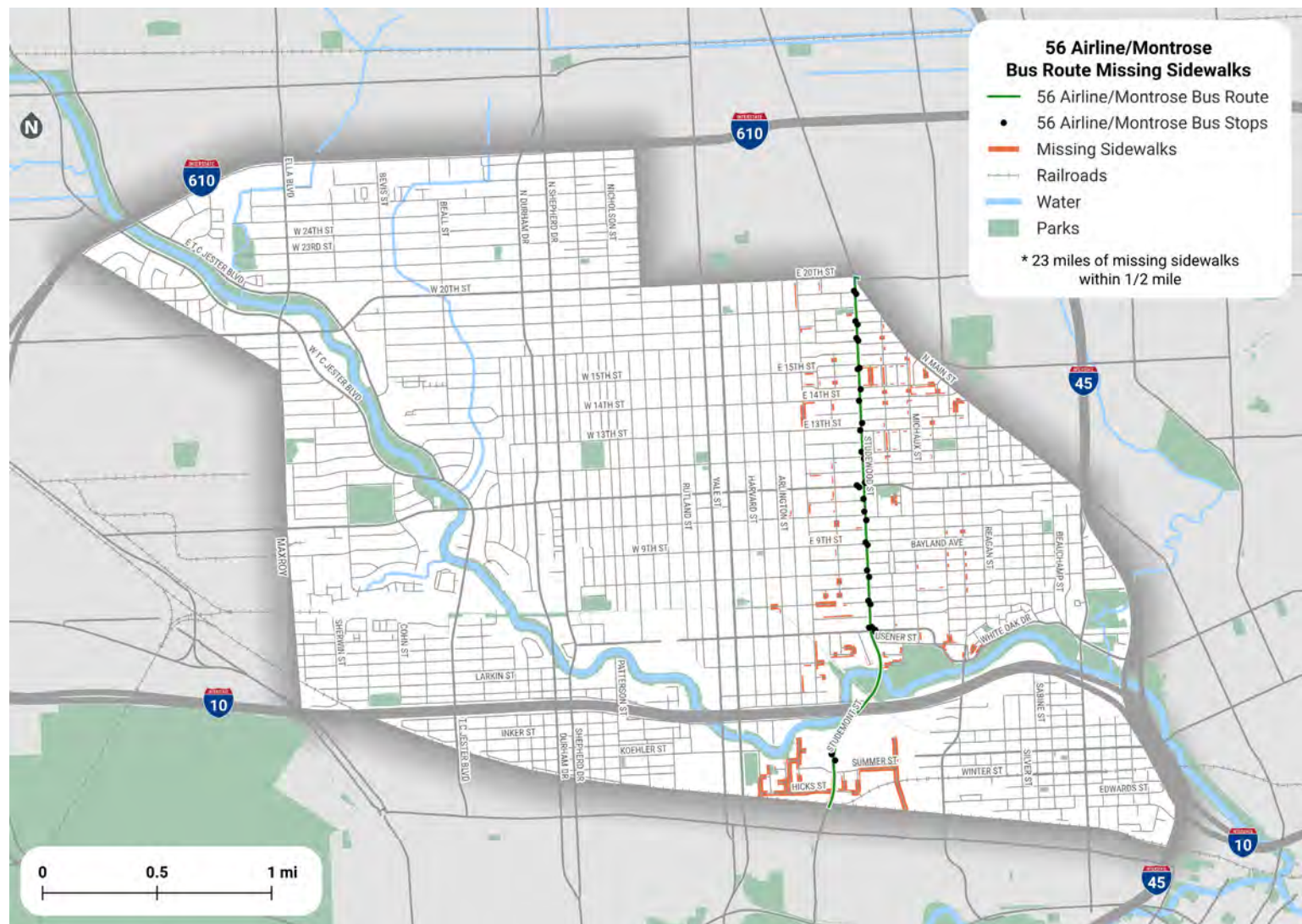


Figure 10. Missing sidewalk data within 1/2 mile of Route 56 Airline/Montrose (Source: Team Analysis, 2026)

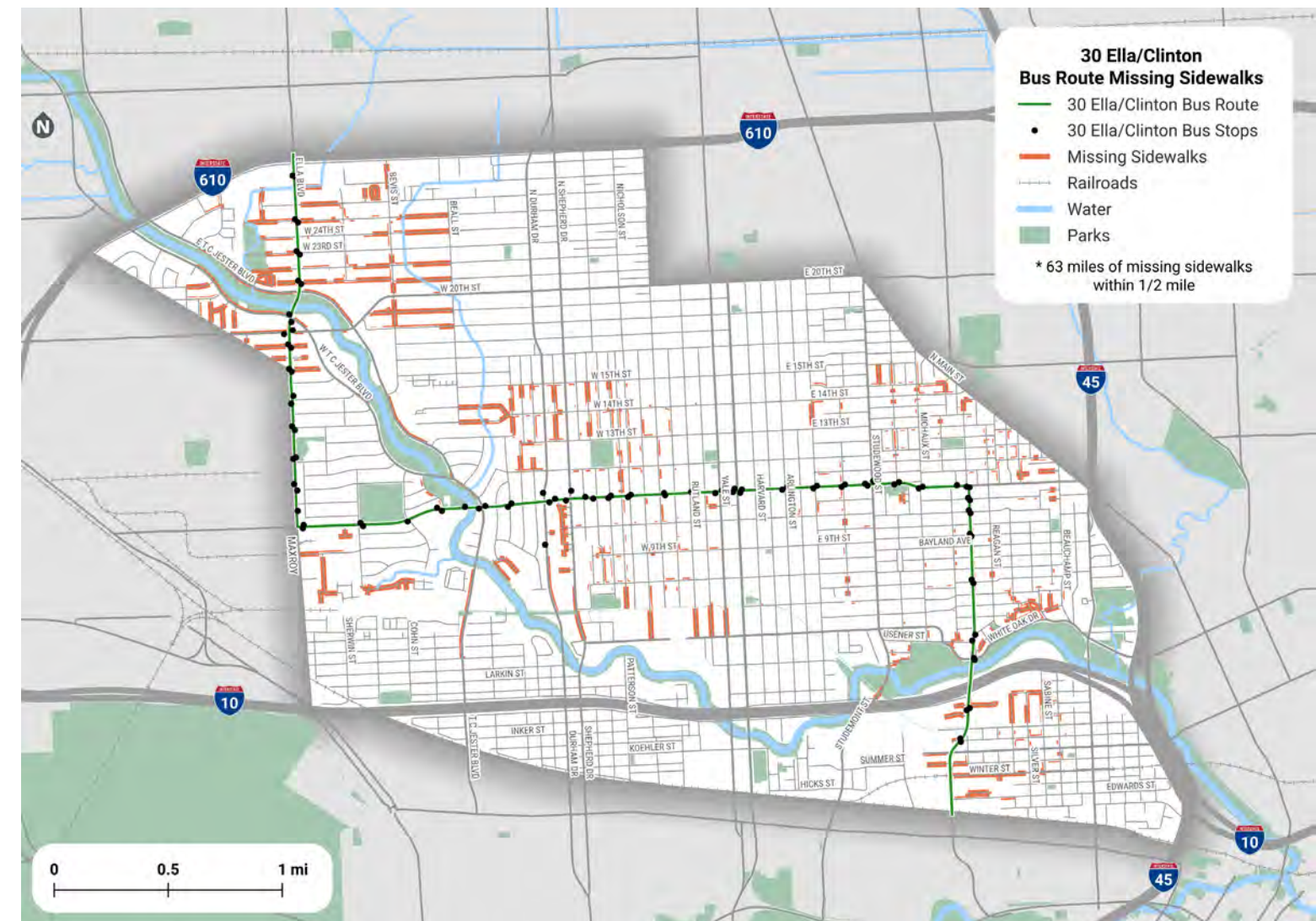


Figure 11. Missing sidewalk data within 1/2 mile of Route 30 Ella/Clinton (Source: Team Analysis, 2026)

Section 4: Safe Routes to Transit: Sidewalk Installation, Repair & Replacement Data

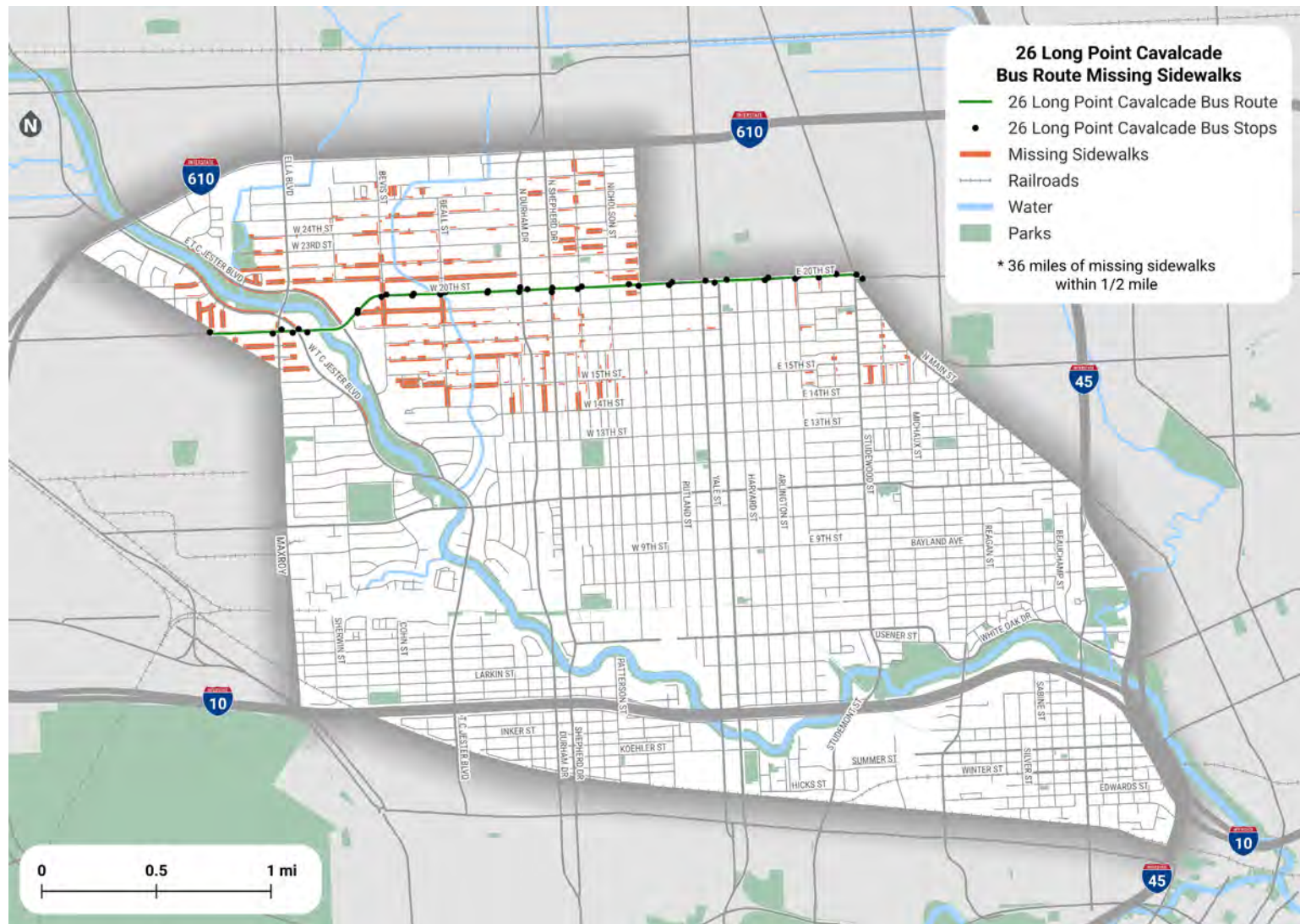


Figure 12. Missing sidewalk data within 1/2 mile of Route 26 Long Point/Cavalcade (Source: Team Analysis, 2026)

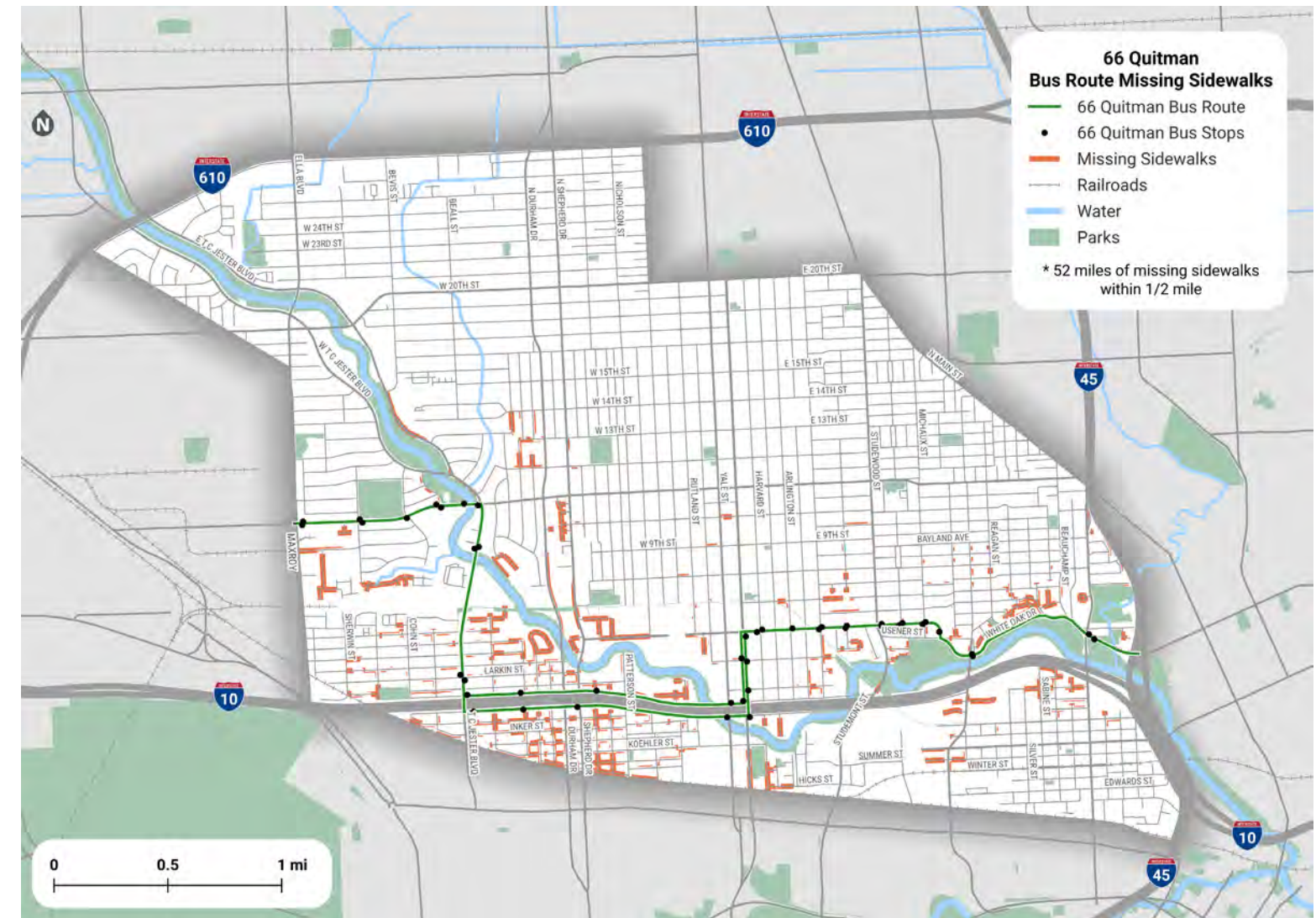


Figure 13. Missing sidewalk data within 1/2 mile of Route 66 Quitman (Source: Team Analysis, 2026)

Section 4: Safe Routes to Transit: Sidewalk Installation, Repair & Replacement Data

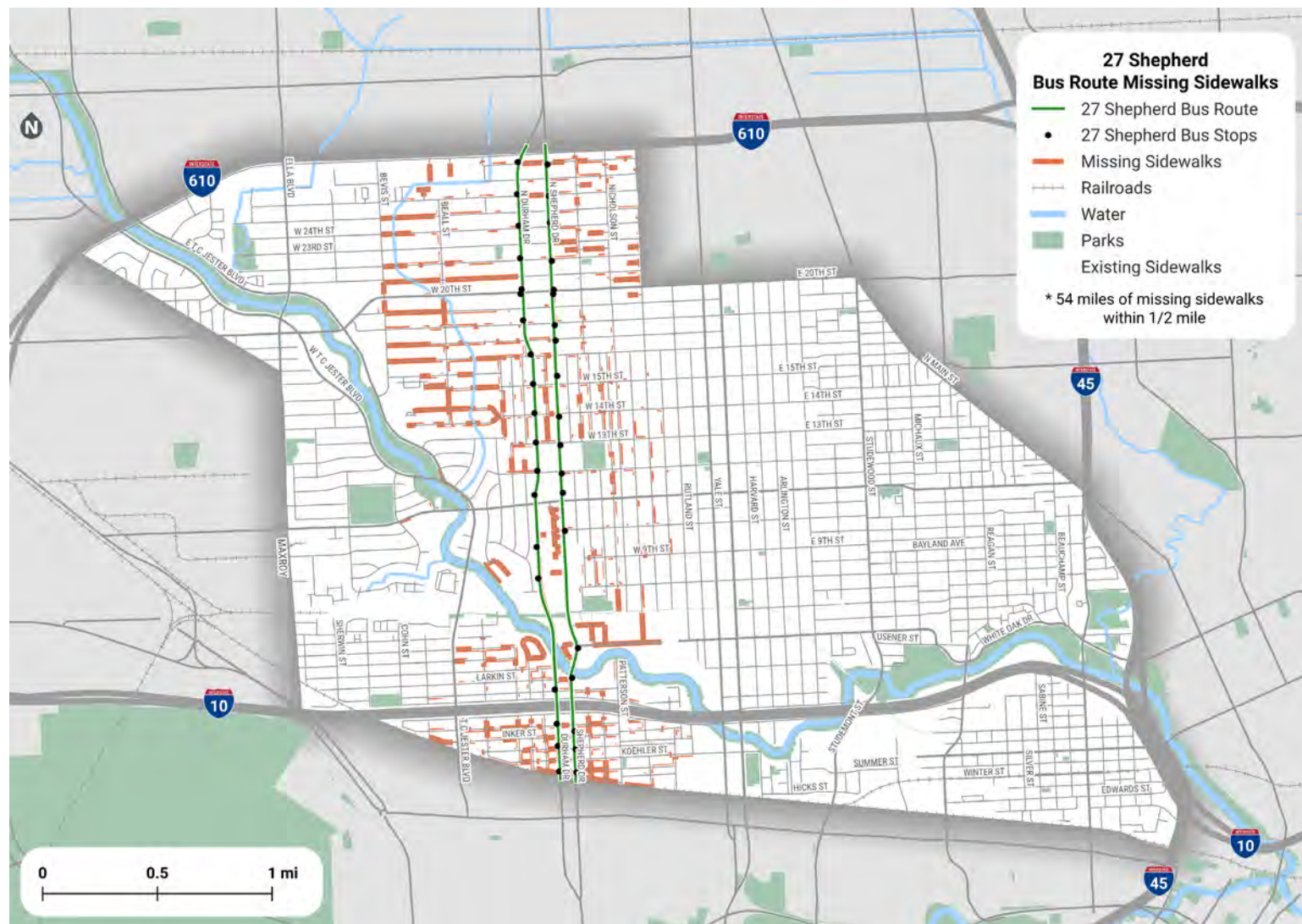


Figure 14. Missing sidewalk data within 1/2 mile of Route 27 Shepherd (Source: Team Analysis, 2026)

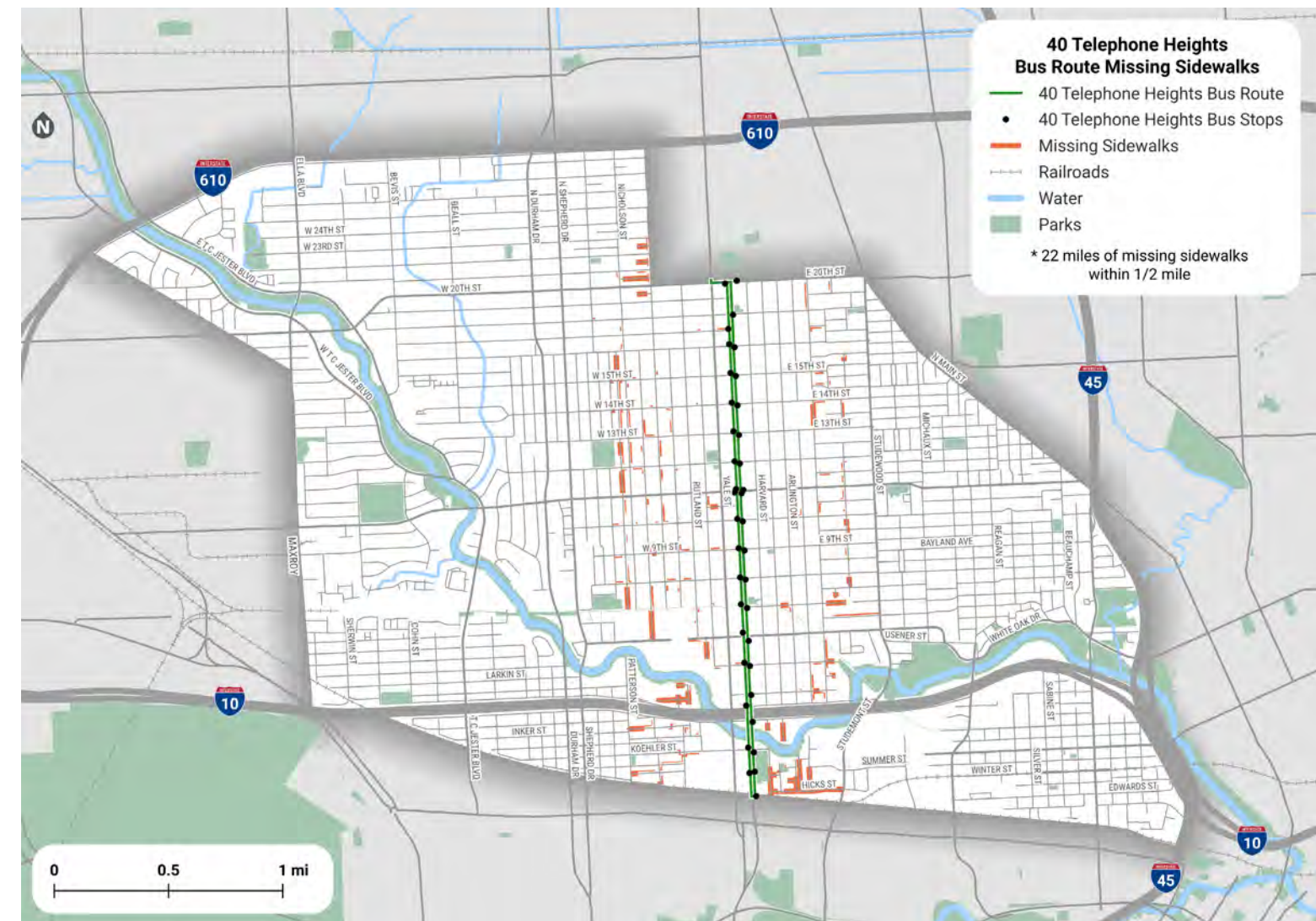


Figure 15. Missing sidewalk data within 1/2 mile of Route 40 Telephone/Heights (Source: Team Analysis, 2026)

Section 4: Safe Routes to Transit: Sidewalk Installation, Repair & Replacement Data

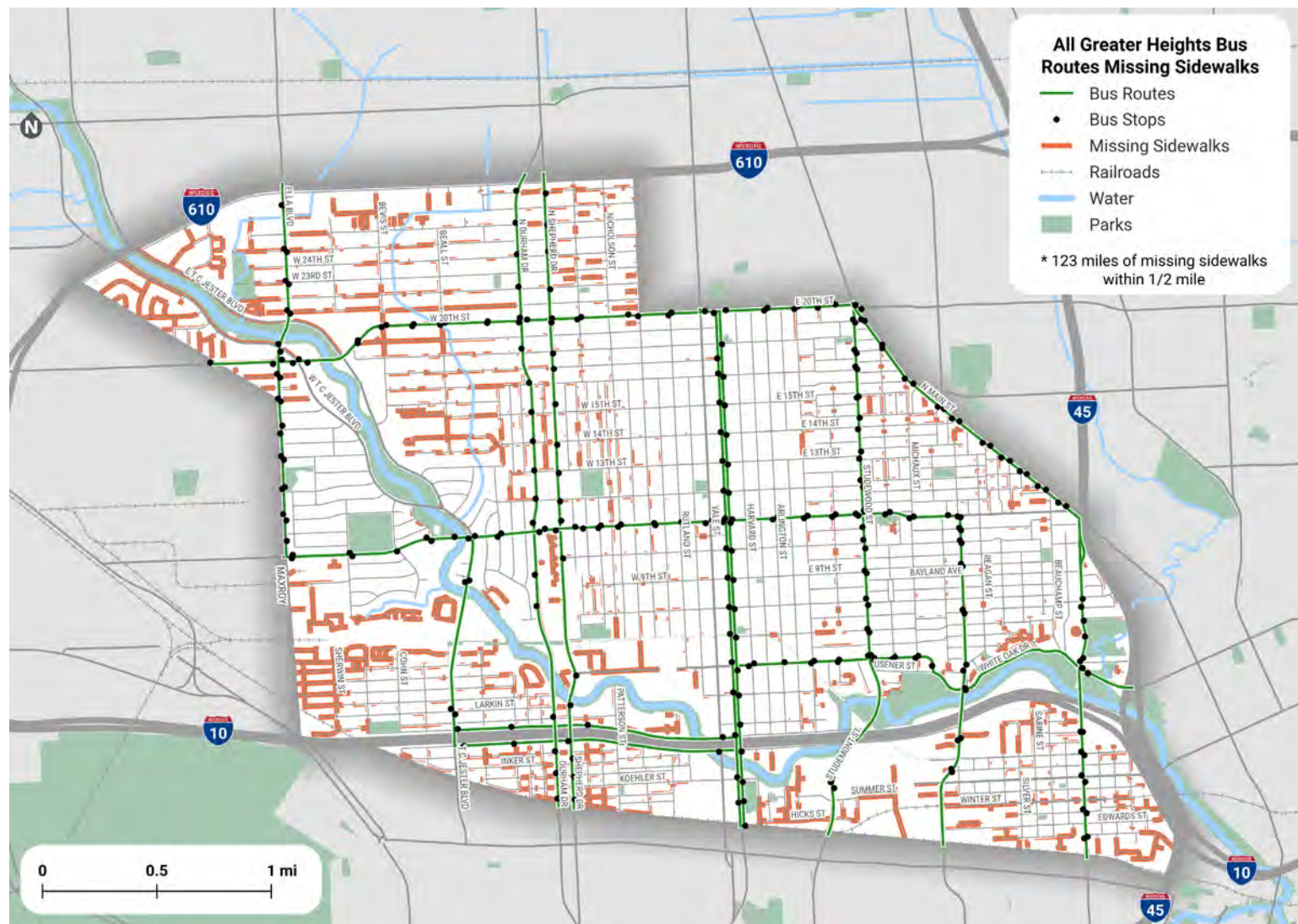


Figure 16. Missing sidewalk data within 1/2 mile of all Greater Heights Bus Routes (Source: Team Analysis, 2026)

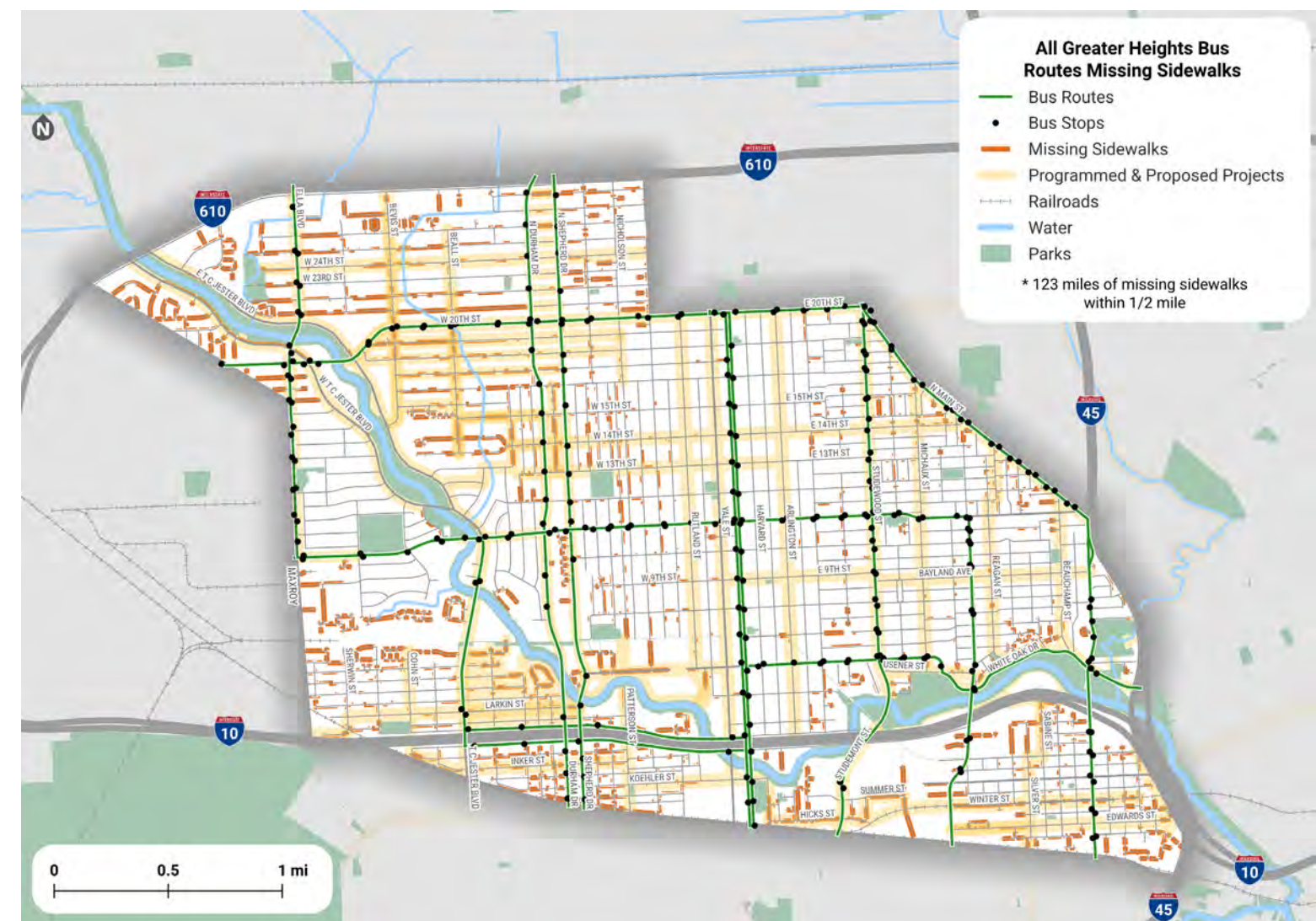


Figure 17. Missing sidewalk data within 1/2 mile of All Bus Routes with Programmed & Proposed Projects (Source: Team Analysis, 2026)

Appendices

- A - Community Engagement Round 1 Overview**
- B - Community Engagement Round 2 Overview**
- C - Community Engagement Round 3 Overview**
- D - Needs Analysis Memorandum**



Appendix A: Community Engagement Round 1 Overview

September 2025

Round I Engagement Overview

Purpose: Why Engage at this Stage?

People who live and work in the neighborhoods of the Greater Heights are highly engaged, and make time to participate in the key conversations about mobility in their community. This was clearly demonstrated during recent planning efforts such as the TIRZ 12 Mobility Plan and both phases of the TIRZ 13 Mobility Study.

These community members have also witnessed or participated in a range of major mobility changes in recent years, such as the design, construction, and completion of the first phase the Shepherd Durham Project, TxDOT's plans for the I-10 White Oak and North Houston Highway Improvement Projects, the completion of the 11th Street

Bikeway, the closure of two key pedestrian and bicycle bridges along the MKT Trail, and more.

For these reasons, the MHRA Board considered it essential to reach out to community members from the very beginning of this Plan to ensure that the many interested residents, civic associations, advocates, and others are aware of the purpose and anticipated outcome of the Greater Heights Strategic Connections Plan.

In addition to raising awareness of the planning effort, Round I was also an opportunity to gain community input on the mobility values, concerns, and opportunities facing these neighborhoods.

Outreach: How Did We Reach People?

For Round I engagement, the planning team developed an online survey and mapping exercise that stayed open for 32 days (from August 18 to September 19, 2025). The team also presented to more than 100 attendees at the Super Neighborhood 14, 15, and 22 meetings and the TIRZ 12 Projects Committee meeting in August 2025.

The team shared digital flyers and outreach language with leadership of the Houston Heights Association, Woodland Heights Civic Association,

First Ward Civic Club, Norhill Neighborhood Association, and A Tale of Two Bridges non-profit to share with their respective members. The outreach resulted in multiple social media posts by Super Neighborhood and civic association groups as well as an article about the plan in *The Leader News*.

The outreach effort resulted in 330 completed surveys and 764 pins placed on the interactive Map My Vision mapping exercise. The results of both of these activities are summarized in this document.

Outcomes: How Will Input Be Used?

The results of this community engagement will be used alongside the results of the Existing Conditions Key Insights from Chapter 1 to develop Mission, Vision, and Goals for the plan, outlined in Chapter 2. These Mission,

Vision, and Goal statements will provide the foundation for the remainder of the planning document, leading directly into the identification of key streets and intersections where improvements will have the greatest impact.

Outreach Efforts by the Numbers

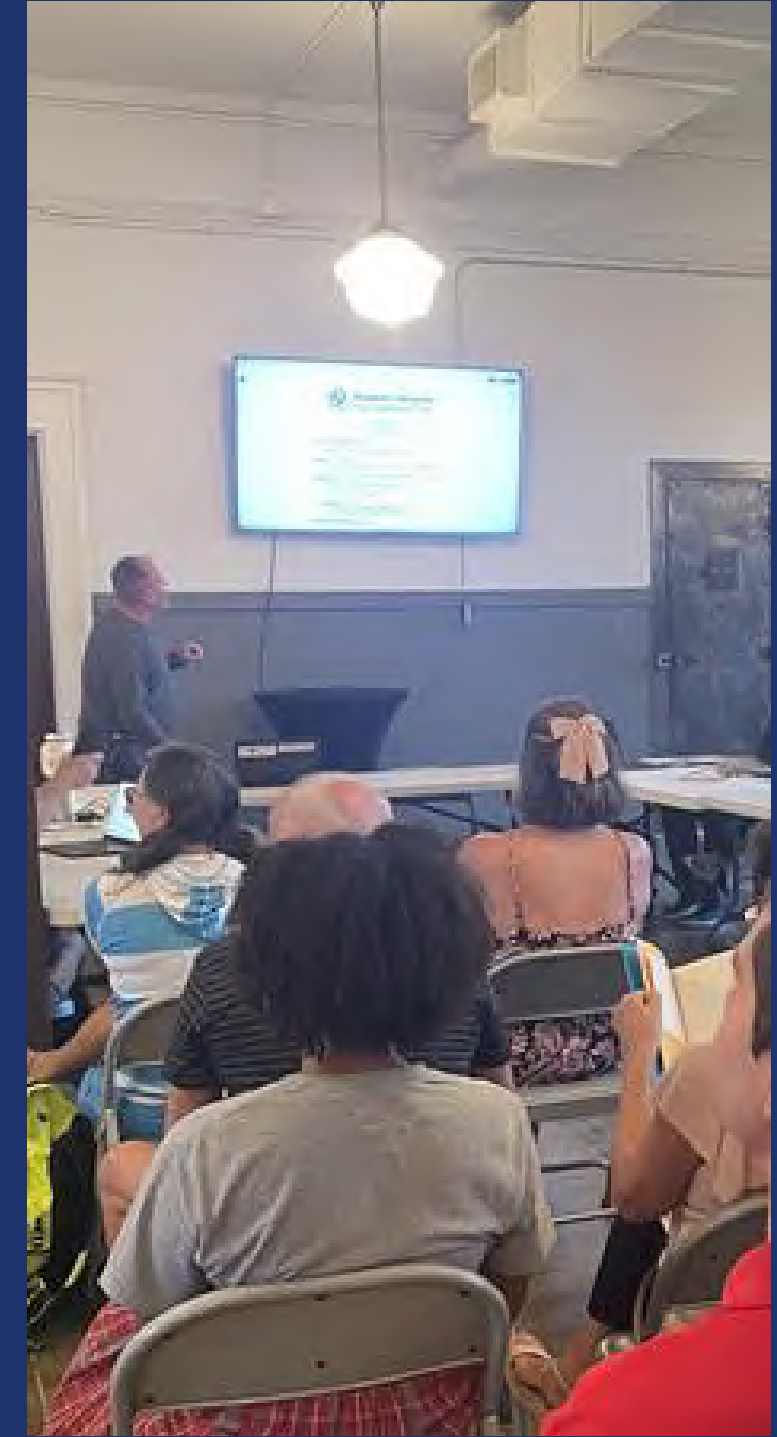
- 3 Super Neighborhood Meetings with 100+ attendees
- 6 Social Media or Newsletter Posts
- 1 News Article

Resulting in...

- 300 completed surveys
- 764 map pins placed



Survey Results Summary



Greater Heights Strategic Connections Plan

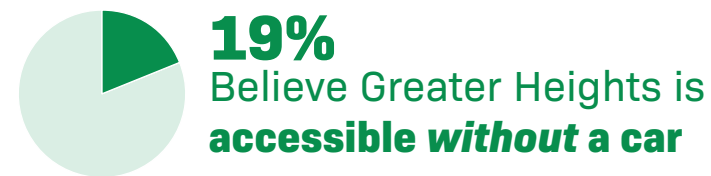


What We Heard: Four Big Takeaways

People want mobility choice, but driving is often seen as the only accessible option

On a scale of one to ten, more than 80 percent of respondents selected an eight or higher when asked if access to multiple transportation options is important to them personally. Nearly 70 percent of people said they wish they could bike more and 62 percent said they wish they could walk more.

Survey respondents were also asked to rate the accessibility of the Greater Heights by car and without a car. The response was clear: 80 percent say that the Study Area is accessible by car (eight to ten on a ten-point scale). Only 19 percent say that the Greater Heights is accessible by bike, transit, or on foot.



**respondents who selected 8-10 on a scale of 1 to 10*

Community Comments & Ideas

Survey respondents value the walkability of their immediate neighborhoods and want to be able to walk and bike longer distances within Greater Heights.

Community members expressed excitement for the possibility of connecting and extending existing trails like the MKT Trail to make more car-free trips possible.

Respondent quote: “We need more options for safe multimodal transportation for work, errand running, leisure, shopping, and environmental reasons.”

Respondent quote: “[Shepherd Durham] could be a great transit corridor if there was frequent service and nice places to wait!”

Safety matters to residents - speeding and unsafe crossings are the major concerns

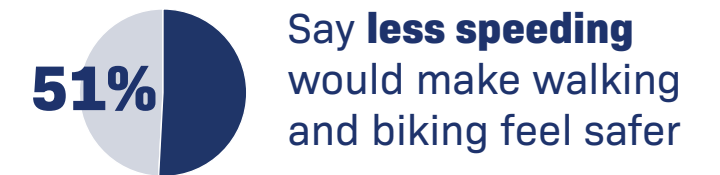
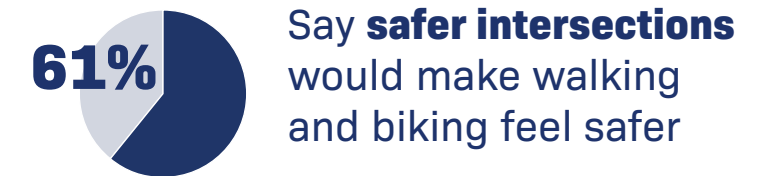
When asked to rank their mobility values, survey respondents chose “Safety” as the highest value by far, with “Clean, well-maintained infrastructure” and “Multiple transportation options” in second and third.

When asked to select safety improvements that would improve conditions for walking and biking, 61 percent chose safer intersections and 51 percent selected slower vehicle speeds.

The survey also asked parents about their children’s mobility on local streets. Respondents named unsafe intersections and speeding as the top two concerns for children’s safety.

Only 7 percent of survey respondents feel safe traveling in the Greater Heights outside of a car.

#1 Safety was the #1 selected mobility value



Community Comments & Ideas

Survey respondents consistently brought up safety concerns including broken, missing, or blocked sidewalks, and vehicles speeding on TC Jester Boulevard, N Main Street, Crockett Street, and 20th Street.

Intersections and crossings feel unsafe with respondents expressing concerns about school kids walking and biking without protection.

Respondent quote: “I’ve been run off the road multiple times with my children in a stroller and thrown into a ditch by careless drivers. All because I’m having to walk into the street due to gaps in the sidewalks.”

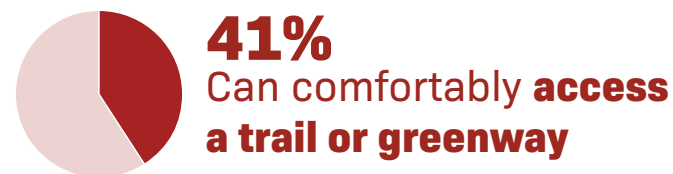
Respondent quote: “20th is a 4-lane road where drivers can speed and mostly refuse to yield to people in the crosswalk... At a minimum needs to be repainted but a 4-way stop or HAWK is much better. The foot and bike traffic on this part of Nicholson is impressive.”

What We Heard: Four Big Takeaways

People want easier access to outdoor spaces to connect with nature and their neighbors

People in the Greater Heights consider access to parks and green spaces an important feature of their neighborhood mobility network. Health and exercise was the fifth highest mobility value selected by community members and a resounding 95 percent of all respondents say that they walk, bike, or roll for recreation.

Despite having a high density of quality parks and trails, only 41 percent of people say that they can comfortably access a trail or greenway (eight to ten on a scale of ten).



**respondents who selected 8-10 on a scale of 1 to 10*

Residents are already walking and biking, but they can't reach many important places

Survey respondents were asked to name the places they do not currently feel comfortable walking, biking, or riding transit. While 62 percent of respondents feel comfortable walking or biking to a friend's or neighbor's house (often a short distance), destinations farther away can be more difficult to access. Only 28 percent feel comfortable walking or biking to community centers and libraries and only 9 percent feel comfortable walking or biking to school.

Responses from parents echo this trend. While two-thirds of children are currently walking as a regular form of transportation, only 39 percent of children are walking and biking to school.

Community members named sidewalks and more protected bikeways as the top two improvements that would improve safety and comfort for people not driving. Many community members provided ideas about where better sidewalks, trails, and bikeways should connect.



Conditions that would improve safety & comfort:

#1 Better sidewalks

#2 More protected bike lanes

Community Comments & Ideas

Survey respondents showed a desire for more green spaces and shaded trails, including an extension of the MKT Trail and a trail along Turkey Gully.

Comments often called for better lighting, security and amenities (drinking fountains, benches) as well as a desire for easier access to existing parks such as Little Thicket Park, Woodland Park, and Love Park).

Respondent Quote: "We need more street trees and need to prioritize beauty and placemaking in our infrastructure projects."

Respondent Quote: "The sidewalks and bike lanes and trees north of 15th are a fantastic improvement. I travel this stretch daily by car mostly and it's so pretty now and inviting. As the weather cools I'll be on my bike..."

Community Comments & Ideas

Respondents showed strong enthusiasm for better connections from Greater Heights neighborhoods to Memorial Park, Buffalo Bayou, & White Oak Bayou.

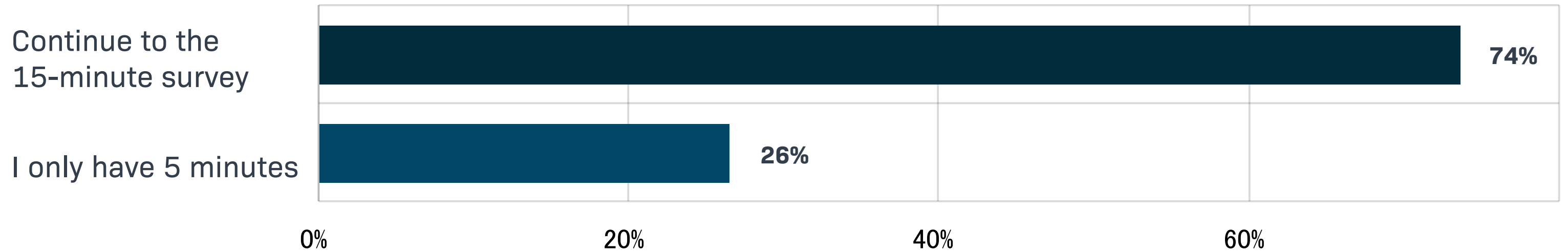
A new bridge over White Oak Bayou at Patterson Street & existing bridge improvements, such as on Studewood Street were recommended by several respondents.

Respondent Quote: "More consistent sidewalks and crosswalks would be a dream. There are walkable businesses I'd like to visit but it's almost impossible without a car or taking additional risk."

Respondent Quote: "Waiting for a bridge/trail to connect Cottage Grove and nearby neighborhoods to Memorial Park so we, as senior citizens, can safely travel there without a car."

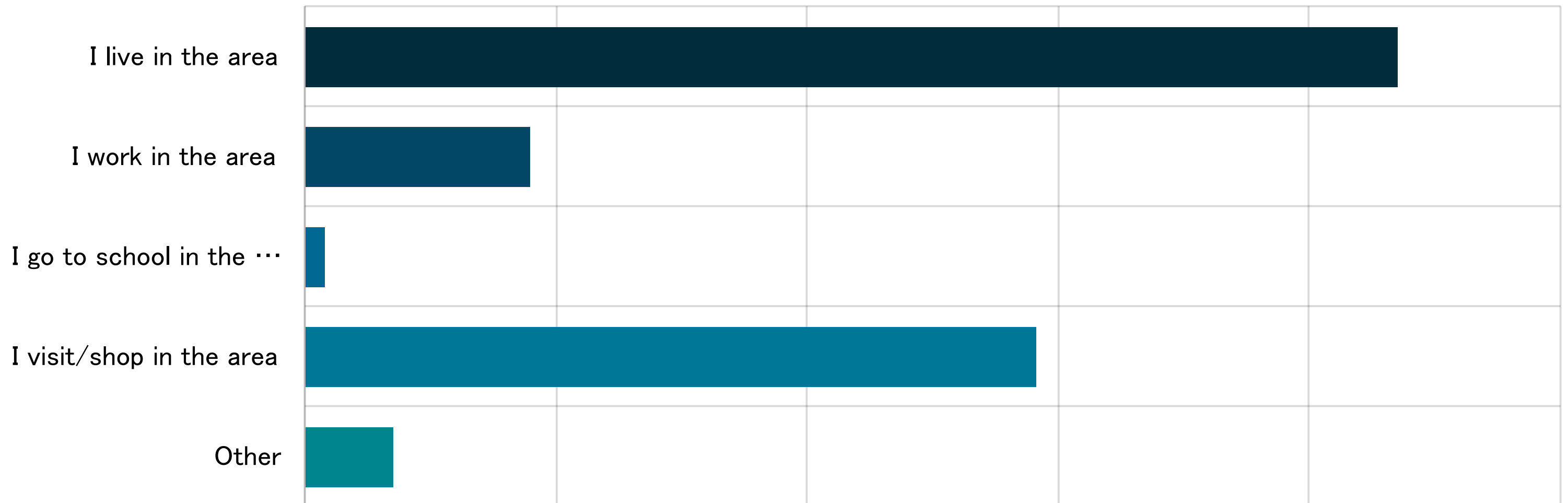
Survey Overview

- **330** survey responses
- **74%** completed the 15-minute survey



Relationship to the Study Area

- **87%** live in the area
- **58%** visit/shop in the area
- **18%** work in the area
- **2%** go to school in the area
- **7%** selected “Other”

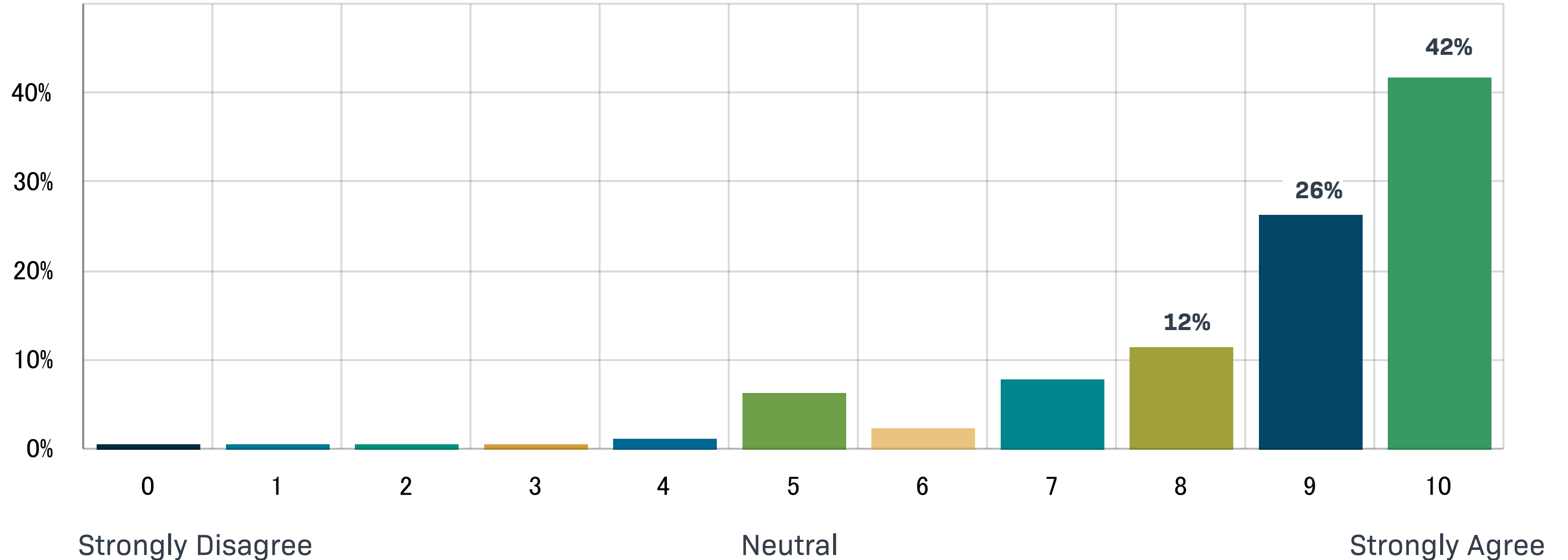


Responses may not add up to 100 percent because respondents could select all that apply.

Car Accessibility

(The Greater Heights area is accessible by car)

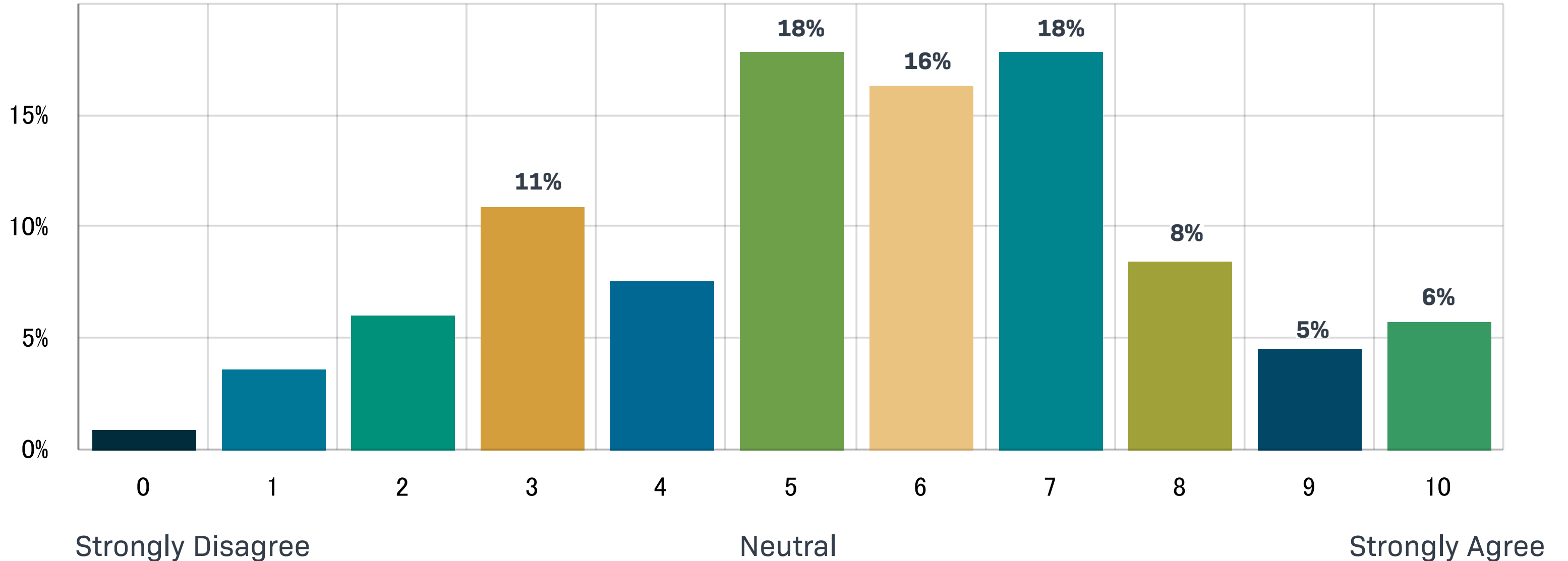
- Average rating is **8.6/10** - **80%** of people agree selected 8-10 when asked if the area is accessible by car on a scale from 1 to 10



Accessibility Without a Car (Bike/Walk/Transit)

(The Greater Heights area is accessible outside of a car)

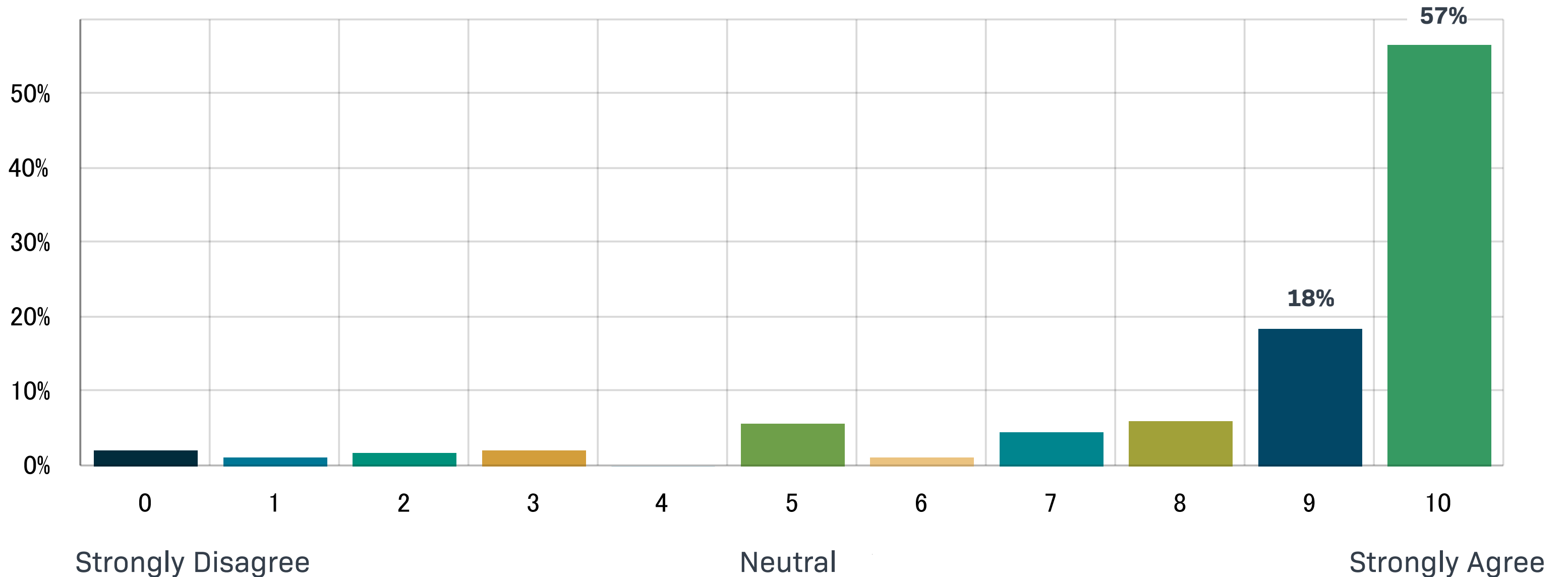
- Average rating is **5.6/10**; While **80%** agree that the area is car accessible, **only 19%** agree that it is accessible without a car (selected 8-10 on a scale of 1 to 10).
- Mixed; many see gaps outside of car travel



Importance of Multiple Transportation Options

(Access to multiple transportation options is important to me)

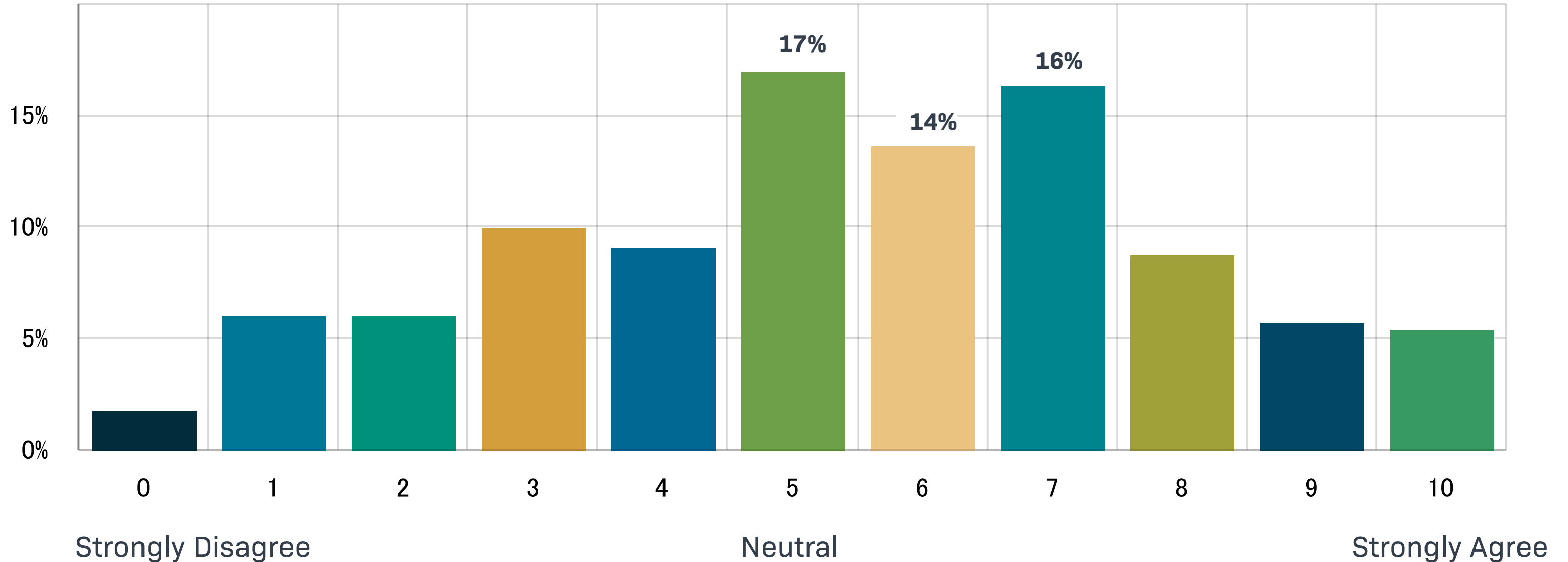
- Average rating is **8.6/10**
- Mobility choice is very important to respondents



Safety Walking/Biking/Rolling

(I feel safe walking, biking, and rolling in the Greater Heights area)

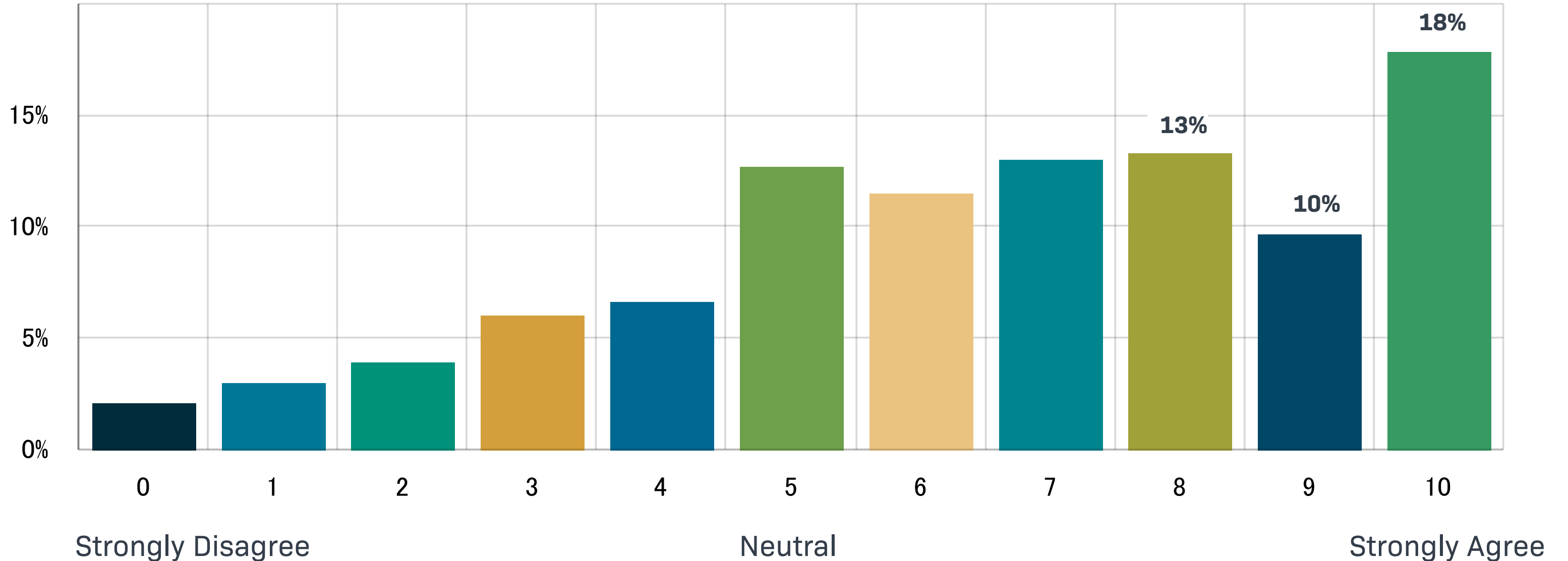
- Average rating is **5.4/10**
- Neutral perceptions of safety



Trail/Greenway Access

(I can comfortably access a trail or greenway in the Greater Heights area)

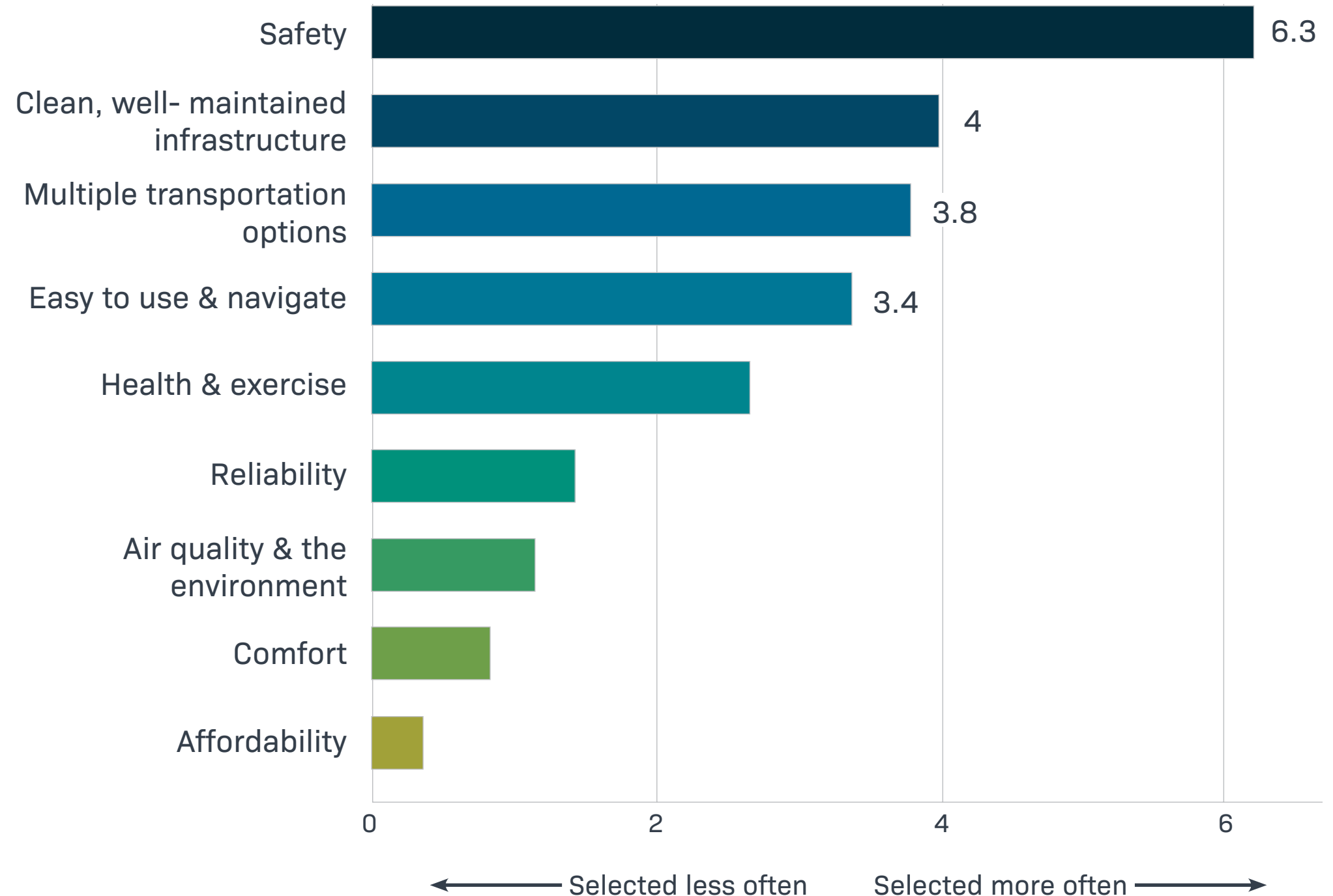
- Average rating is **6.5/10**; **41%** can comfortably access a trail or greenway (selected 8-10 on a scale of 1 to 10)
- Some access, but not consistent or easy for all



Values When Getting Around

(I value the following when choosing how I (and my family) get around - rank top 3)

- **#1 Safety**
- **#2 Clean, well-maintained infrastructure**
- **#3 Multiple transportation options**
- Also important: easy to use & navigate, health/exercise

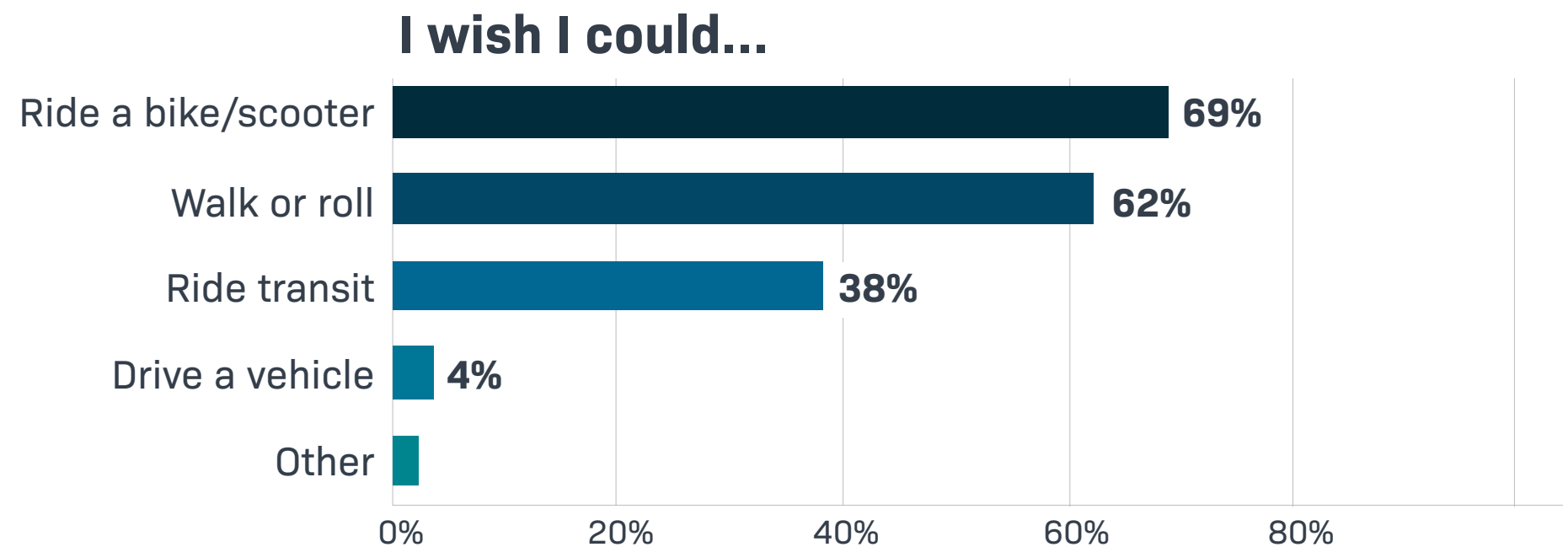
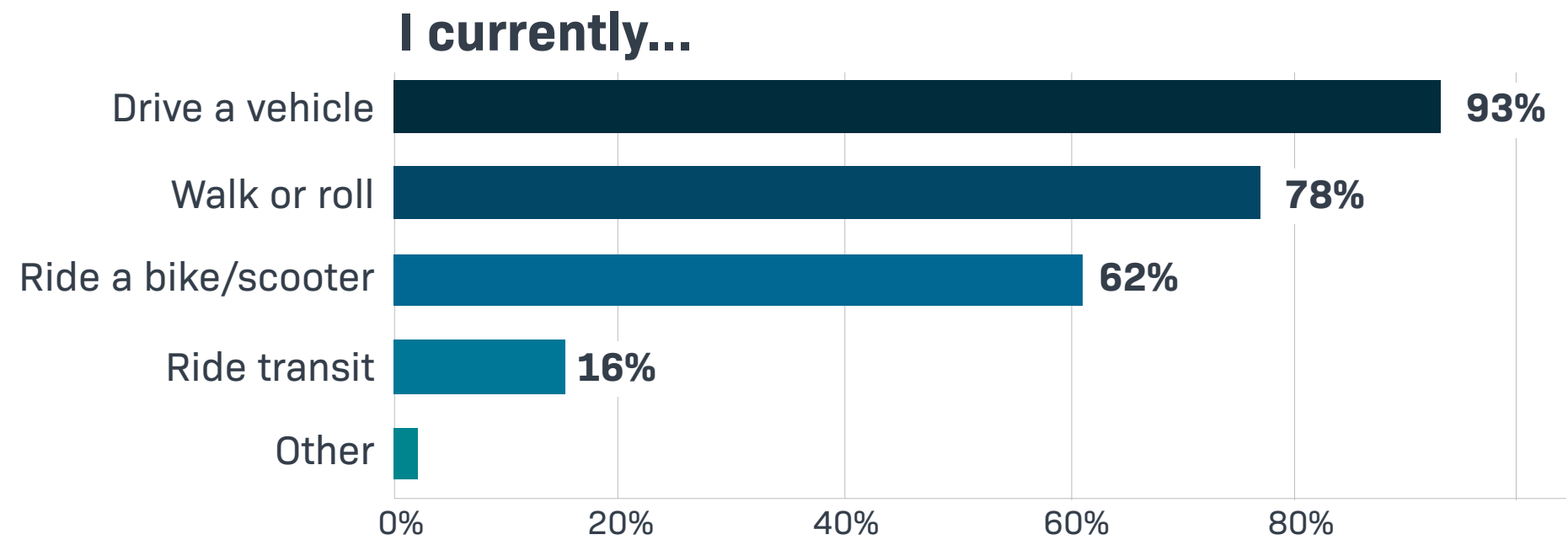


Current Vs. Wished-For Travel Modes

(Select all that apply)

Wished-For Modes (more often)

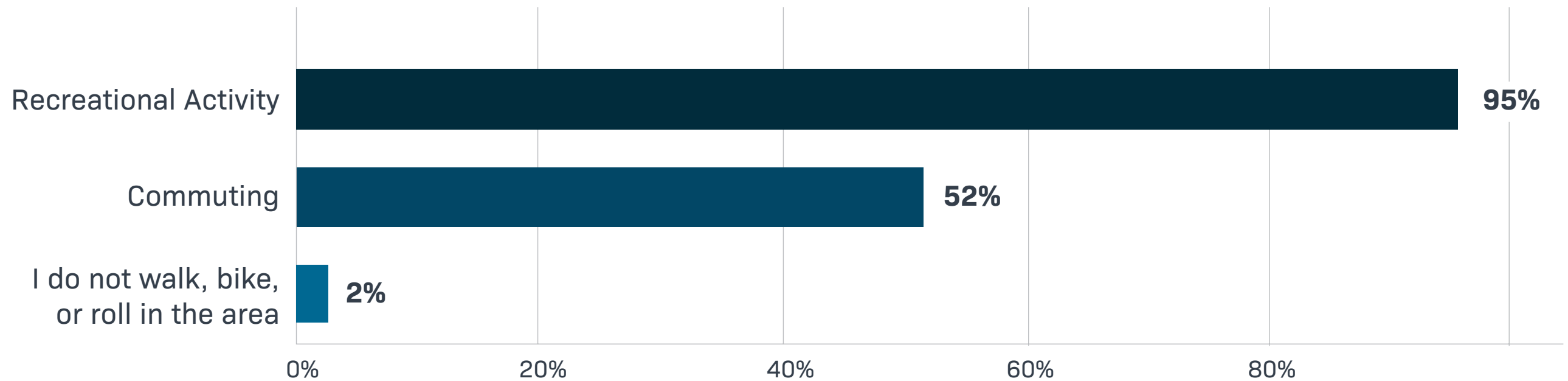
- # 1 Bike/scooter
- #2 Walk
- #3 Transit
- Very few wish to drive more (4%)



Responses may not add up to 100 percent because respondents could select all that apply.

Reasons for Walking/Biking/Rolling

(If I walk, bike, or roll in the Greater Heights area, I do so for... select all that apply)

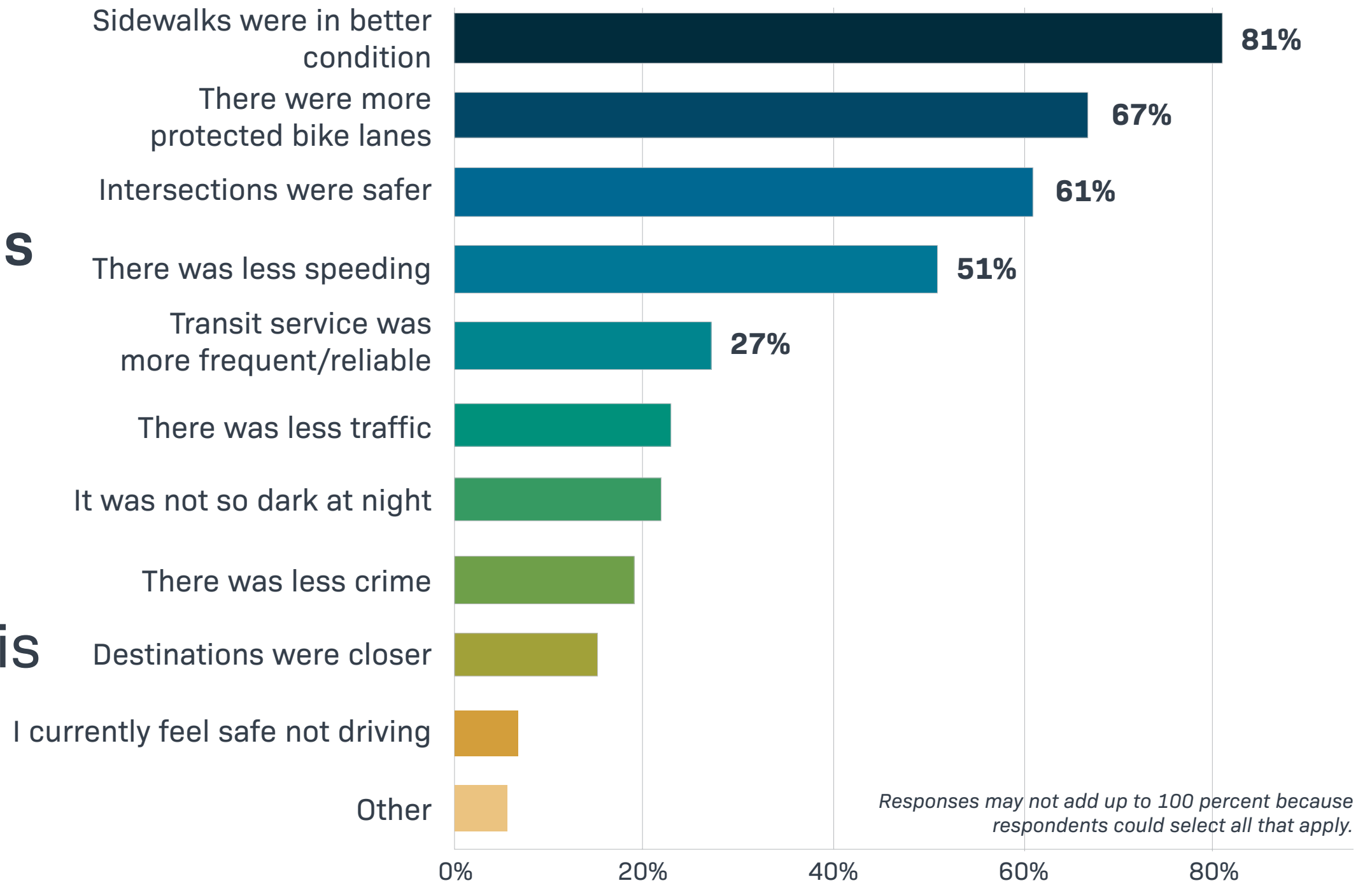


Responses may not add up to 100 percent because respondents could select all that apply.

Conditions That Would Improve Non-Driving Safety/Comfort

(When I'm not driving, I would feel safer and more comfortable traveling if...)

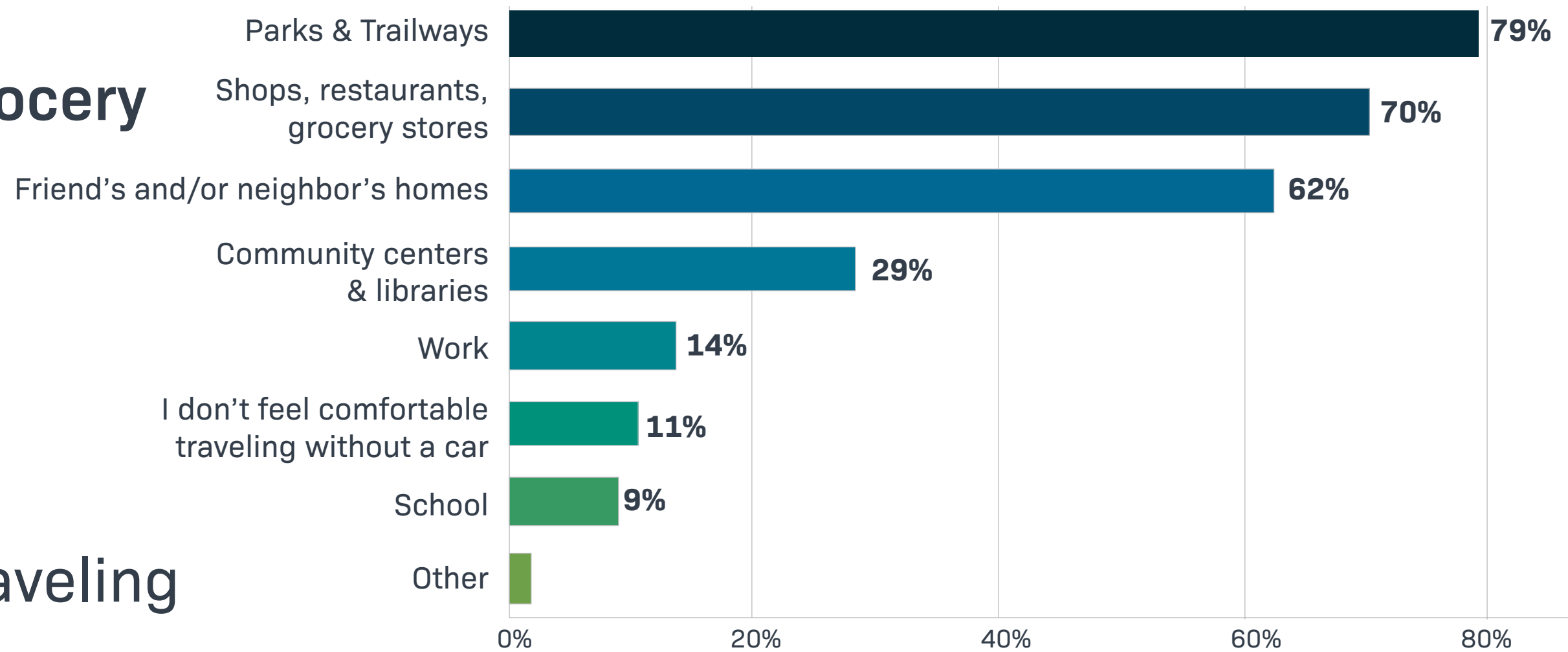
- **#1 Better Sidewalks**
- **#2 More Protected Bike Lanes**
- **#3 Safer Intersections**
- **#4 Less Speeding**
- **#5 More Frequent & Reliable Transit**
- **Only 7% feel safe as-is**



Comfortable Destinations Without a Car

(When I'm not driving, I currently feel comfortable traveling to...)

- **#1 Parks/trails**
- **#2 Shops/
restaurants/grocery**
- **#3 Friends/
neighbors**
- Only 14% said
work/school
- 11% don't feel
comfortable traveling
without a car



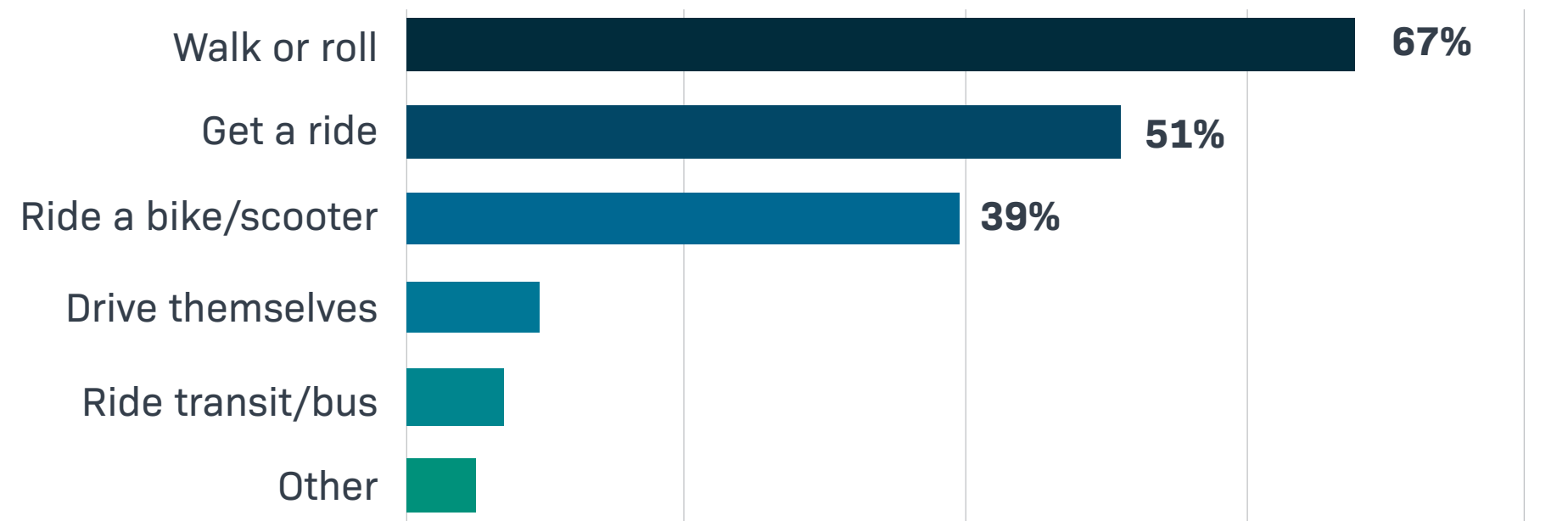
Responses may not add up to 100 percent because respondents could select all that apply.

Children's Travel Modes

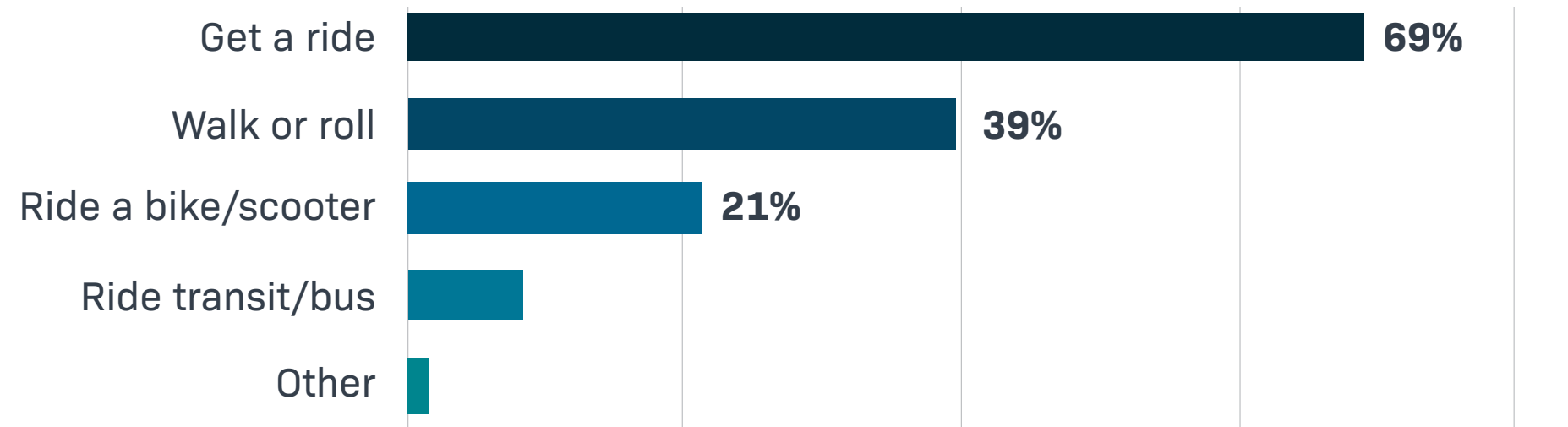
(My school-aged children regularly get around by...)

- **25% of respondents are parents of school-aged children**
- **92% accompany their child to school**
- **67% of kids regularly get around by walking but only 39% walk to school**
- **69% of kids get a ride to school**

On a regular basis



To school

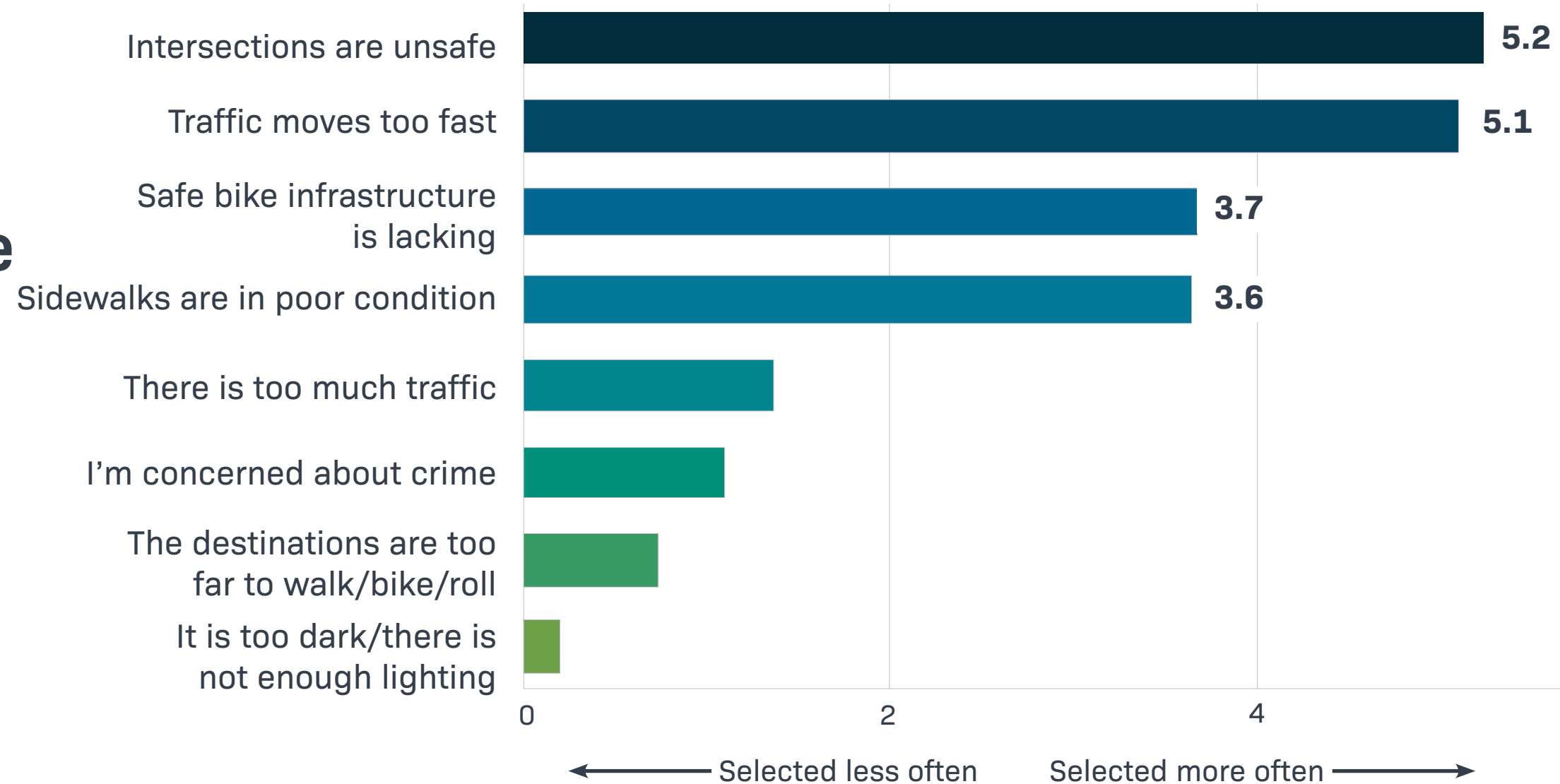


Responses may not add up to 100 percent because respondents could select all that apply.

Parent's Top Safety Concerns

(What are your top three safety concerns when thinking about your children traveling?)

- **#1 Unsafe Intersections**
- **#2 Speeding**
- **#3 Lack of Safe Bike Infrastructure**
- **#4 Poor Sidewalks**



Survey Comments: Key Themes

Community Aspirations

1. Connectivity & Trails

- Strong enthusiasm for connecting the Heights to Memorial Park, Buffalo Bayou, & White Oak Bayou
- Patterson Bridge & existing bridge improvements are mentioned often

2. Parks & Public Spaces

- Desire for more trees, shade, & beautification along trails

Respondent Quotes

“Waiting for a bridge/trail to **connect Cottage Grove and nearby neighborhoods to Memorial Park** so we, as senior citizens, can safely travel there without a car.”

“Interested in **Patterson Bridge**. I go to MKT every week and wish there was a safer and faster route to Patterson across the bayou.”

“We need more **street trees** and need to **prioritize beauty and placemaking** in our infrastructure projects.”

“The sidewalks and bike lanes and trees north of 15th are a fantastic improvement. I travel this stretch daily by car mostly and **it’s so pretty now and inviting**. As the weather cools I’ll be on my bike...”

Survey Comments: Key Themes

Community Aspirations

3. Walking & Biking

- Many families value walkability and biking as a core part of the area's character
- Excitement for extending MKT and other trails to make more car-free trips possible

4. Transit & Mobility Options

- Support for circulator shuttles and improved bus service
- Enthusiasm for express connections to Downtown & Galleria

Respondent Quotes

“My friends all love the Heights cause **we can bike and walk there and it's safe and pleasant.** My [family] loves Donovan Park...because there's **restaurants within walking distance.**”

“More consistent sidewalks and crosswalks would be a dream. **There are walkable businesses I'd like to visit but it's almost impossible without a car** or taking additional risk.”

“We need more options for **safe multimodal transportation** for work, errand running, leisure, shopping, and environmental reasons.”

“Please consider **expanding services like circuit or the circulator bus in DC** to link areas with lots of businesses and entertainment.”

Survey Comments: Key Themes

Challenges & Concerns

1. Sidewalks & Safety

- Broken, missing, or blocked sidewalks push families into streets
- Intersections and crossings feel unsafe
- Concerns about schoolkids walking and biking without protection

2. Traffic & Speeding

- Concerns around speeding, cut-through traffic, and lack of enforcement
- Desire for more traffic calming measures

Respondent Quotes

“I’ve been run off the road multiple times with my children in a stroller and thrown into a ditch by careless drivers. All because **I’m having to walk into the street due to gaps in the sidewalks.**”

“Need a continuous sidewalk on 13th between Durham and Shepherd to access Love Elementary safely...**So UNSAFE to get to Love with a child!!!** Almost get smoked by a car everyday....”

“I really want major roads - like Houston Ave - to accommodate me in both my car and my bike (and on foot). Currently, **it is very hard to navigate given the speed of cars driving fast** in wide lanes.”

“The Greater Heights area is primed to be a leading Houston example for walk and bike alternatives, but **we dramatically need increased traffic calming measures** (TC Jester, Durham, and Shepherd are still not safe to use on a regular basis.”

Demographic Overview

(Based on respondents who filled out the full survey)

Disabilities

- **94% reported no disability;** small numbers noted mobility or vision disabilities

Age Distribution

- Largest group: **35-44 (32%),** followed by **25-34 (29%)**
- Very few under 24 or over 75

Race

- **83% White,** 7% Asian, 3% Biracial/Other

Ethnicity

- **16% Hispanic/Latino;** 84% Not Hispanic/Latino

Language at Home

- **99% English,** 14% Spanish, 4% Other

Gender Identity

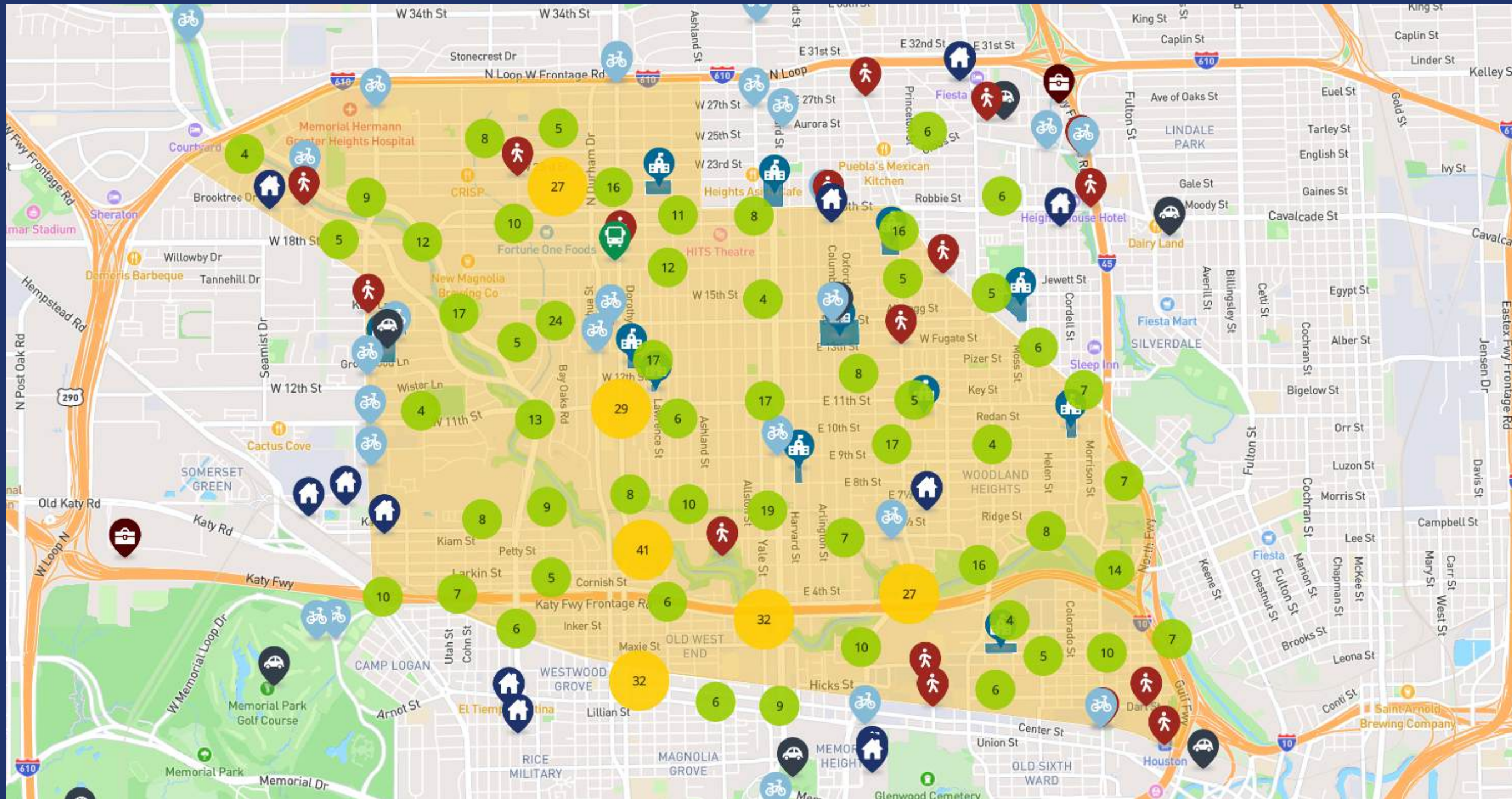
- Roughly even split: **49% Female,** 48% Male

Housing

- **82% own homes,** 8% rent, 10% live outside the area



Map My Vision Overview



Greater Heights Strategic Connections Plan

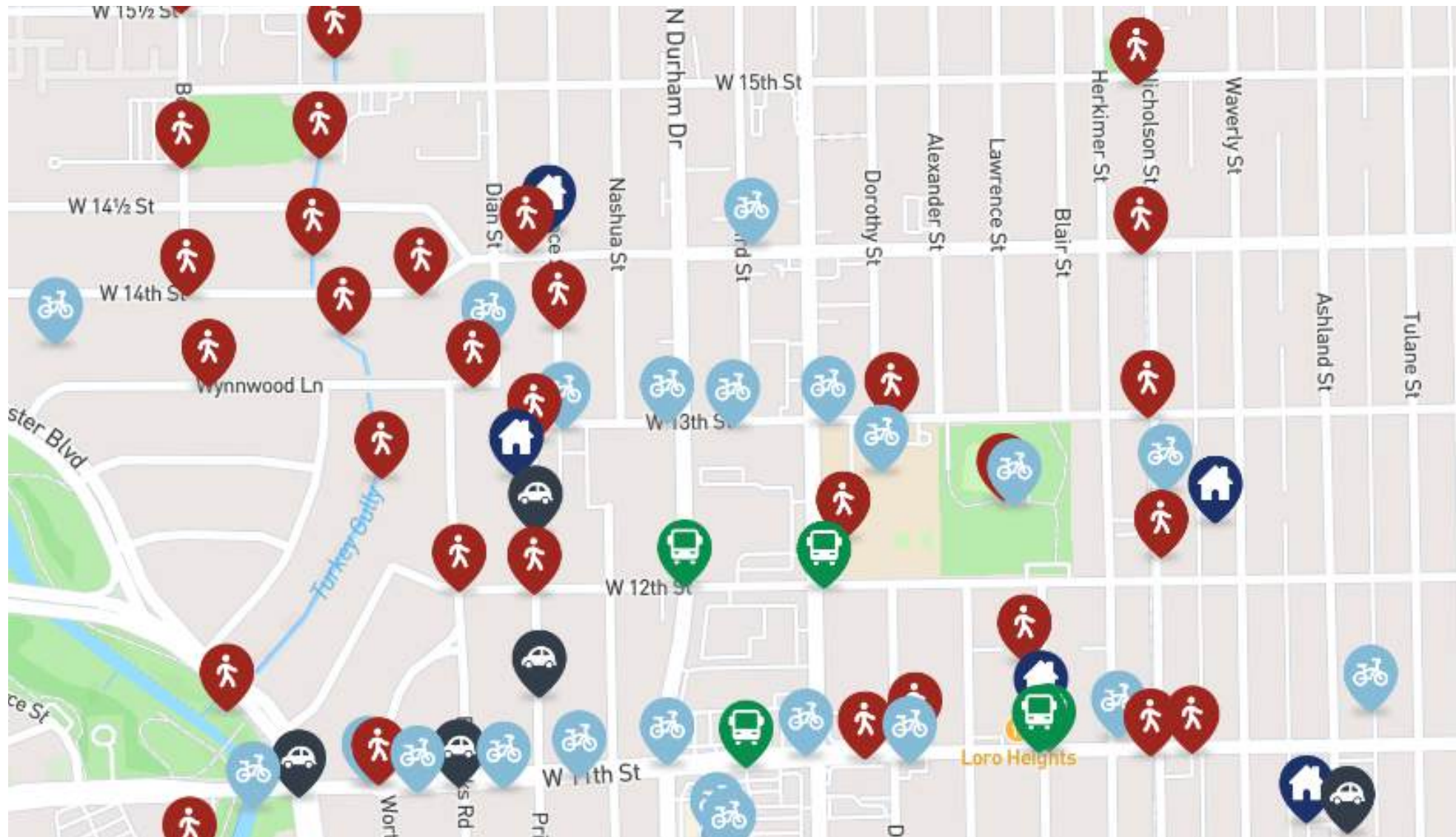


Map My Vision

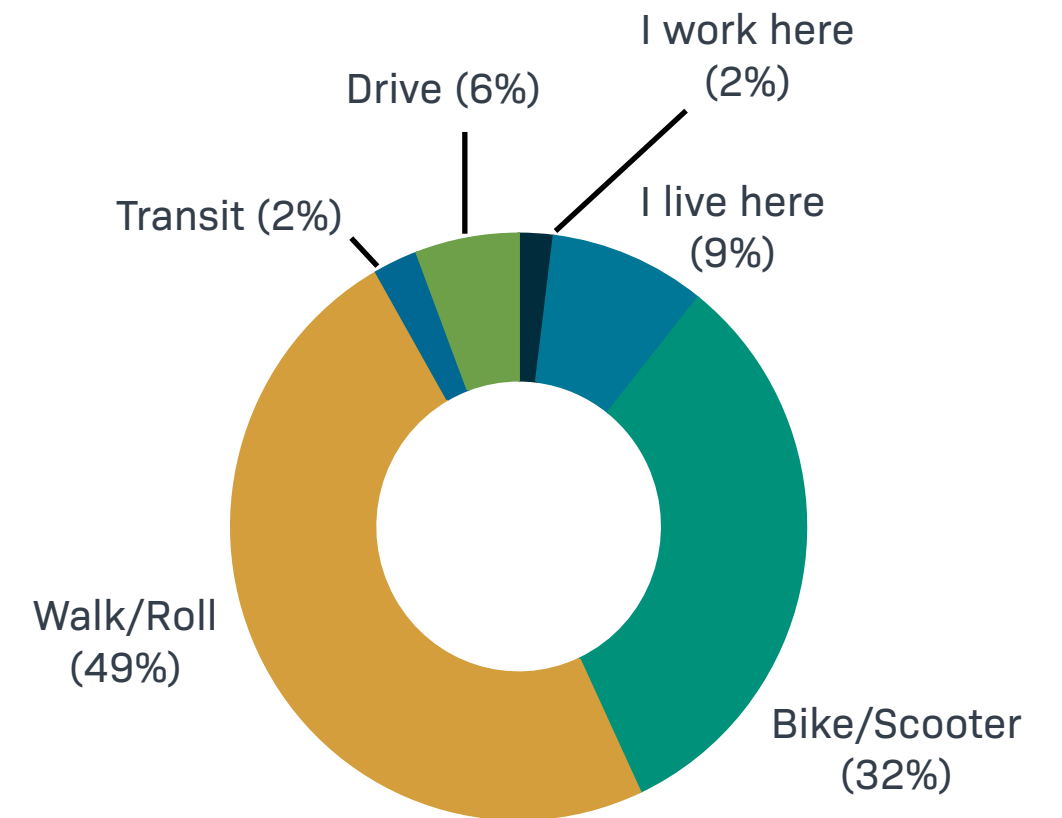
764
Posts

101
Contributors

What destinations do you wish were more accessible by driving, walking/rolling, biking, and taking transit?



Portion of Pin Types Used



Map Comments: Key Themes



1. Connectivity & Crossings

- Strong demand for safe pedestrian and bike crossings over barriers like I-10, White Oak Bayou, and major corridors (*Shepherd, Durham, Yale, N Main, Washington, Studewood*)
- Patterson Bridge and Hogan Bridge repeatedly mentioned as **critical but unsafe, closed, and/or needed connections**
- Desire for new bridges, underpasses, and continuous trail connections

2. Sidewalks & Pedestrian Safety

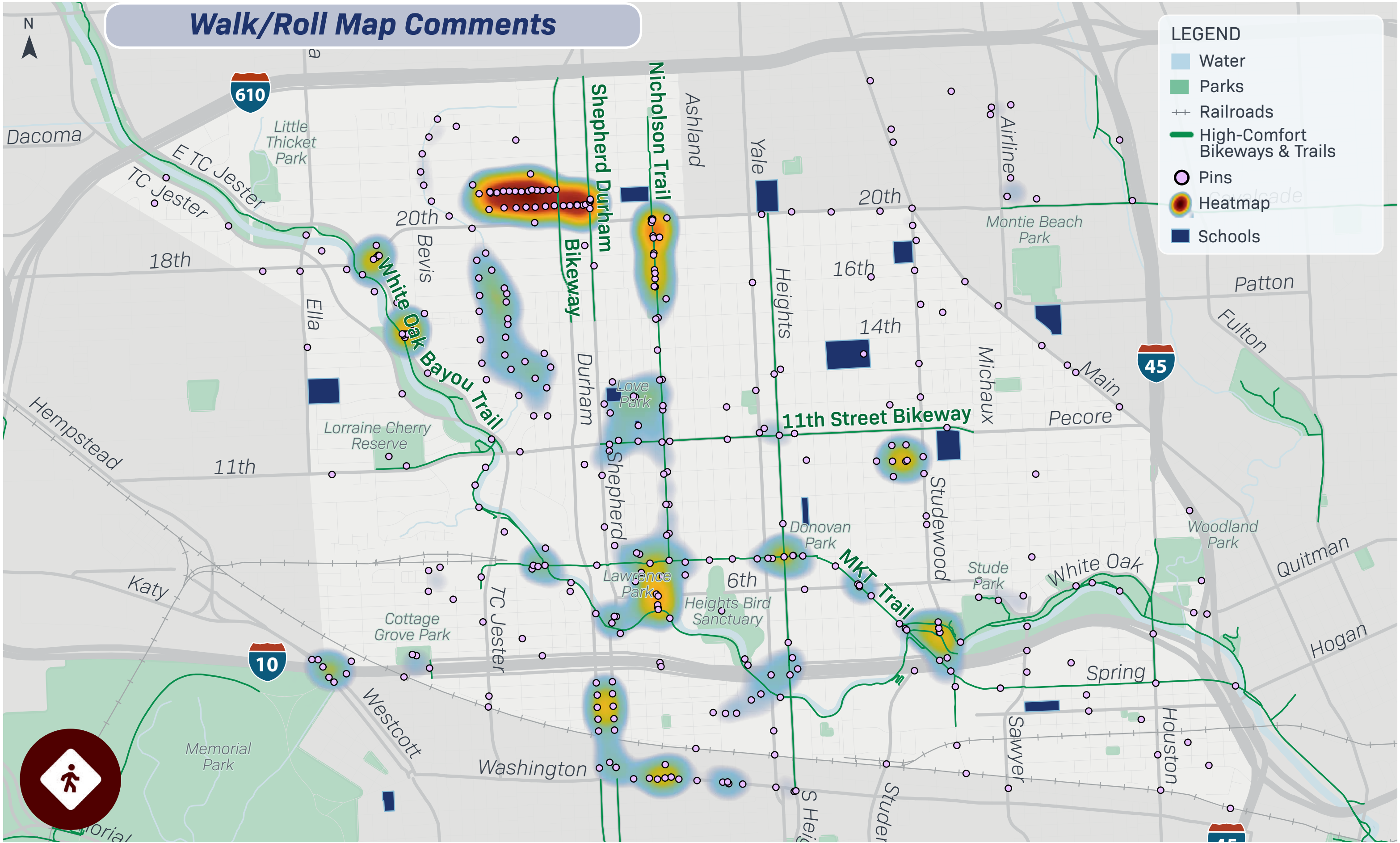
- Many sidewalks are missing, broken, or too narrow (*Yale, Studewood, White Oak, Oliver, Ella*)
- **Dangerous crossings near schools** (*Love, Field, Harvard, Browning Elementary*) due to speeding traffic and lack of safe pedestrian infrastructure
- Requests for median refuge islands, raised crosswalks, curb cuts, and ADA accessibility

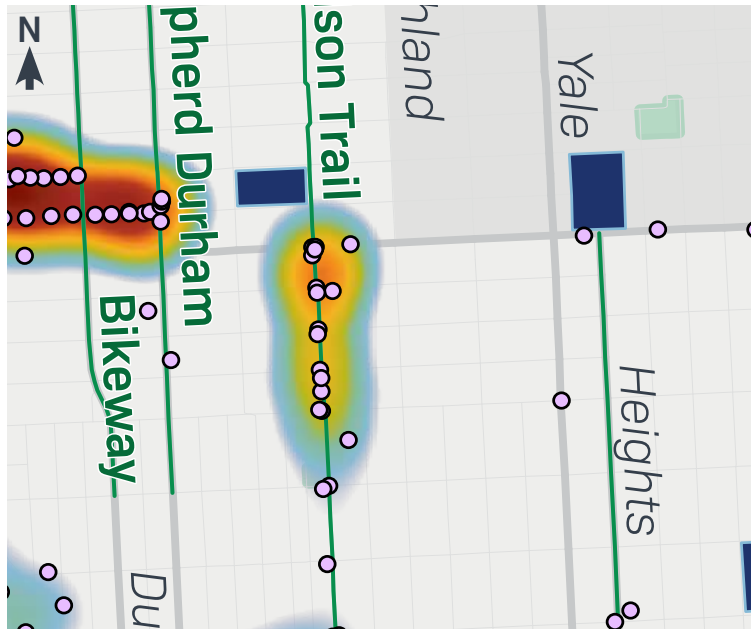


Walk/Roll Map Comments

LEGEND

- Water
- Parks
- Railroads
- High-Comfort Bikeways & Trails
- Pins
- Heatmap
- Schools





Respondent Quotes

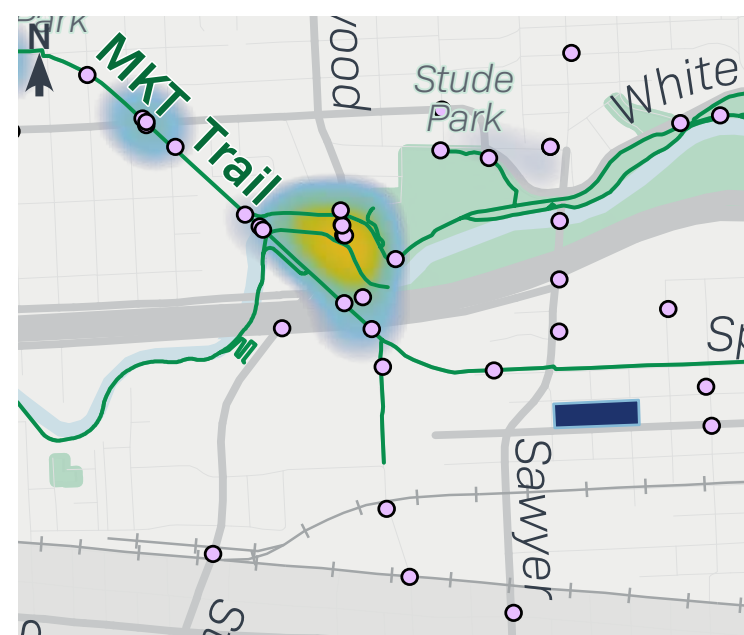
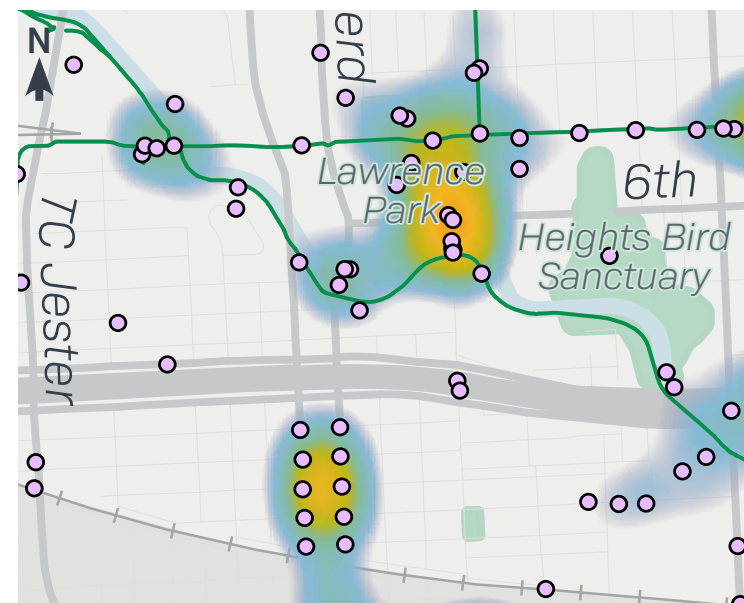
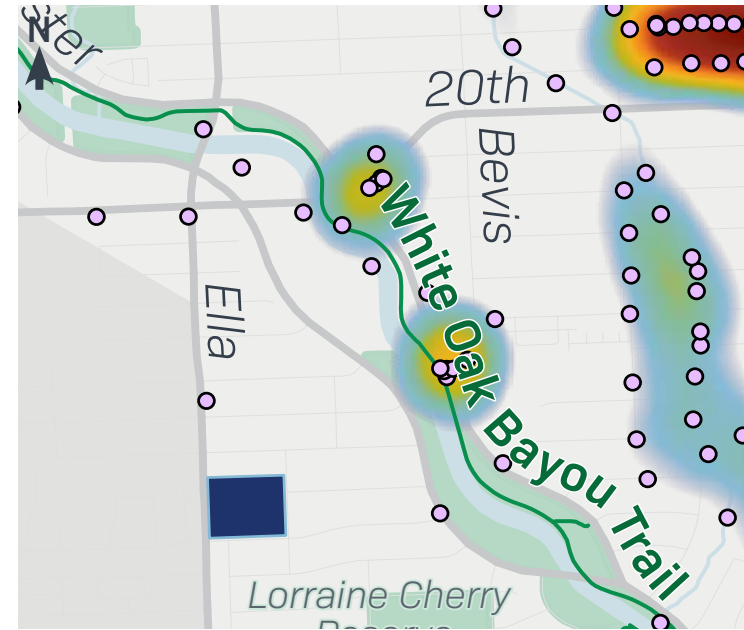
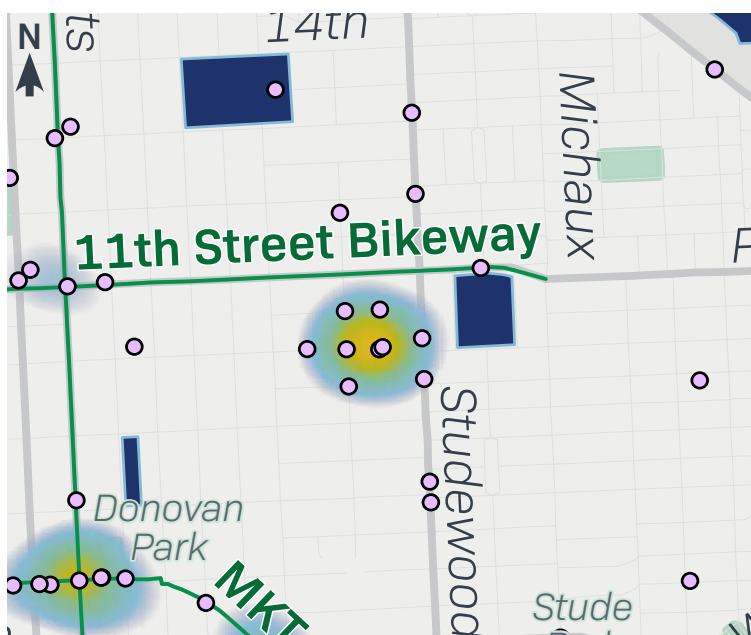
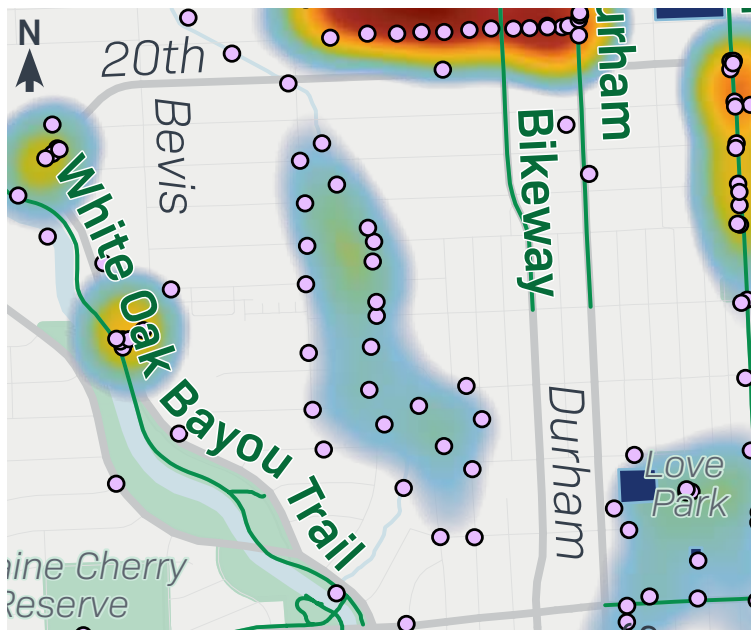
“20th is a 4-lane road where **drivers can speed and mostly refuse to yield to people** in the crosswalk... At a minimum needs to be repainted but a 4-way stop or HAWK is much better. **The foot and bike traffic on this part of Nicholson is impressive.**”

“**Connect all sidewalks to 19th and 20th.**”

“**Turkey Gully greenway.**”

“**Many dog walkers and strollers with children** in this area.”

“**Lots of schoolchildren use 10th to walk home.**”



Respondent Quotes

“**Sidewalk is in terrible shape here.** I use this to get to WOB Trail.”

“**We need to have a crossing here.** It’s infuriating to navigate TC Jester and then go through a mud pit. I can’t walk our stroller here...”

“**Pedestrian/bike bridge across White Oak Bayou** connecting Patterson St to MKT Heights.”

“**Missing crosswalks at intersections.**”

“**Studemont bridge pedestrian ROW is narrow and dangerous.**”

“**Can’t get to the trail from Studewood St sidewalk.**”

Map Comments: Key Themes

3. Biking Infrastructure & Trail Gaps



- Calls for protected bike lanes on key corridors (*20th, Houston Ave, Ella, Heights Blvd*)
- Trails often dead-end or lack safe connectors, forcing detours or unsafe mixing with traffic (*MKT/Memorial Park, WOB/11th, Shepherd Durham gaps*)

4. Traffic Calming & Driver Behavior










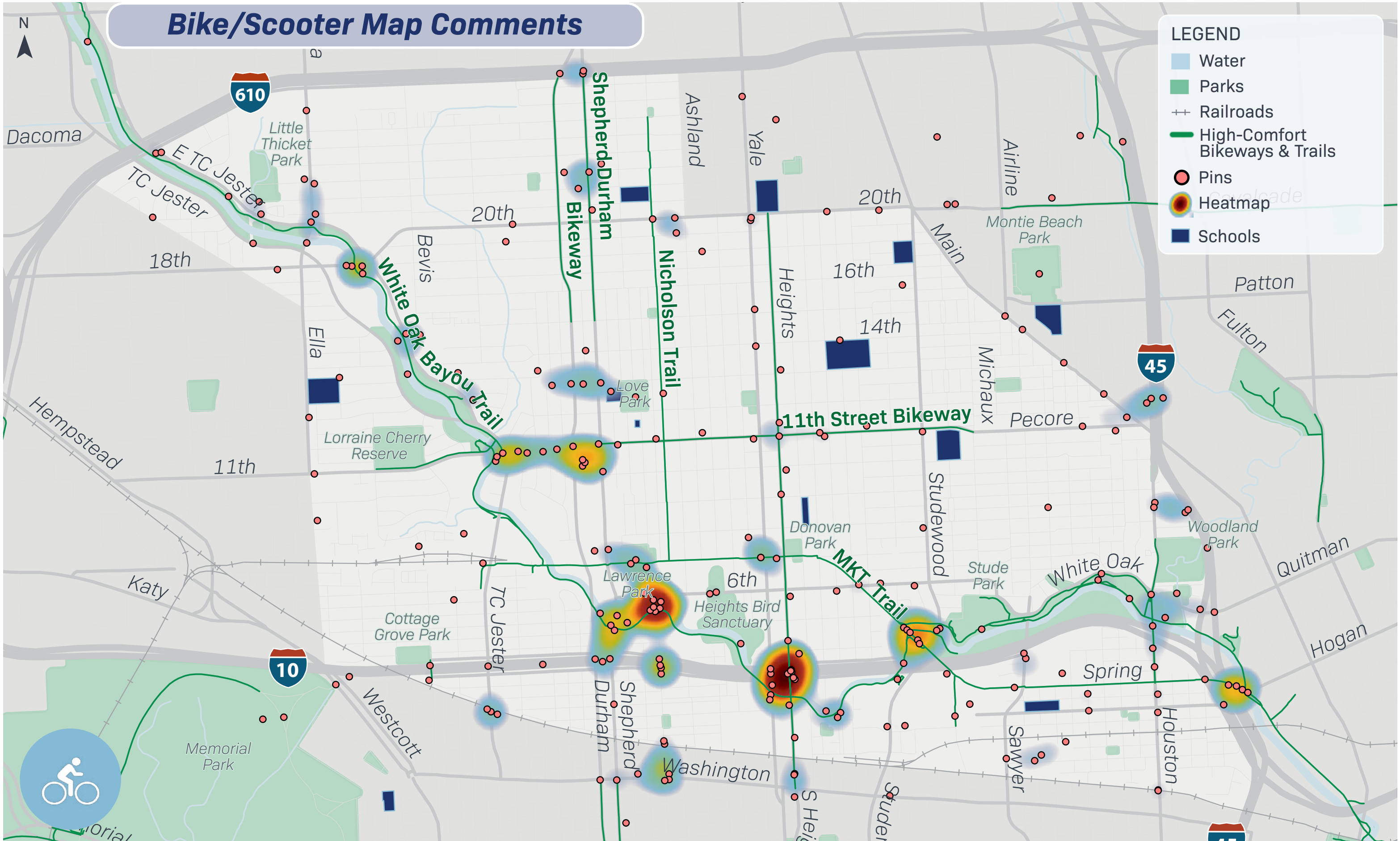
- Cars speeding on major corridors and residential streets (*White Oak, TC Jester, N Main, Crockett, 20th*)
- Frequent failure of drivers to yield to pedestrians at crosswalks
- Suggestions for more stop signs and traffic calming (*especially on Nicholson and near parks*)

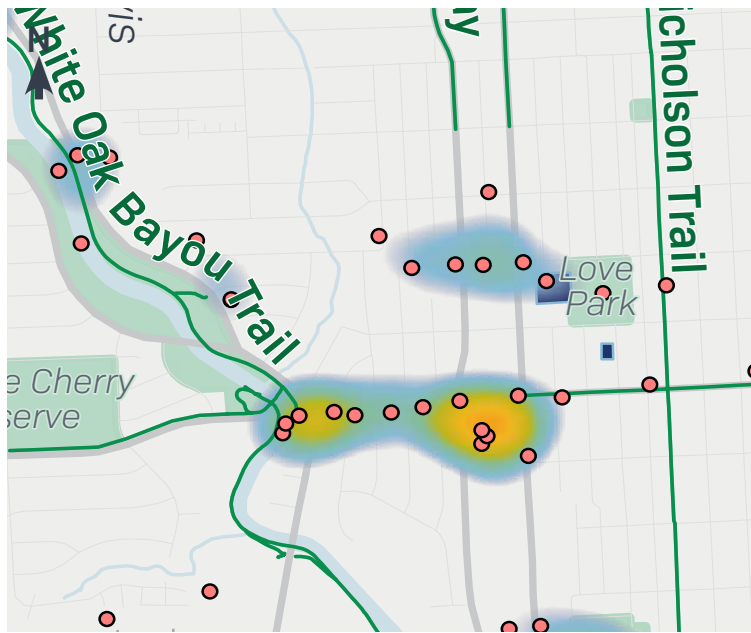


Bike/Scooter Map Comments

LEGEND

-  Water
-  Parks
-  Railroads
-  High-Comfort Bikeways & Trails
-  Pins
-  Heatmap
-  Schools

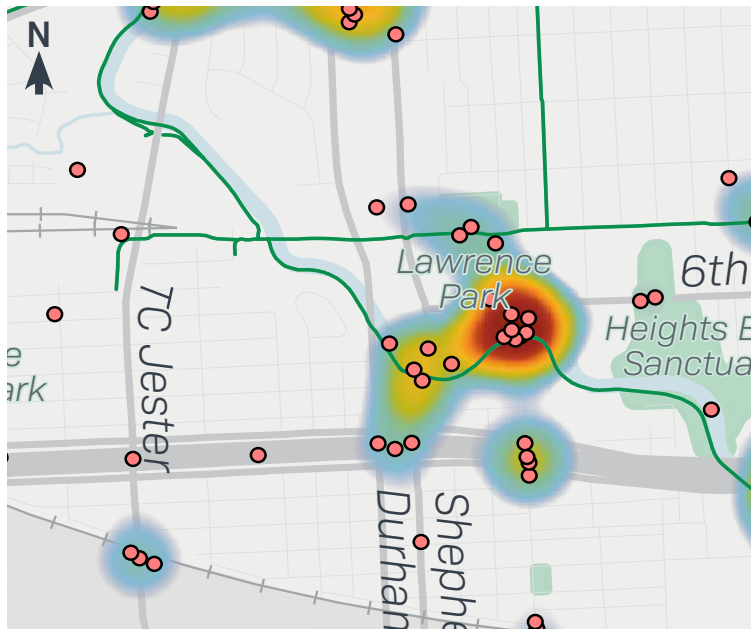




Respondent Quotes

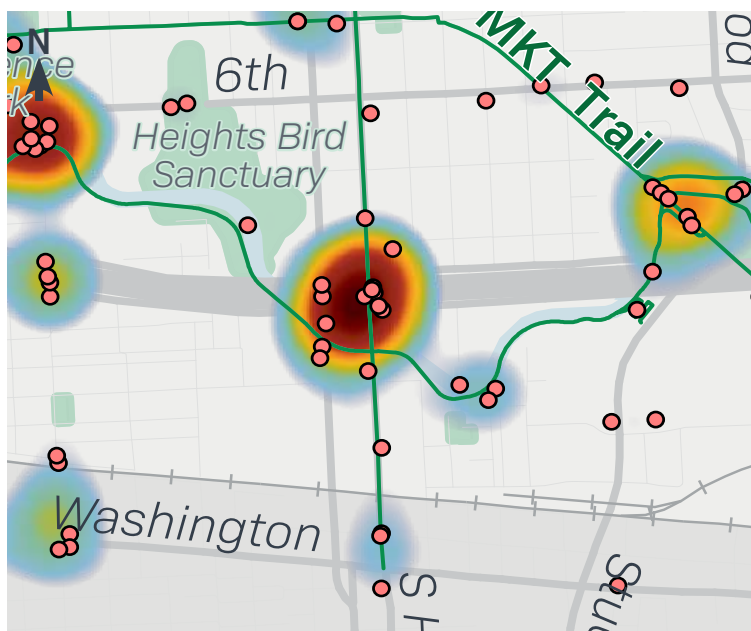
“Connecting 11th to Shepherd/Durham bike lanes, and going all the way to White Oak Bayou Trail.”

“I want to bike to **Kroger.**”



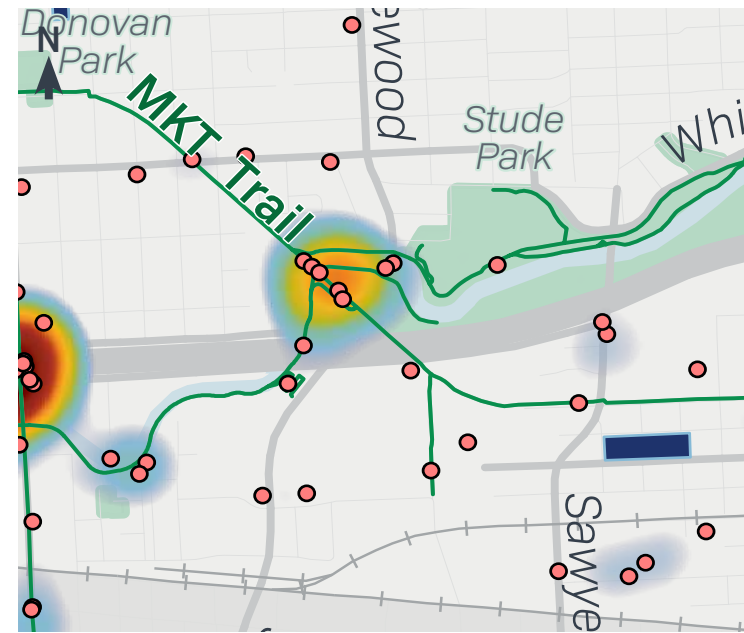
“Please add a **connector from WOB Trail to Shepherd Dr.** Currently there is no way for trail user to get to businesses on Shepherd.”

“Bridge crossing Katy Freeway along Patterson is a great way to **bike from the Heights to Rice Military.**”



“Heights Blvd bike lane under I-10 **needs to be protected.**”

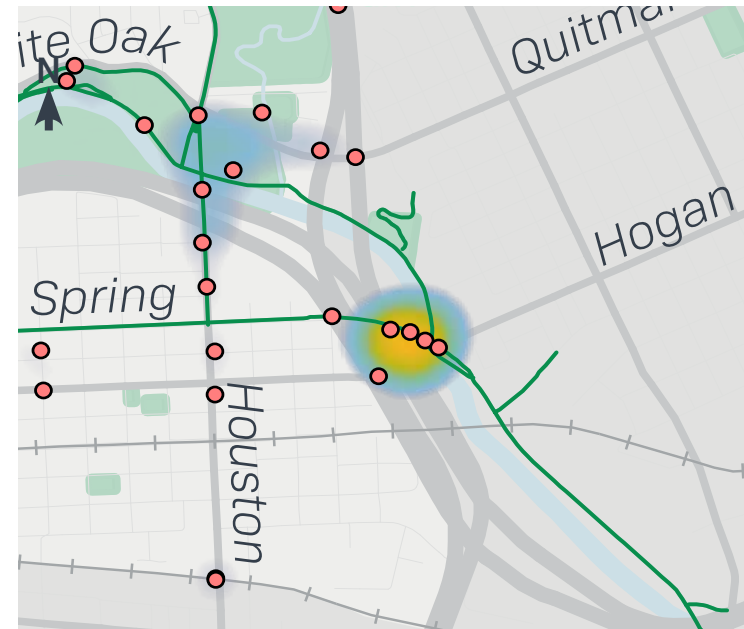
“**Poor pathways** under I-10 Yale/Heights.”



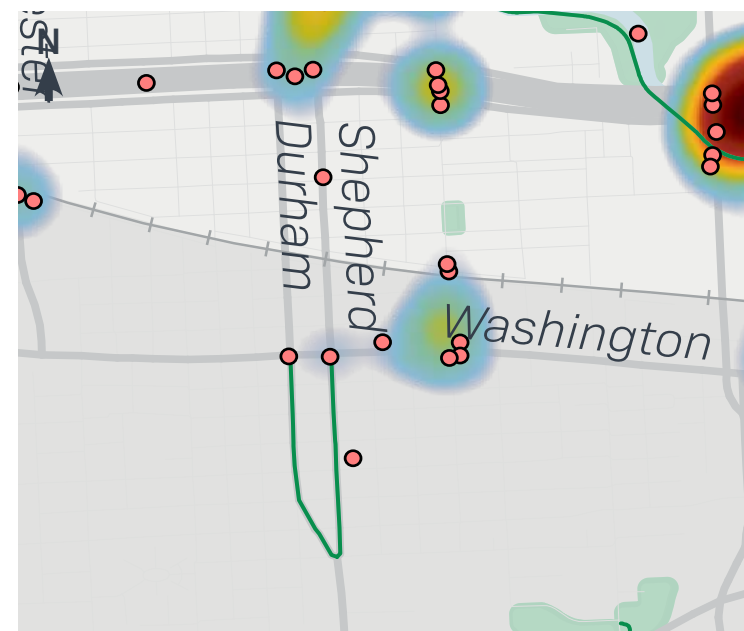
Respondent Quotes

“Fix the bridges!”

“**Studemont bridge is a key connector** for accessing the Heights from south of I-10.”










“**Hogan bridge is a key connector** for accessing downtown and buffalo bayou.”

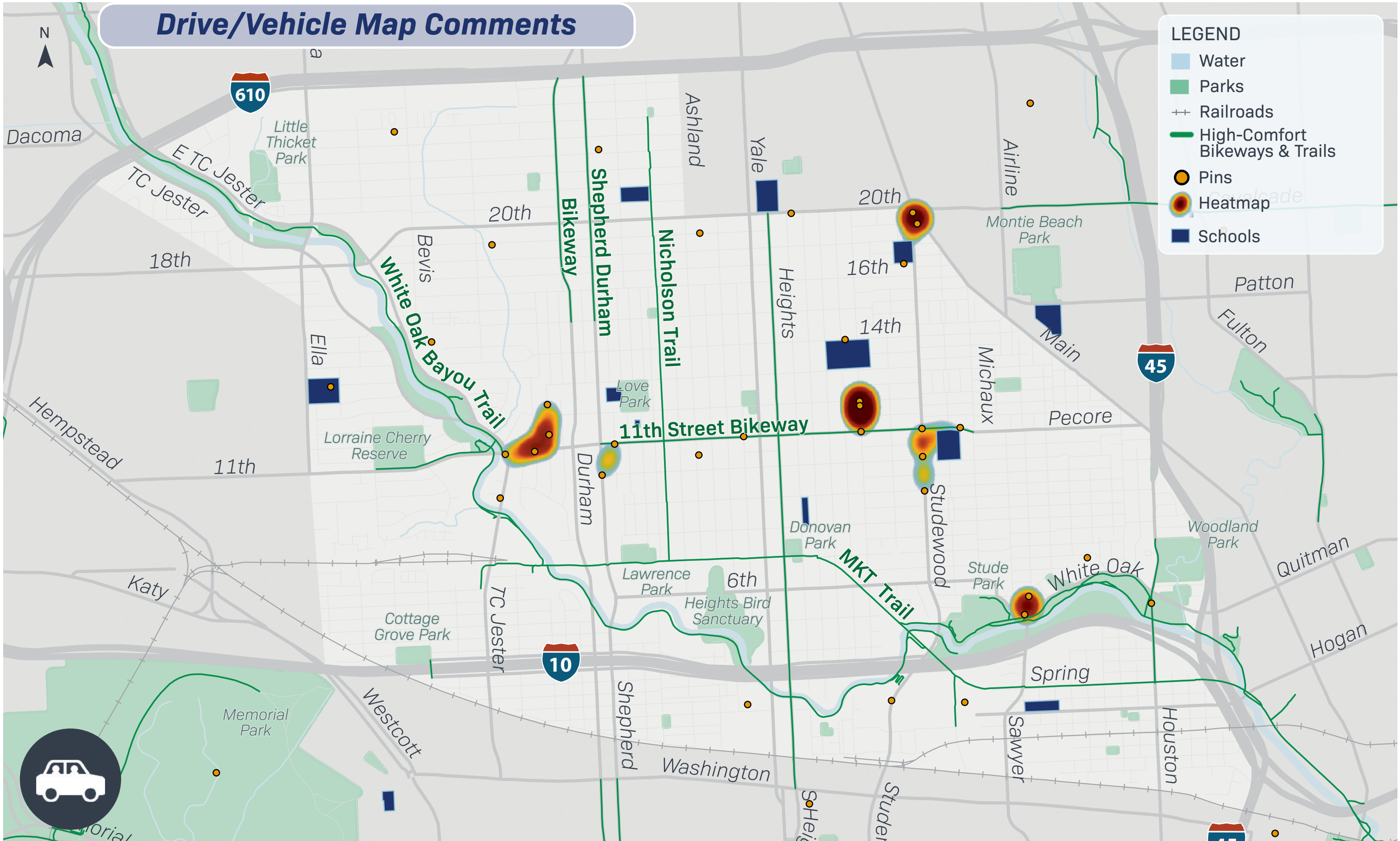


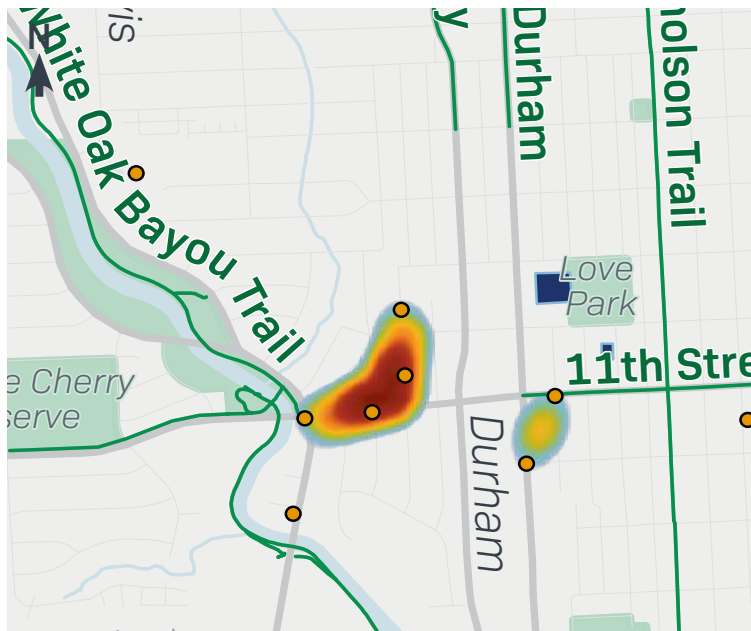
“Need **north/south thoroughfare for hike/bike** going from general Heights Blvd to Buffalo Bayou trail... could even extend further to Braeswood Bayou eventually.”

Drive/Vehicle Map Comments

LEGEND

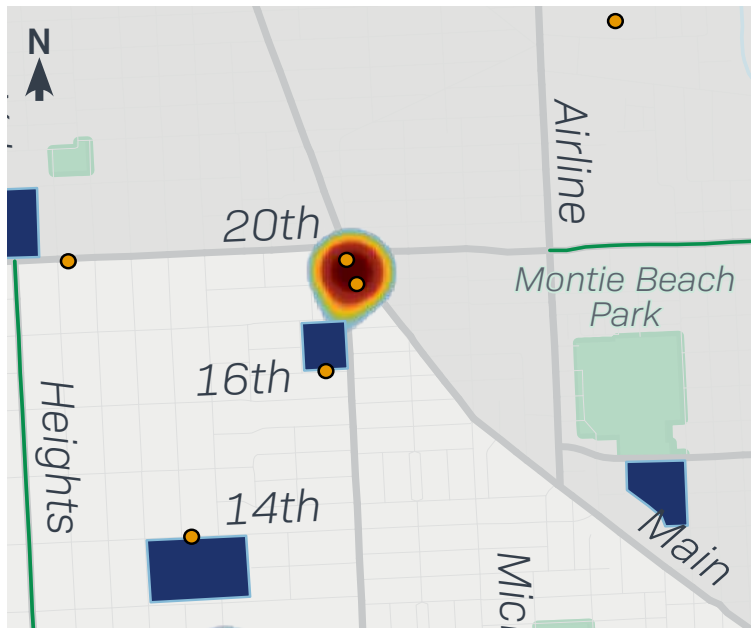
-  Water
-  Parks
-  Railroads
-  High-Comfort Bikeways & Trails
-  Pins
-  Heatmap
-  Schools



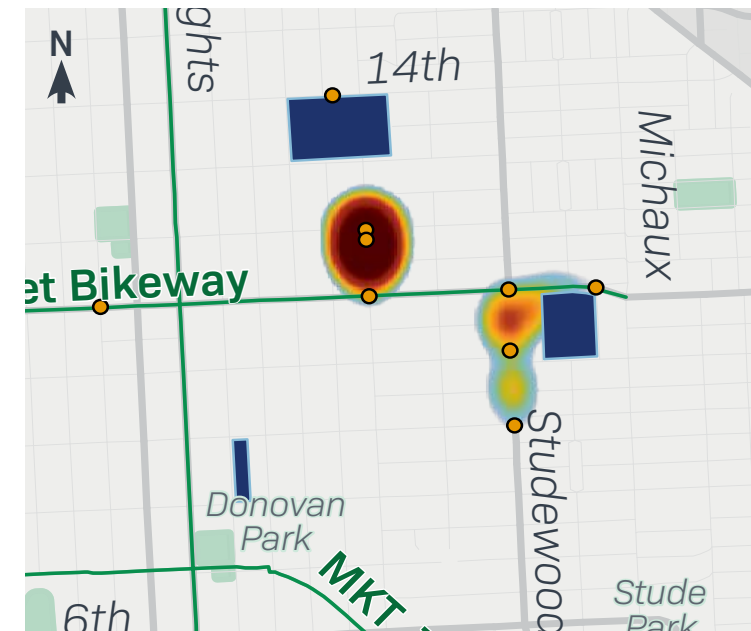


Respondent Quotes

“Would be good if 10th would get a **street light to go into Merchant’s Park**. And reduce the number of driveways on Merchant’s Park.”



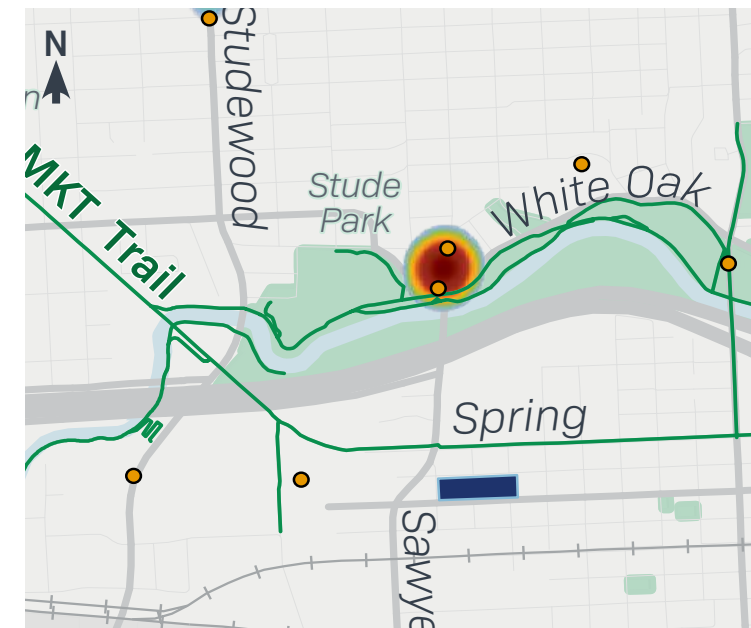
“I **access businesses** here.”



Respondent Quotes

“**Driver’s view is blocked** when turning from Oxford onto 11th.”

“**I use Studewood regularly to drive** to other places and businesses in Houston.”



“I **drive here many times to go downtown.**”

“I drive here frequently **to go to Target and other businesses.**”



Map Comments: Key Themes

5. Parks, Greenways, & Public Space Enhancements

- Enthusiasm for more greenspaces and shaded trails (MKT expansion, Turkey Gully)
- Calls for better lighting, security and amenities (drinking fountains, benches)
- Desire for easier access to existing parks (Little Thicket, Woodland, Love)

6. Transit & Access to Destinations

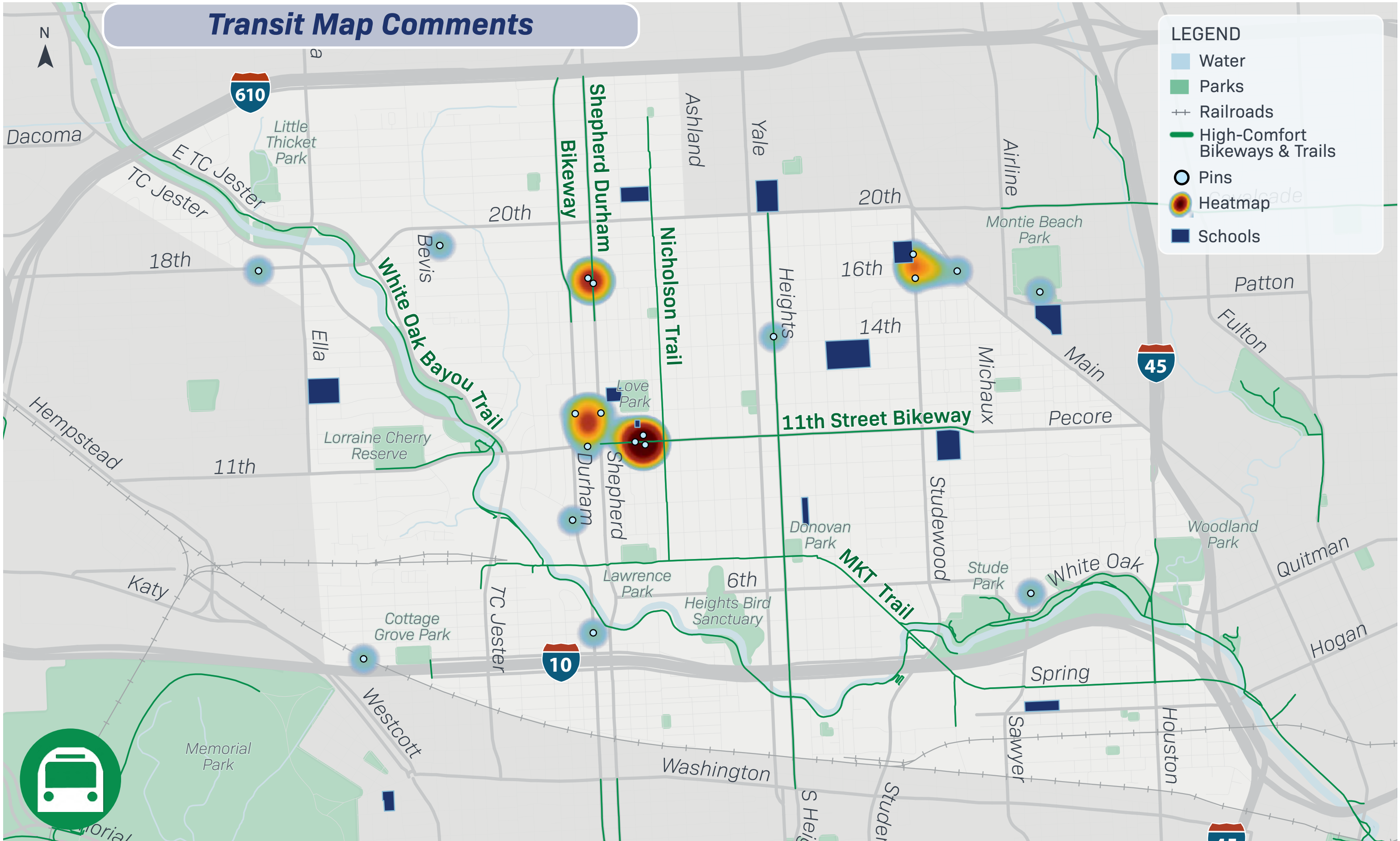


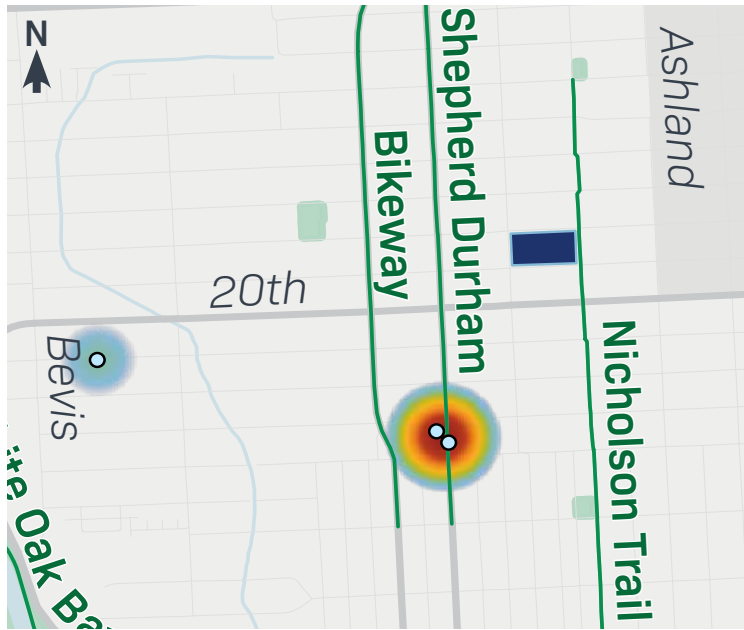
- Requests for better bus connections and safer access to stops (along 11th, Patterson, Shepherd)
- Frustration that local businesses, groceries, gyms, and schools are hard to reach without a car because of unsafe crossings

Transit Map Comments

LEGEND

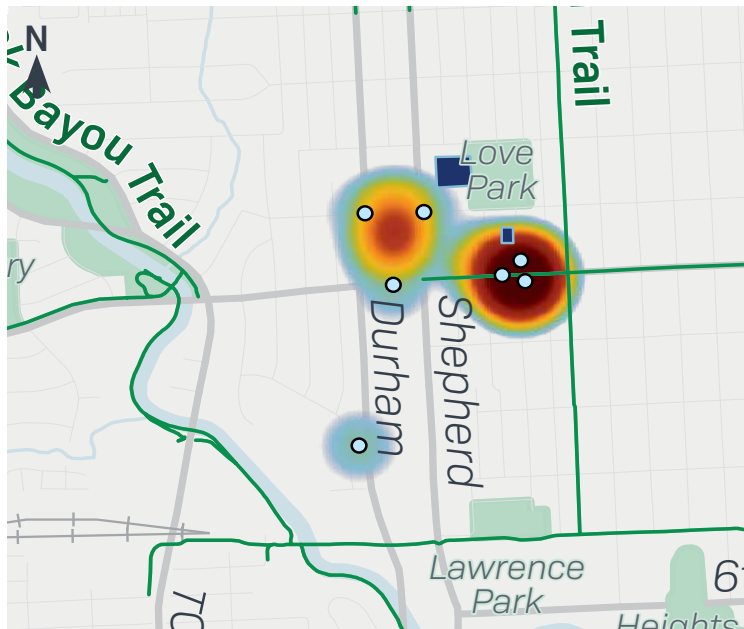
- Water
- Parks
- Railroads
- High-Comfort Bikeways & Trails
- Pins
- Heatmap
- Schools





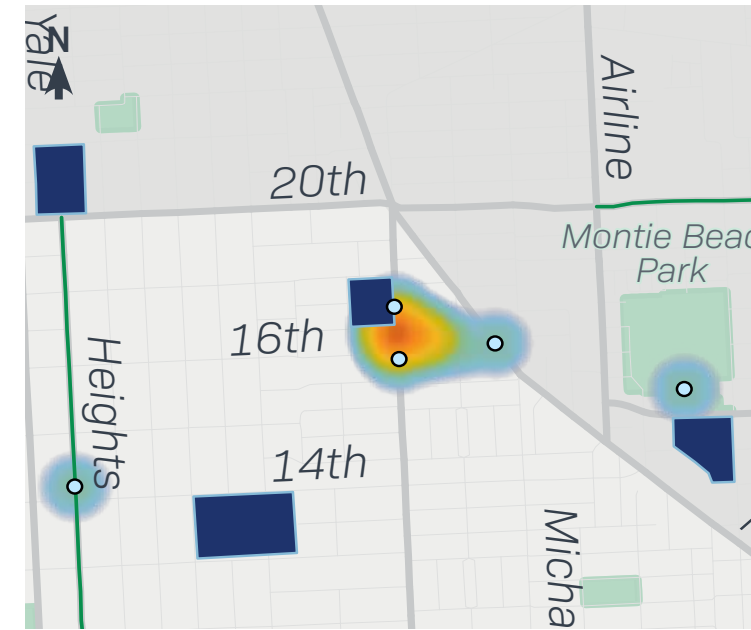
Respondent Quotes

“This could be a great transit corridor **if there was frequent service and nice places to wait!**”



“11th Street bus stop has no **shelter or place to sit.**”

“SB bus stop **needs access to MKT Trail.** Currently can only be reached by walking down from 11th or playing Frogger across Durham.”



Respondent Quotes

“Bus stop not **wheelchair accessible.**”

Summing it All Up: Major Themes & Goals Discussion

Survey and map comments were summarized into themes, which helped to shape the Vision and Goals for the planning effort.

1. Connectivity & Crossings

Themes

- People want **safe, direct ways to cross barriers** like I-10, White Oak Bayou, and major corridors
- Existing bridge closures and the Patterson Bridge repeatedly flagged as **critical but missing links**
- **Existing and future trails are desired** to stitch together neighborhoods, parks, and destinations

Goal Framing

- Build/repair pedestrian/bike bridges
- Close trail gaps
- Connect through barriers (underpass/overpass lighting, ADA access, wider sidewalks)

2. Sidewalks & Pedestrian Safety

Themes

- Many sidewalks are missing, broken, narrow, or blocked
- Parents cite unsafe school crossings
- Intersections are wide, chaotic, and poorly marked. Families with strollers and people with disabilities are often forced into the street

Goal Framing

- Continuous, ADA-compliant sidewalks
- Key crossing interventions
- School safety zones/
safe routes to school

3. Bike Infrastructure & Trail Network

Themes

- People want more **protected bike lanes** extended on key connector corridors
- Current **unprotected bike lanes feel unsafe**
- Trails are popular but often **dead-end, flood, or lack safe connections** to neighborhoods and businesses

Goal Framing

- Protected bike lane network on priority corridors
- Safe connections to schools, shopping, gyms, transit stops
- Trail maintenance and connectivity

4. Traffic Calming

Themes

- **Cars speed through residential areas and often fail to yield at crosswalks** (Nicholson crossings mentioned repeatedly)
- **Some intersections are too wide and crossing feels unsafe**

Goal Framing

- **Traffic calming at key corridors and intersections**
- **Prioritize safety interventions at high-crash corridors**

5. Parks, Greenways, & Public Spaces

Themes

- Enthusiasm for **expanding green spaces, shaded trails, and community amenities**
- Requests for **trees, shade, lighting, and seating** to make trails more inviting
- Calls to **integrate natural areas** (Little Thicket, Woodland) more directly into the network

Goal Framing

- Expand trail-connected parks and greenways
- Shade, trees, benches, water fountains, lighting along major trails
- Activate underutilized spaces (underpasses) as community assets

6. Transit & Multimodal Access

Themes

- Transit is **underused but desired** - people want better transit access
- Bus stops **lack safe access, shelters, or seating**
- People want **better connections between transit and trails**

Goal Framing

- Improve bus stop access and amenities (shelters, ADA, crosswalks)
- Improve access to area destinations and trails

7. Safety, Security, & Maintenance

Themes

- Trails and underpasses **sometimes feel unsafe** due to crime, encampments, poor lighting, flooding, or trash
- Families emphasize **safety as a barrier to kids walking/biking independently**

Goal Framing

- Improve lighting and security along trails and underpasses
- Maintain trails and sidewalks to prevent flooding, trash, or overgrowth



Appendix B: Community Engagement Round 2 Overview

November 2025

Round II Engagement Overview

Purpose: Why Engage at this Stage?

During Round I of community engagement, the Greater Heights community provided input on the mobility values, concerns, and opportunities facing the neighborhoods in the study area. Round II provided an opportunity to share key findings and highlights from the first round of engagement and pinpoint specific streets, trails, and intersections where improvements are most needed to meet the plan goals.

This stage of engagement provided an important opportunity to be transparent and communicative about the findings from Round I and allowed the community to provide greater detail on the locations most in need of attention. Round II helped to further refine understandings of community needs and led to the development of project recommendations.

Outreach: How Did We Reach People?

For Round II engagement, the planning team developed a mapping exercise that stayed open for 32 days (from October 16 to November 17, 2025). The team also participated in community “pop-ups” at existing events and activity locations, including “Sip & See: Trails & Trees,” hosted by A Tale of Two Bridges on the Nicholson Street Trail, “Shady Acres Spooktacular,” hosted by Shady Acres Parents Group at Wright-Bembry Park, and “Play Ball! Stude Park”, a project team pop-up at the Stude Park baseball fields. These in-person outreach events resulted in over 100 contributions to the interactive map activity.

In addition to these events, the team shared digital flyers and outreach language for emails and newsletters with leadership of area school

PTOs, LINK Houston, Bike Houston, the Houston Heights Association, Woodland Heights Civic Association, First Ward Civic Club, Norhill Neighborhood Association, and A Tale of Two Bridges non-profit to share with their respective members. Event and website promotion was also presented at the Super Neighborhood 15 community meeting and the Shady Acres Civic Club meeting. The outreach resulted in multiple social media posts by Super Neighborhood, civic association, and local organizations and schools.

The outreach effort resulted in 830+ map contributions total between the in-person map and the project website online map. The results of these activities are summarized in this document.

Outcomes: How Will Input Be Used?

The results of this community engagement will be used alongside the results of Round I engagement to develop project recommendations focused on specific streets, trails, and intersections throughout the study area. These mapped locations

will lead to the development of a project portfolio of mobility projects that MHRA can feasibly implement in the Greater Heights community.

Outreach Efforts by the Numbers

- 3 Community Pop-Up events with 100+ attendees
- 2 Super Neighborhood & Civic Club Meetings
- 5 School PTOs

Resulting in...

- 830+ map pins placed



Survey Results Summary



Greater Heights Strategic Connections Plan



What We Heard: Six Major Themes

1. Sidewalk and ADA access are the #1 concern, especially near schools and trail access points.

2. Crossing safety is the #2 concern, with a strong geographic pattern: Washington Ave, Yale, Main, 11th, 20th, Ella, and TC Jester.

3. Lighting deficits on trails and access routes are constraining evening and wintertime mobility.

4. There is strong public demand for completing trail connections between MKT Trail, White Oak Bayou Greenway, Memorial Park, and the Greater Heights street grid.

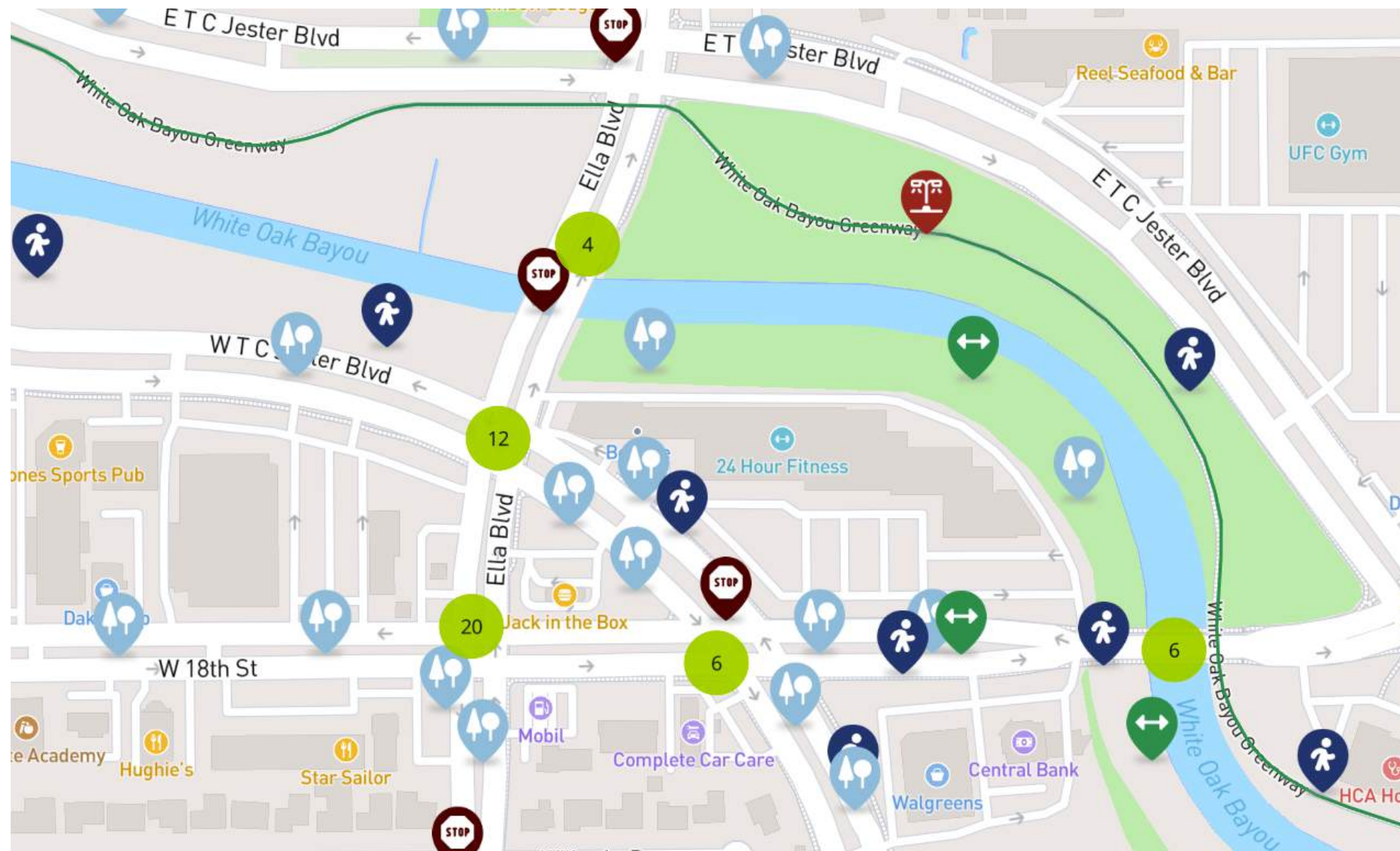
5. Traffic calming around schools should be a top priority, based on both comment frequency and severity.

6. Residents strongly value shade and heat mitigation, which should inform the comfort index of the *People-First Network*.

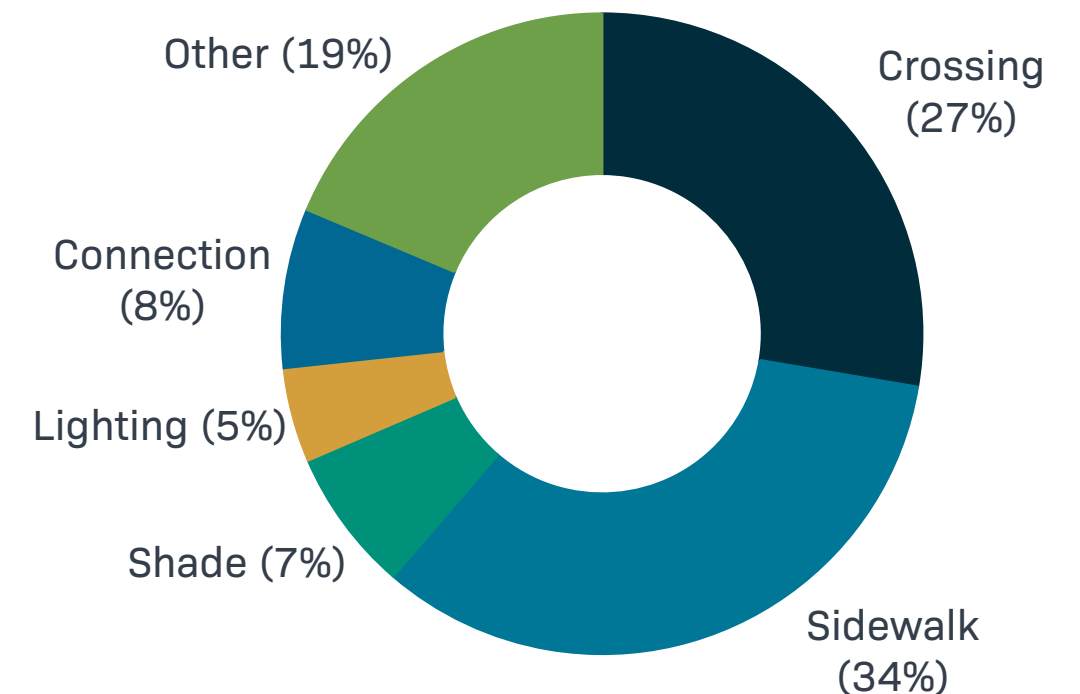


Map My Needs **734** Posts **119** Contributors

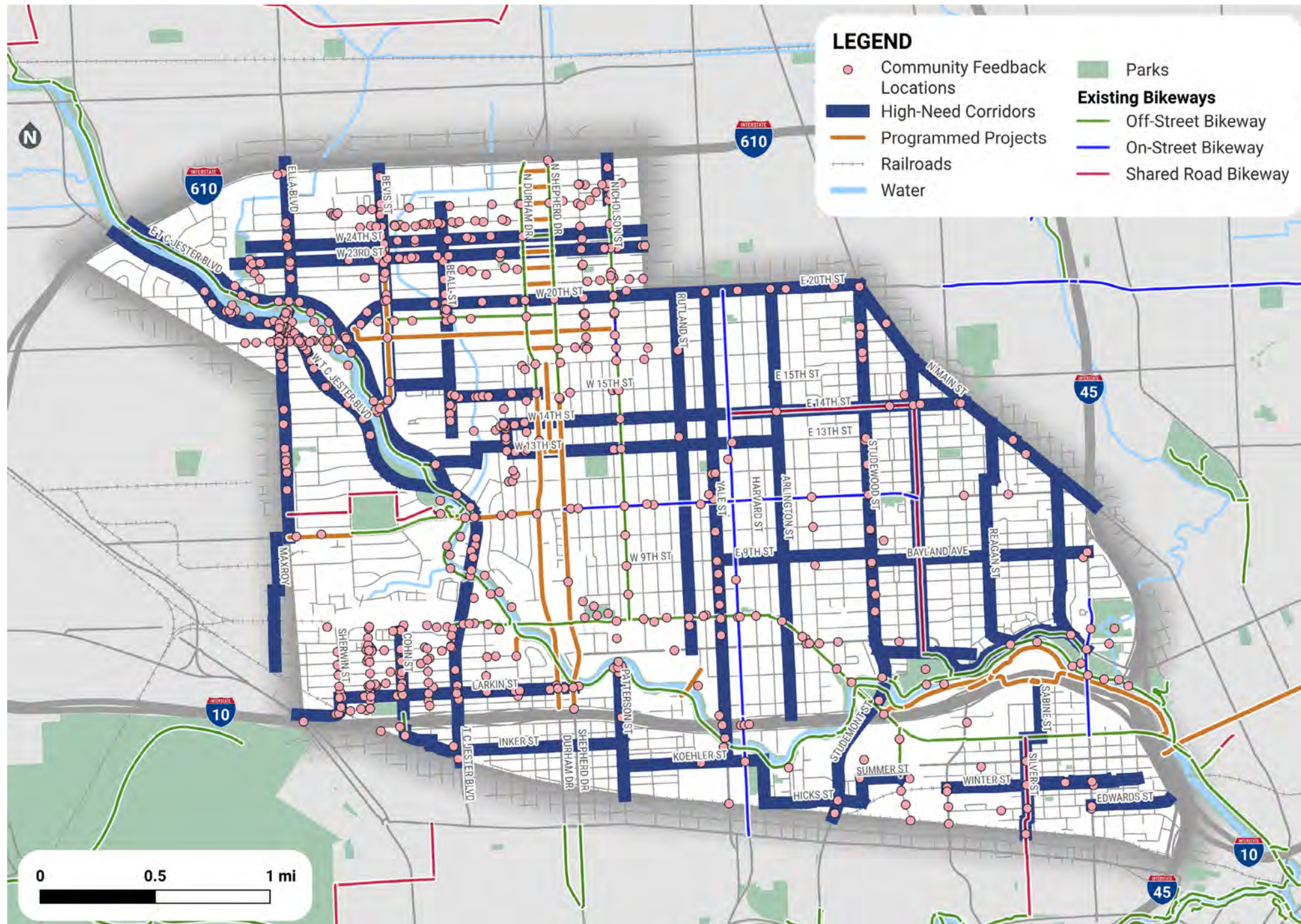
Project website activity: Identify streets, trails, and corridors where improvements would help us achieve the Vision and five Goals.



**Portion of Pin Types Used:
Social Pinpoint**



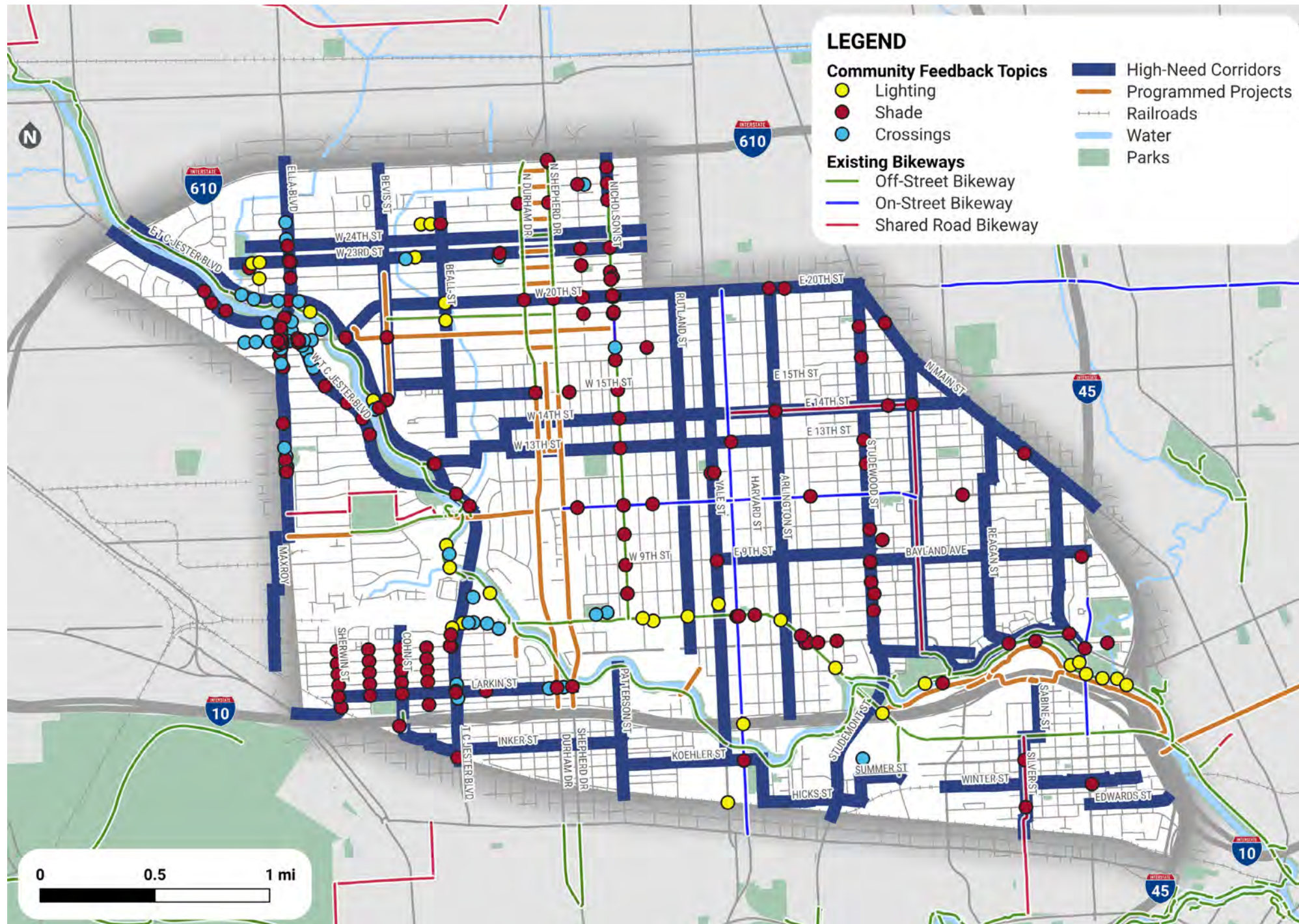
Map My Needs



This map shows all the community feedback points geocoded, combining feedback points from the various round II engagement events as well as feedback points from the Social Pinpoint interactive online “Map My Needs” activity. These points, along with the specific feedback associated with each, was then used to identify what types of improvements the community is in support of, and to provide additional scoring for the assessment of the People-First Network (PFN).

Map My Needs

Lighting, Shade, & Crossings



This map further highlights the specific locations where the community provided feedback on lighting, shade, and crossing needs. These points will be used to inform the purpose and need statements and project recommendations. In total, there were **728 comments** received, 673 of which were in the study area. Of the 673 comments in the study area, 45% were within 50 feet of a high-need corridor, and 5% were within 50 feet of a programmed project. Overall, **50.5% of comments were within 50 feet of a programmed project or a high-need corridor.**

Community Comments: Key Themes

1. Sidewalk Gaps, Repairs, ADA Access

32% of comments

There are systemic sidewalk and ADA access challenges across the Greater Heights, especially around schools (Helms and Sinclair Elementaries), along Lawrence, W 27th, Dian, Beall, Yale, and around the MKT Trail/White Oak Bayou trail interfaces.

Comment themes:

- Missing sidewalks
- Sidewalk gaps between newer builds
- Cracked, uneven, flooded, or obstructed sidewalks
- ADA ramps missing, misaligned, or in disrepair
- Sidewalks narrowed by vegetation, parking, or drainage issues
- “Goat trails” indicate where sidewalks should be
- Feeling of unsafe access to schools, parks, trails

Community Comments: Key Themes

2. Crossing Safety & Need for a Stop Control

28% of comments

Unsafe crossings, especially on wide or fast corridors (Main, Ella, Yale, Shepherd, Durham, Washington), represent a clear barrier to walking, biking, and accessing schools and parks.

*Residents repeatedly noted unsafe crossings due to speeding vehicles, long distances between protected crossing points, and lack of signals. **Common requests included:***

- More stop signs / 4-way stops
- HAWK signals near schools
- Mid-block crossings on Washinton, 20th, 11th, and Yale
- Raised crosswalks
- Better markings or pedestrian refuges
- Aligning crosswalks with actual pedestrian desire lines
- Safer crossings to reach parks and trails (White Oak Bayou, MKT Trail, Stude Park)



Community Comments: Key Themes

3. Bike Network

14% of comments

People want a *connected, comfortable, and protected* bike network that integrates trails with neighborhood streets.

Comment themes:

- Requests to extend existing trails (MKT to Memorial Park, White Oak Bayou, Ella, 12th, 11th)
- Add protected bike lanes (Houston Ave, Heights Blvd, N Main)
- Restore or improve bike lanes removed by resurfacing (E 20th)
- Address misaligned or unsafe trail/street crossings)
- Repurpose on-stree parking to protected bike lanes



Community Comments: Key Themes

4. *Traffic Speeding, Calming, & Driver Behavior*

10% of comments

Streets engineered for speed - or lacking traffic control - consistently endanger people walking and biking.

Comment themes:

- Speed bumps
- Road diets
- No-turn-on-red signage
- Traffic calming around schools/daycares
- Speeding along Shepherd/Durham, 20th, 11th, Ella, TC Jester
- Requests for enforcement

Community Comments: Key Themes

5. Lighting & Personal Security

8% of comments

Lighting deficits affect perceived and actual safety on trails and access routes and are a notable barrier to evening activity.

Comment themes:

- Trails that are too dark (MKT Trail, White Oak Bayou Greenway, Heights Trail)
- Underpasses with no lighting
- Lighting gaps around parks, trailheads, and at W 23rd, Ella, Yale, and TC Jester
- Safety concerns linked to recent assaults on trails
- Desire for emergency call boxes similar to Buffalo Bayou Park

Community Comments: Key Themes

6. Drainage, Flooding, & Surface Conditions

3% of comments

Comment themes:

- Flooded ramps
- Puddling sidewalks
- Streets with potholes

7. Shade, Trees, Heat Mitigation

3% of comments

Comment themes:

- Shade structures at parks and trails
- Street trees along walking routes
- Mitigating “heat island” sidewalk segments

Community Comments: Key Themes

8. Transit Access Improvements

2% of comments

Comment themes:

- Connect to bus stops
- Add shade/shelters at stops
- Extend METRO Rapid service to Heights

9. Parking/Visibility Conflicts

2% of comments

Comment themes:

- Cars blocking sidewalks
- Visibility issues due to parked vehicles
- No-parking zones needed on narrow streets



Appendix C: Community Engagement Round 3 Overview

March 2026

Round III Engagement Overview

Purpose: Why Engage at this Stage?

After developing project recommendations based on analysis and community input from Rounds I and II, Round III provided an opportunity to share recommended projects across Greater Heights. Community members provided feedback on projects that would make the greatest impact

on an individual and community level while also assessing project effectiveness in terms of safety.

This stage of engagement allowed the community to see the results of the greater planning effort and validate the project outcomes.

Outreach: How Did We Reach People?

For Round III engagement, the planning team posted a map and table of project recommendations on the project website. Visitors were asked to rank the overall safety effectiveness of the recommended projects and identify the project(s) that would make the greatest impact to them. The team translated the online activities into in-person engagement during community “pop-ups” at existing events and activity locations, including “The Heights Mercantile Farmer’s Market,” on the MKT Trail, a community meeting hosted by Super Neighborhood 22, “Soundscapes on the Trail” hosted by A Tale of Two Bridges on MKT Trail, and Council Member Castillo’s Town Hall in the First Ward.

In addition to these events, the team shared digital flyers and outreach language for emails and newsletters with leadership of area school PTOs, LINK Houston, Bike Houston, the Houston Heights Association, Woodland Heights Civic Association, First Ward Civic Club, Norhill Neighborhood Association, and A Tale of Two Bridges non-profit to share with their respective members. Event and website promotion was also presented at each in-person event.

The outreach effort resulted in 425 contributions total between the in-person and the website feedback opportunities. The results of these activities are summarized in this document.

Outcomes: How Will Input Be Used?

The results of this community engagement will be used to help refine and finalize project recommendations throughout the study area. Feedback collected on safety and project impact will help validate the success of

the planning process and help the project team understand which projects are the most important to community members.

Outreach Efforts by the Numbers

- 4 Community Pop-Up events with 75+ attendees
- 2 Super Neighborhood & Council District Meetings
- 353 Website activity contributions

Resulting in...

- 425 points of feedback



Feedback Summary



Greater Heights Strategic Connections Plan



What We Heard: Six Major Themes

1. Intersections and Schools Represent Urgent Safety Risks

Certain intersections, most notably Silver & Crockett, stand out as critical safety concerns, with repeated reports of speeding, crashes, and unsafe crossings near schools. Respondents describe these as immediate risks that require near-term intervention. Safety improvements, including traffic management near schools was also mentioned frequently and noted as a high priority.

2. The Network has Critical Gaps that Limit Connectivity

Residents consistently identified missing links between neighborhoods, trails, parks, and destinations. Barriers such as bayous, highways, unsafe intersections, and rail lines create gaps where safe crossings do not exist. Bridge projects, notably the Patterson Street bridge project, received significant support from respondents. Additionally, key park and greenspace connectivity projects, including the Memorial Park connection, the Heights to Buffalo Bayou corridor project, and the MKT Bird Sanctuary Loop were noted as priority projects for the community in a high number of responses.

3. Strong Preference for People-Focused Investments

There is broad support for improving access and connectivity with projects prioritizing sidewalks, trails, safe crossings, and protected bikeways. Respondents emphasized that facilities should feel safe and comfortable for people of all ages and abilities and prioritize projects where modes can be safely separated or facilities built with adequate space to accommodate multiple modes (trails, shared use paths, etc.).

4. Neighborhood Streets Need Traffic Management to Function as Part of the Network

Cut-through traffic and speeding are major concerns on neighborhood streets. Residents specifically called out corridors like Crockett, Silver, Colorado, and 15½ Street and expressed strong support for tools such as speed cushions, stop control, and traffic management.

5. Delivering Early, Visible Improvements is Critical to Maintaining Trust

Residents expressed frustration with stalled or incomplete projects and emphasized the importance of delivering tangible improvements quickly, addressing gaps, improving crossings, and moving forward with “shovel ready” projects as soon as possible.

6. Strong Community Support Exists. *Note: Expectations Are Increasing*

Overall sentiment is highly supportive of improving walkability and bikeability. However, there seems to also be a sense of growing urgency and expectation for follow-through, particularly given ongoing construction impacts and past delays.

What We Heard: Safety

(Fifty-two percent of you ranked safety as your top transportation value on the “Community Vision” survey. Would you feel safer walking, biking, and rolling in the Greater Heights area if these projects were implemented?)

Community Pop-Up Events:

16% of respondents “Agree”

84% of respondents “Strongly Agree”

Website Survey:

99% of respondents “Strongly Agree”

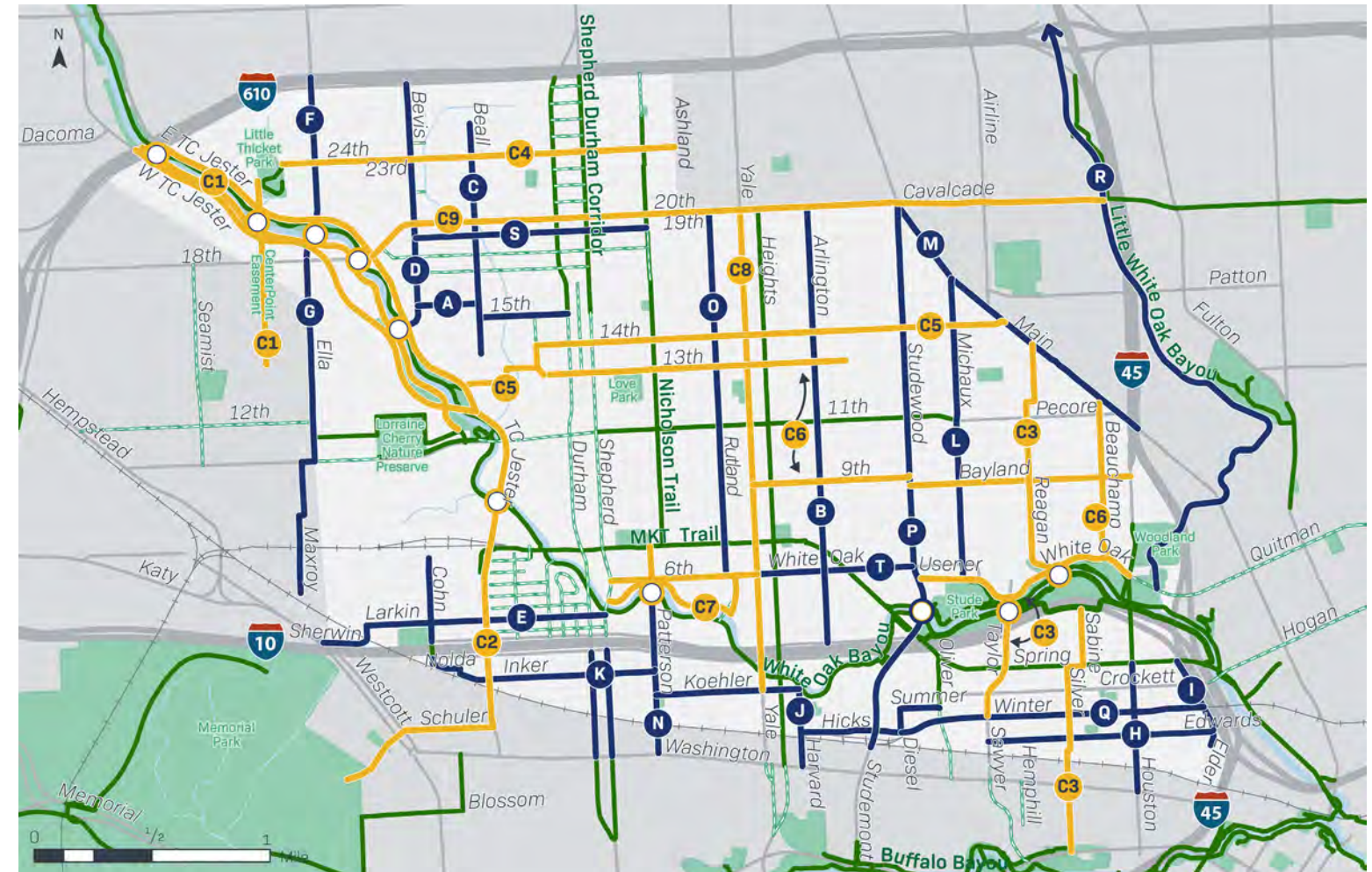


What We Heard: Project Impact

(Which project would have the biggest impact on improving your access to neighborhood destinations?)

Top Community Priorities:

- #1 C7** (36% of responses)
- #2 C2** (17% of responses)
- #3 C3** (16% of responses)
- #4 C1** (7% of responses)
- #5 C4** (4% of responses)



What We Heard: Respondent Quotes

Safety

“C3 would be a huge safety improvement.

Crossing White Oak with my family has always taken a bit of timing. You have to RUN across between waves of traffic.”

“Address major roads and thoroughfares first

(Yale, Studewood, Main, TC Jester, Ella). Those will have the most impact. Too many drivers use those roads as cut throughs from highways, and attention to safety is needed.”

Connectivity

“With connections provided by the MKT Heights Bird Sanctuary Loop **I would be able to reach destinations**

such as the MKT Trail, Buffalo Bayou Trail, the MKT Shopping Center, Lawrence Park, HEB, St. Thomas High School, and 11th Street much safer and easier.”

“One of my strongest desires is to **be able to bike to Hermann Park and the med center.**

Implementing these projects would not get me all the way there, but it would be a huge step in the right direction!”

Quality of Life

“Being able to cross the bayou by foot or bike **transforms how people live in Houston.**”

“My kids love exploring new paths and bridges, **it changes how they experience the neighborhood.**”

Trust

“Show how these projects improve the neighborhood for everyone, **most of us rely on multiple ways of getting around.**”

“The I-10 and 45 construction has really shown **how fragile our community network is** - spent almost a year with the First Ward on an island due to bridge access to White Oak Bayou.”

Appendix D: Needs Analysis Memorandum



MEMORANDUM

December 11, 2025

To: Clint McManus

Organization: TEI Planning + Design

From: Susannah Barton, Ben Magallon

Project: 12093 MHRA TIRZ 5 – Greater Heights Strategic Connection Plan

Re: Task 3.1: Needs Analysis and Task 3.2 Corridor Inventory

This analysis identifies the streets and intersections in the Greater Heights Study Area where walking, bicycling, and rolling are most challenging today and where improvements can have the greatest impact and improve safety and connectivity for everyday trips like reaching school, transit, parks, shops, and trails. By combining data, community feedback, and findings from fieldwork, the project team developed a *People-First Network* – a network of existing active streets and trails, programmed transportation improvement projects, and streets identified through the needs analysis and corridor inventories as high-need for infrastructure improvements to strengthen connectivity and improve safety. This *People-First Network* provides the foundation for the development of a series of project recommendations focused on closing sidewalk gaps, improving crossings and connections, and creating a network of safer, more comfortable walking and bicycling routes across the Greater Heights Study Area. This memo outlines the methodology used to identify the high-need corridors and intersections where these improvements are most needed.

Overview

Building on mobility goals informed by community input, GIS data was identified to map corridor and intersection characteristics that correspond to each project goal. These characteristics were then layered in GIS, and the resulting maps identify locations where multiple characteristics overlap, ultimately informing the basis for the roadways needed to complete a full *People-First Network (PFN)* that fulfills the mission of the Greater Heights Strategic Connections Plan and meets the goals of the study.

Mission

The Greater Heights Strategic Connections Plan will provide a blueprint for thoughtful design and intentional investment in a set of corridor and intersection projects that realize the Vision of safer, healthier, and more connected streets.

Vision Statement

The Greater Heights community is an active community built on an historic street grid where safe, easy mobility choices cultivate a healthy, connected, and dynamic place for people to grow up and grow old.

Goals

- A. **Safe Crossings:** Improve crossing conditions on key corridors by **reducing the average distance between safe crossings** to bring major roadways into compliance with city standards.
- B. **School Streets:** Create an environment in which students feel safe walking or riding a bicycle to school by **implementing safe street infrastructure initially within a 1/4-mile radius of all schools.**
- C. **Connected Network:** **Increase the share of residents who have excellent or direct access (1/8 mile) to active streets and trails** from 20 percent to 100 percent by delivering targeted street and intersection projects that connect to everyday destinations and expand the existing bikeway and trail network.
- D. **Complete Sidewalks:** **Close all gaps in the sidewalk network** and repair existing sidewalks that provide important connections to local destinations.
- E. **Great Streets:** **Design streets and trails as destinations**, where abundant shade, lighting, and other amenities encourage people to spend time together outside.

Methodology for Mapping the *People-First Network*

The development of the Needs Analysis framework was coordinated with TEI staff. Prior to completing the scoring and overlay analysis, Toole Design reviewed the proposed framework criteria and methodology with TEI to confirm that the approach aligned with the overall project goals and data availability. This collaborative review ensured that the criteria used to evaluate corridors were mutually agreed upon and appropriate for identifying priority segments. Building off the existing conditions analysis for this plan, this needs analysis follows a multi-step process to identify a grid of streets that are best suited to be considered for the *People-First Network*. This methodology in essence uses data, public involvement inputs, and fieldwork to ask the following questions:

1. **What do people need?** Asking this question through a data driven process involved mapping GIS Data that corresponds to the stated goals and mapping other notable destinations and demographic context data.
2. **Where are the biggest barriers?** This question involved overlaying all the mapped features from the first question and processing them into a single prioritization network to identify the areas of highest need for a *People-First Network*.
3. **Which streets rise to the top?** This step involved visualizing the cumulative scoring of the above two questions to highlight which roadways had the highest need when considering all the combined factors.
4. **What did we confirm in the field?** This involved the team performing fieldwork on the proposed *People-First Network*.
5. **How do we complete a comprehensive *People-First Network*?** This final question involved comparing the community feedback to the needs analysis. In addition, through a series of working meetings the project team finalized the conditions assessments of the *People-First Network* for prioritization and project development and identified any additional roadways or connections needed to fully realize the PFN.

The accessibility and proximity analyses used in this Needs Analysis build upon the **walk and bike shed models developed in Task 1.2**. The 1/8-mile and 1/4-mile buffers used in Map 4 are derived from that base dataset, ensuring consistency between the foundational accessibility analysis and the identification of corridors that improve direct connections to existing trails and bikeways.

For clarity and consistency with the project scope, the *People-First Network* described throughout this memo serves as the project's Critical Mobility Network. Both terms refer to the same network of priority corridors and intersections where improvements will provide the greatest safety and connectivity benefits for people using all modes, including walking, riding a bicycle, and rolling.

The datasets used in this analysis represent the most current information available as of 2024–2025, including pedestrian and bicyclist crash records from TxDOT CRIS and publicly available sidewalk and bikeway inventories. While data accuracy is sufficient for planning-level analysis, some fields (e.g., sidewalk condition and lighting coverage) were validated through field verification as part of Task 3.2. All assumptions and data sources are documented to ensure transparency and reproducibility.

The following sections outline the above-described steps, including conclusions reached through the initial needs analysis of potential network recommendations.

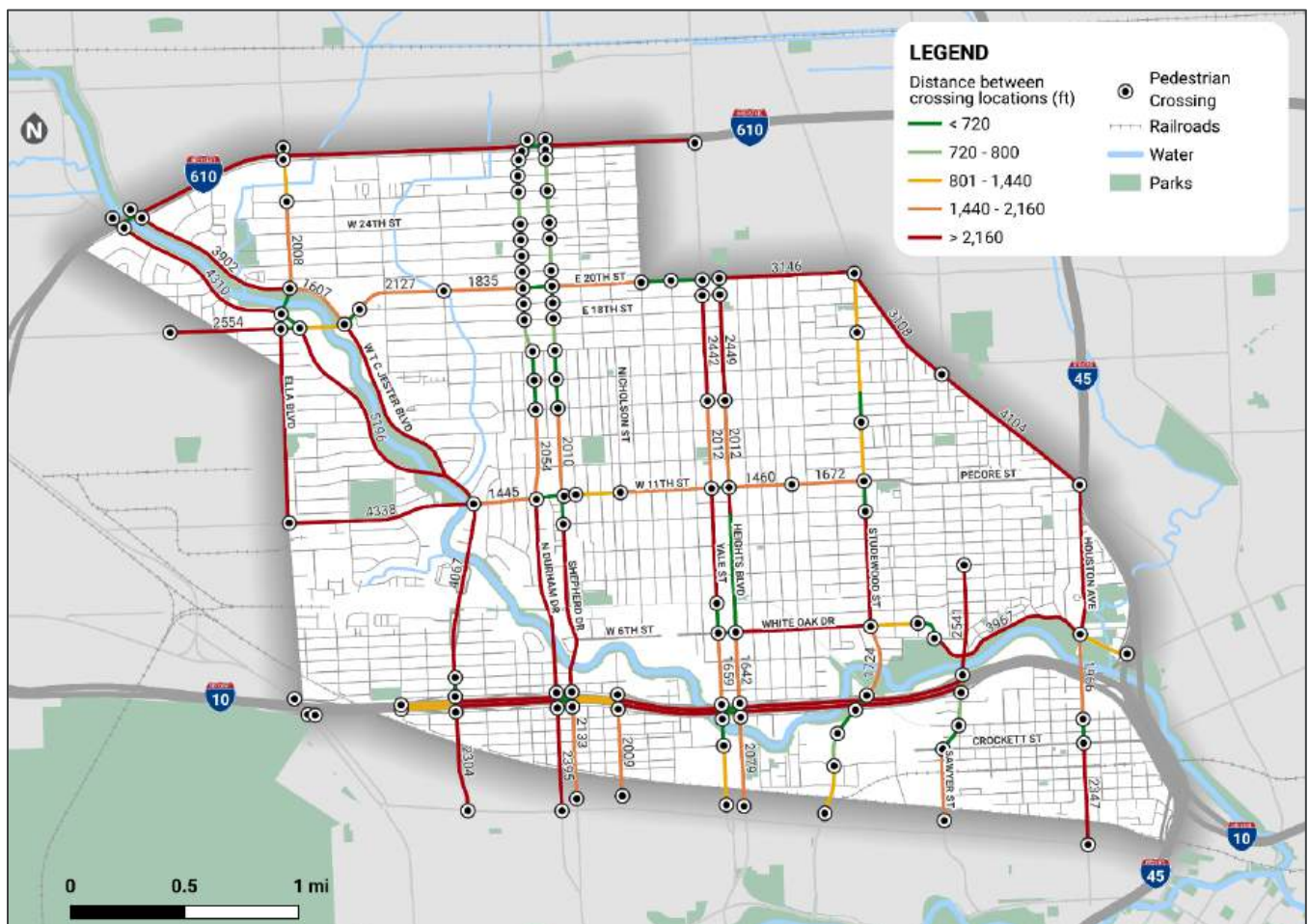
Step 1: What do People Need? Mapping GIS Data Corresponding to Project Goals

The Needs Analysis is based on the goals of the plan. Goals A-D correspond to the following needs:

A. Need for shorter distances between marked crossing locations

Map 1 shows distances between crossings of major roadways. Red and light red segments are focus areas and potential links in the *People-First Network* because they have long distances between crossings.

Map 1: Distance Between Marked Crossings

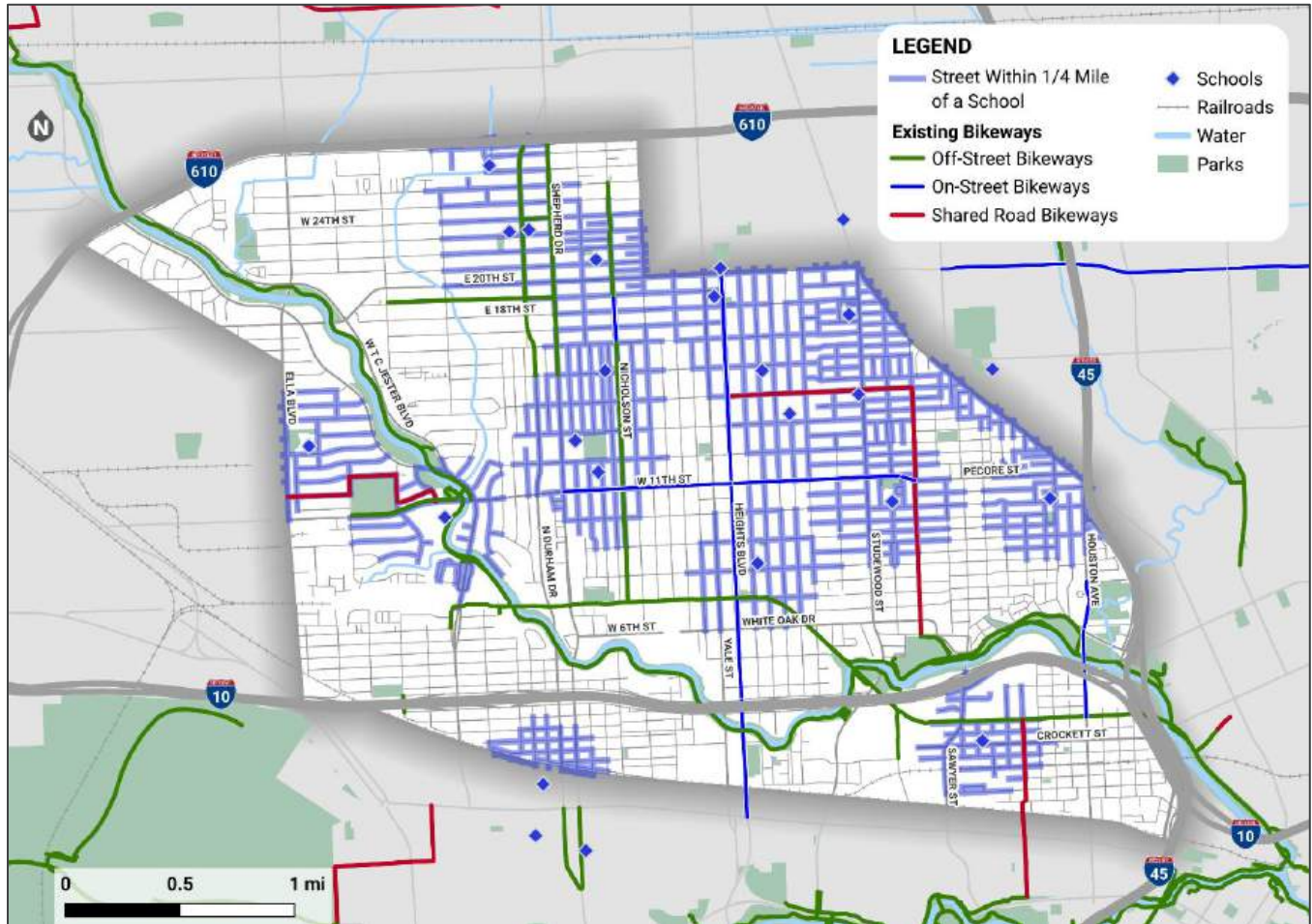


Long distances between crossings make it difficult and often unsafe for people to walk or roll across major streets. Identifying locations where projects can improve crossing safety or reduce the distance someone has to go to cross a street safely will improve mobility across the Greater Heights Study Area and make strides towards achieving this corresponding goal.

B. Need for safe access to schools

The project team created a quarter-mile buffer around every school, including private schools and pre-k, and highlighted network segments that fall within this buffer. Blue streets in Map 2 represent focus areas for the *People-First Network*.

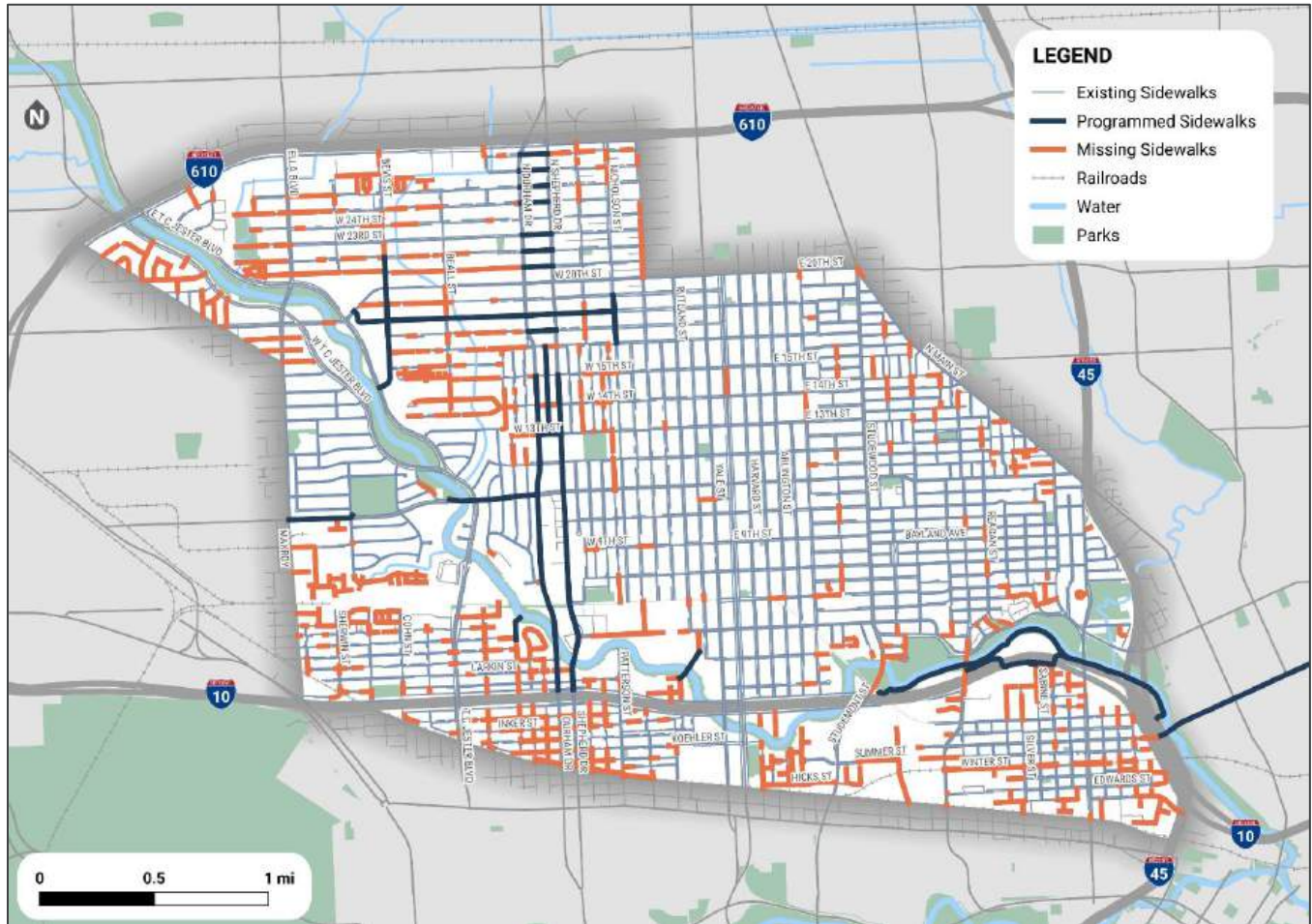
Map 2: Access to Schools



C. Need for more sidewalks

Map 3 displays sidewalk gaps in the Study Area based on Houston-Galveston Area Council's 2024 regional sidewalk inventory. Highlighted segments are focus areas and potential links in the *People-First Network* because they lack complete sidewalk connectivity.

Map 3: Sidewalk Gaps



Sidewalk gaps especially affect access to parks, schools, and transit. These were among the most common concerns the project team heard from residents during outreach. Some of the sidewalk gaps shown in the data will be addressed by programmed projects. As part of the Needs Analysis, the project team also considered where sidewalk gaps will be addressed through programmed projects in subsequent steps outlined in the memo.

D. Need to improve access to active streets

Map 4 displays areas with excellent access (1/8 mile or less) to active streets. Active streets here are defined as streets and trail corridors where walking, riding a bicycle, and rolling are generally safer and more comfortable. This includes designated walking and bicycle facilities such as White Oak Bayou Trail and the Shepherd Durham Corridor, as well as streets like Michaux Street that are designated as a Shared On-Street bikeway by the City of Houston.

White areas on the map, outside of the yellow segments are focus areas. Segments in these areas are potential links in the *People-First Network* because they are beyond 1/8 mile from active streets.

Map 4: Access to Active Streets



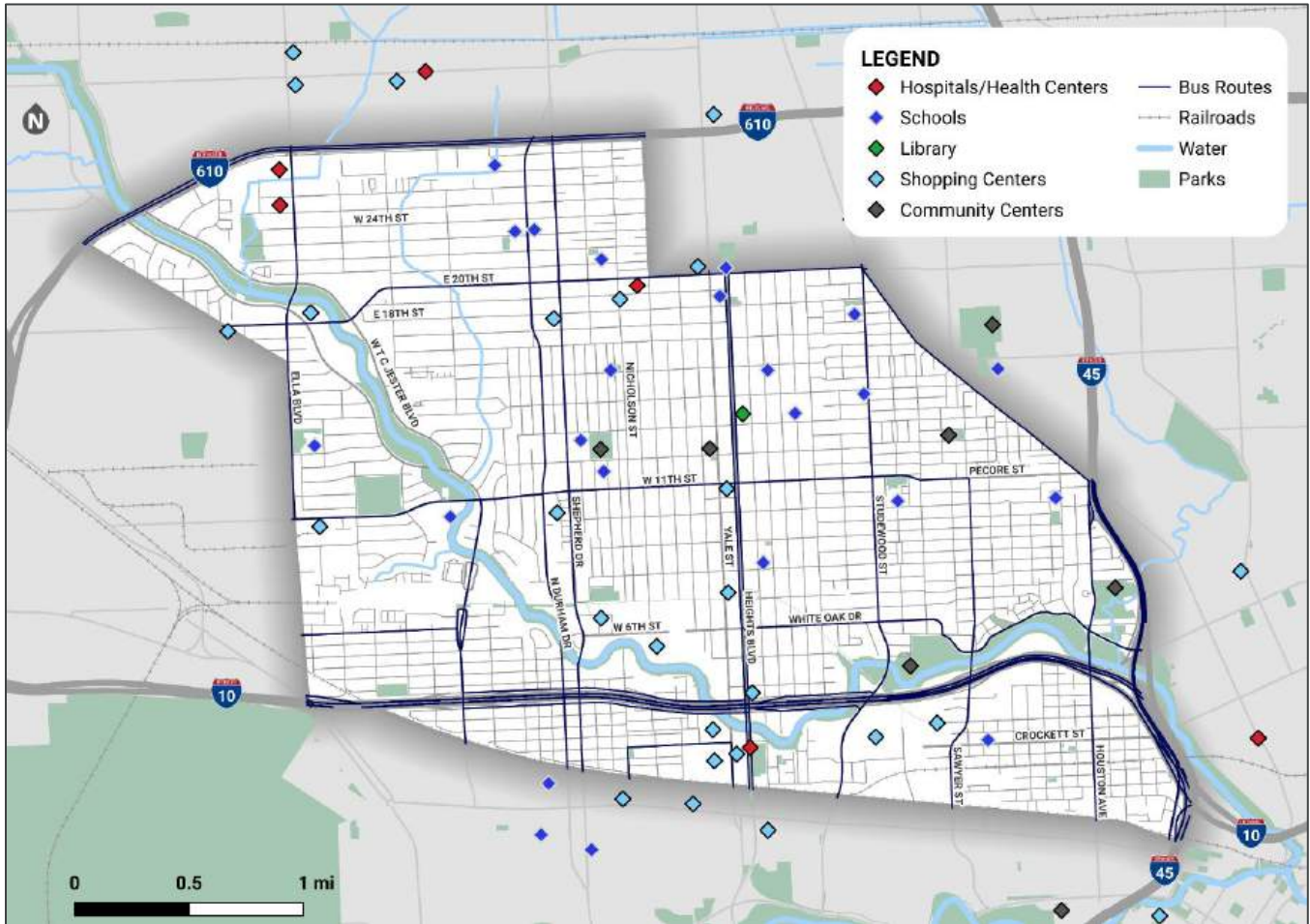
Together, Maps 1, 2, 3, and 4 identify where crossing gaps, sidewalk gaps, and distance from active streets overlap the physical barriers that shape everyday movement and community connectivity in the Study Area.

Mapping Other Notable Destinations and Context/Demographic Information

Destinations for Daily Life Activities

Map 5 displays the locations of various destinations in the study area. The *People-First Network* prioritizes connecting to as many of these destinations as possible with safe walking and bicycle routes.

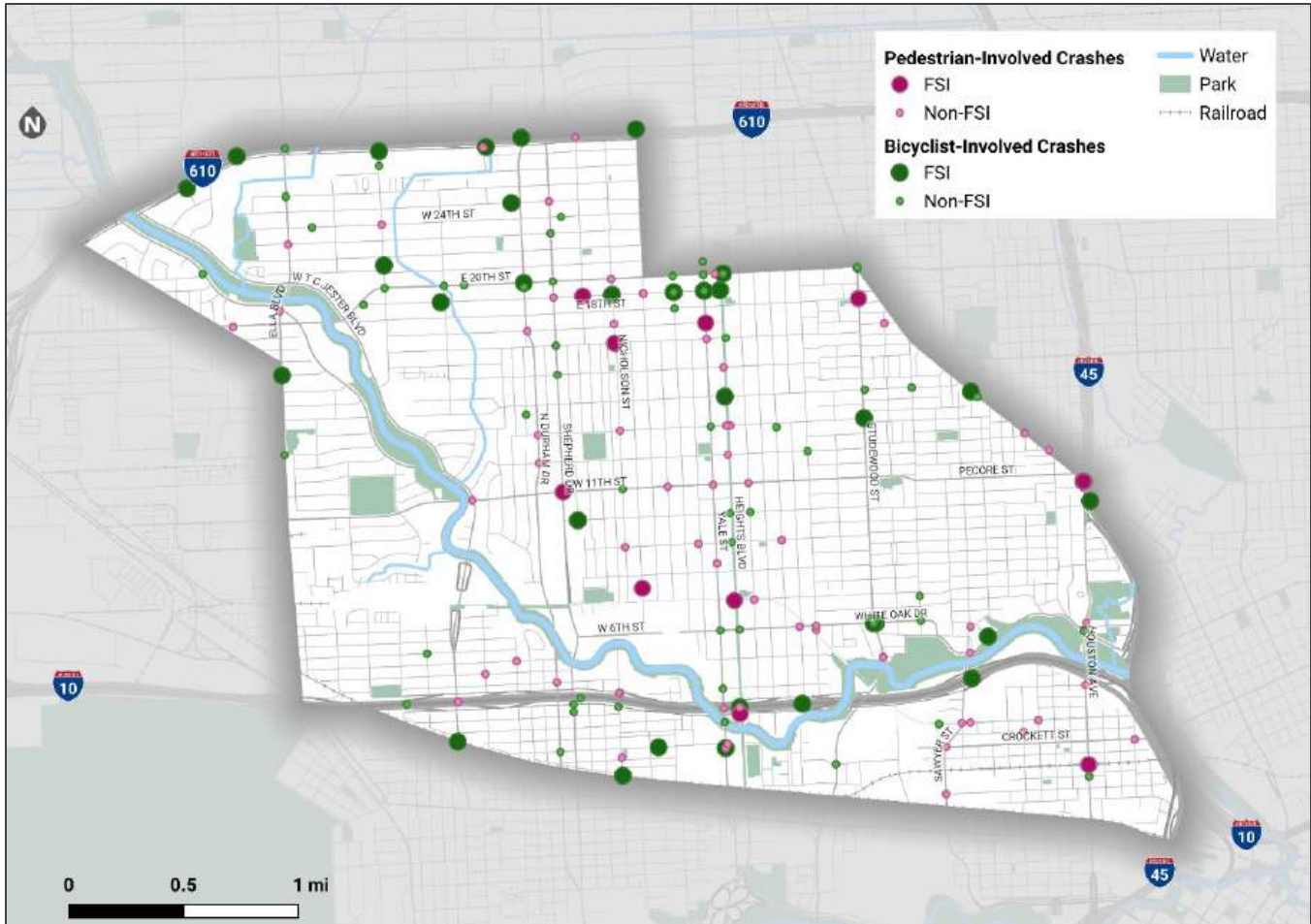
Map 5: Destinations to Daily Life Activities



Crashes Involving Vulnerable Road Users

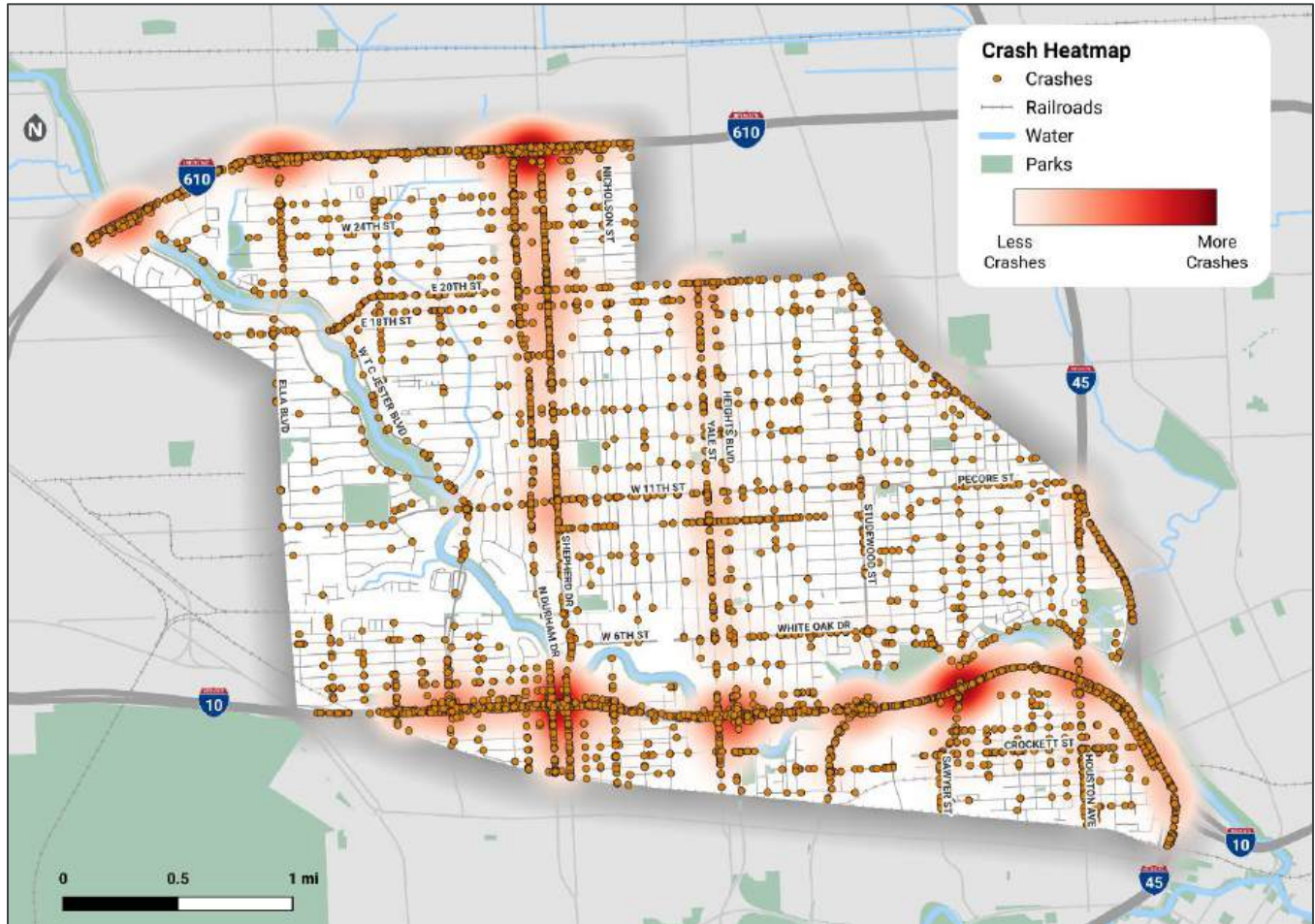
To assess the traffic safety needs of vulnerable road users (VRU) and all road users, the project team queried TxDOT's Crash Records Information System (CRIS). Map 6 displays crashes involving pedestrians and bicyclists that occurred in the Study Area from 2020 through October 21st, 2025. The *People-First Network* added scoring and prioritization for segments where these VRU crashes occurred. Higher crash volume corridors reflect higher scores for opportunities to improve safety conditions. In the context of this analysis, Fatal and Severe Injury (FSI) Crashes are identified separately from Non-FSI crashes to assess if there are environmental conditions leading to more severe crash outcomes in any given location.

Map 6: Crashes Involving Vulnerable Road Users (2020 through Oct 21, 2025)



Map 8 shows the crash density for all crashes between 2020 and October 2025 for all road users in the study area. This review of crash data, in addition to the review of VRU crash data, provides insight into where the highest risk areas are for all road users and can be used as an additional screening and prioritization tool when defining projects' purpose and need in following tasks.

Map 7: All Crashes 2020-2025 Heat Map



Step 2: Where are Biggest Barriers?

Where is the *People-First Network* Most Needed?

To identify which streets should be included in the *People-First Network*, the project team added each of the need indicators from the earlier steps to the underlying street map. Each block of every street was evaluated to determine whether it had a sidewalk gap, a missing or difficult crossing, safety concern, or another mobility need. For every condition that applied to a block, that block received one point.

After assigning points for all relevant conditions, each block received a total score representing how many different needs occur in the same place. No weighting was applied; all indicators were treated equally. Blocks with higher scores have more overlapping needs and therefore a higher priority for improvement. Because there were nine total indicators, scores ranged from zero to nine.

These scores form the basis of the draft *People-First Network*. Step 3 consolidates many different types of information into a single, clear picture of where mobility investments will have the greatest benefit. The analysis from Step 1 illustrated gaps in sidewalks, crossings, and safety. The analysis from Step 2 identified the destinations people travel to most often and the communities that depend most on walking, bicycling, and transit. Combining these elements helps reveal the blocks where multiple needs occur in the same location. These are places that both lack comfortable walking and bicycling conditions and provide access to important destinations such as those shown in Map 5.

This cumulative approach ensures that the *People-First Network* reflects the intersection of infrastructure gaps, safety challenges, and everyday access needs.

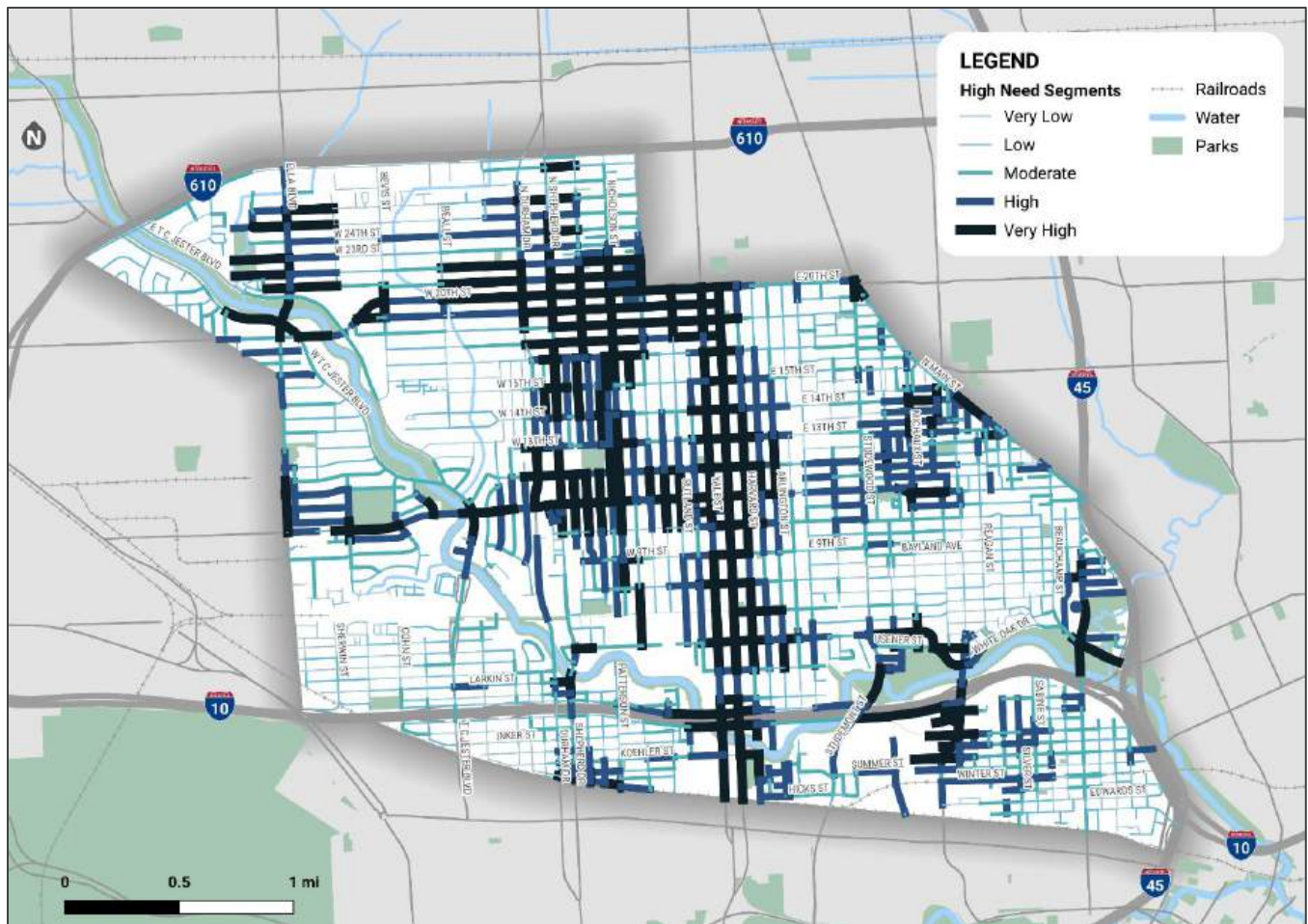
Table 1: Attributes Applied to the Network

	<i>Segment receives 1 point towards the level of need if the following attributes are TRUE</i>
<i>Safe Crossings Goal</i>	Any point on a block had a distance to nearest marked crossing greater than 2,160 feet
<i>School Streets Goal</i>	School within 1/4 mile
<i>Connected Network Goal</i>	More than 1/8 mile to Active Streets
<i>Complete Sidewalks Goal</i>	Presence of gap in the sidewalk network
<i>Daily Life Activities</i>	Shopping Center within 1/4 mile
	Hospital within 1/4 mile
	Health Care Center within 1/4 mile
	Community Center within 1/4 mile
	Library within 1/4 mile
	Parks within 1/4 mile
<i>Context / Demographics</i>	VRU crashes within 50 feet
	Bus stop within 100 feet

Table 2: Breakdown of Need Level

Number of True Attributes (i.e. sum of points)	Level of Need
0-1	Very Low
2-3	Low
4-5	Moderate
6-7	High
8-9	Very High

Map 8: Initial Development of High-Need Corridor Segments



Map 8 presents the first iteration of high-need corridors. Dark clusters of network links represent the corridor segments with the highest number of attributes. These darker segments indicate focus areas for potential safety and connectivity improvement projects. To summarize, these areas have some combination of the following attributes:

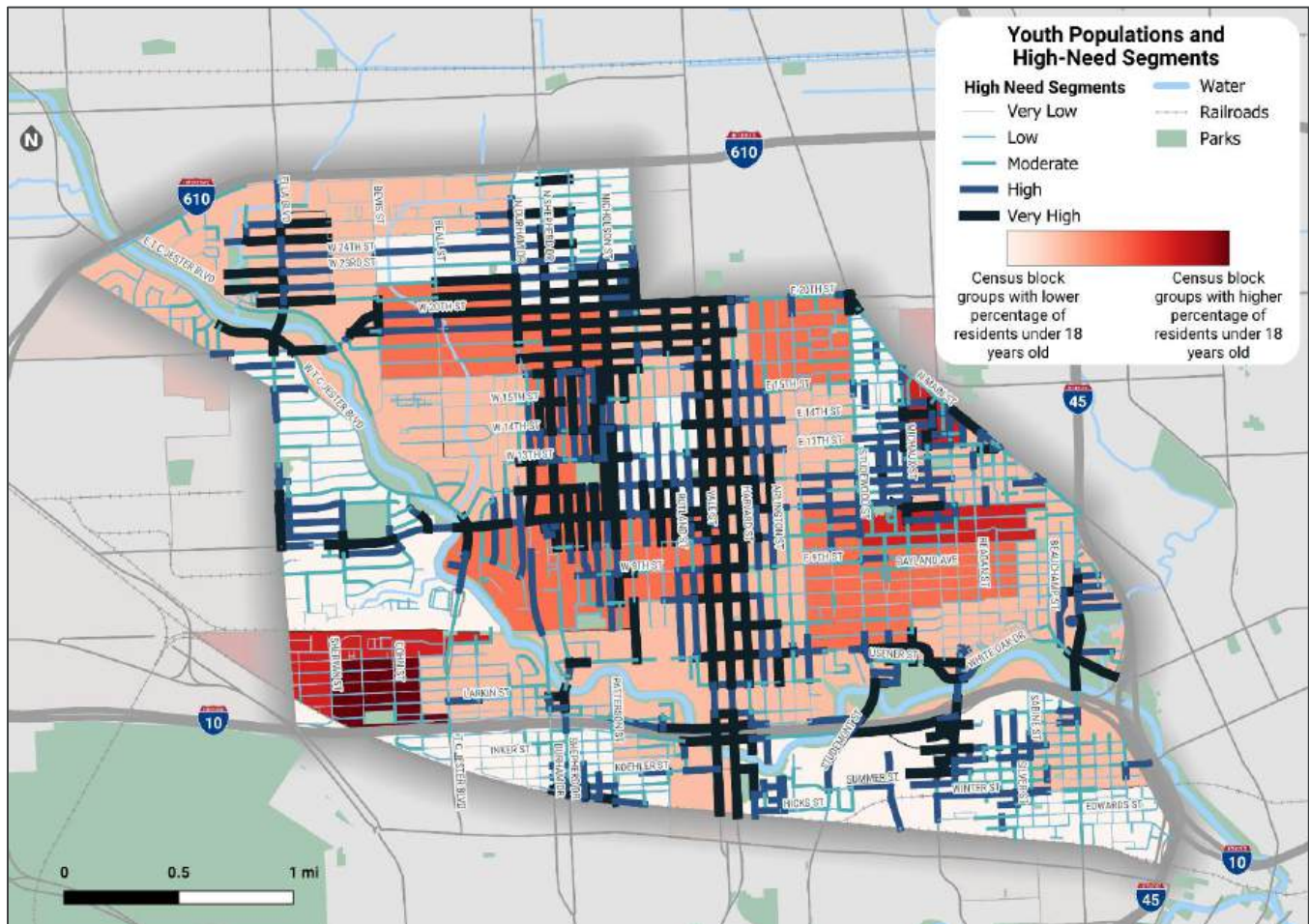
- Long distances between crossings
- Gaps in the sidewalk network
- Proximity schools

- Proximity to parks
- Proximity to other destinations
- Lack of proximity to Active Streets
- Proximity to a bus stop
- Presence of a VRU crash

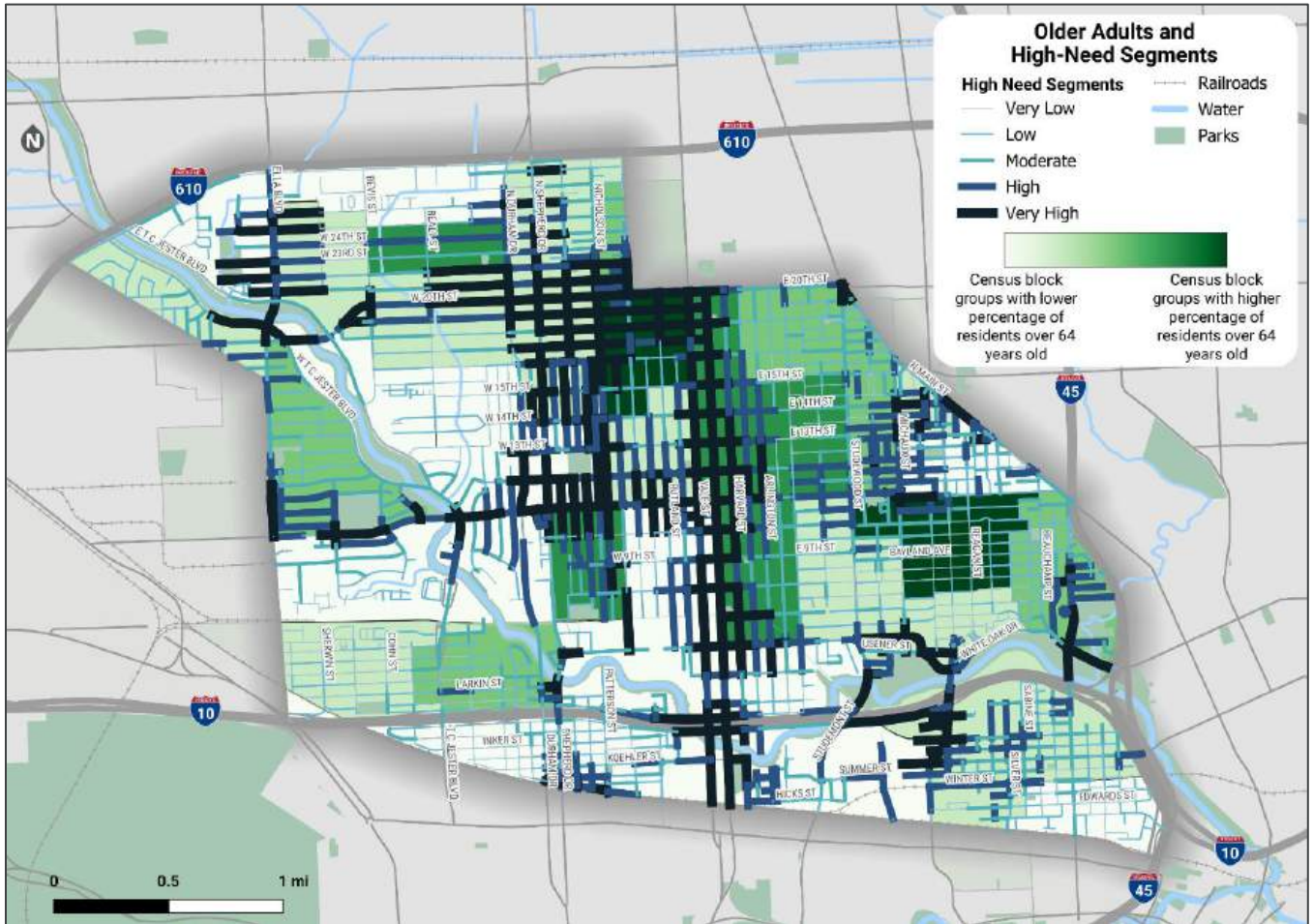
Vulnerable Populations

Map 9 and Map 10 display the population density of youth and older adults, respectively. The high-need corridors are overlaid with the areas where youth and older adult populations are located. This helps to provide additional context for where there are higher concentrations of more vulnerable potential users of active transportation infrastructure. Although this data was not used to inform the high-need corridors, it can be used as a prioritization for selecting projects by assessing where different demographic age groups are most densely concentrated in relation to the larger PFN.

Map 9: Youth Population Density and High-Need Corridor Segments



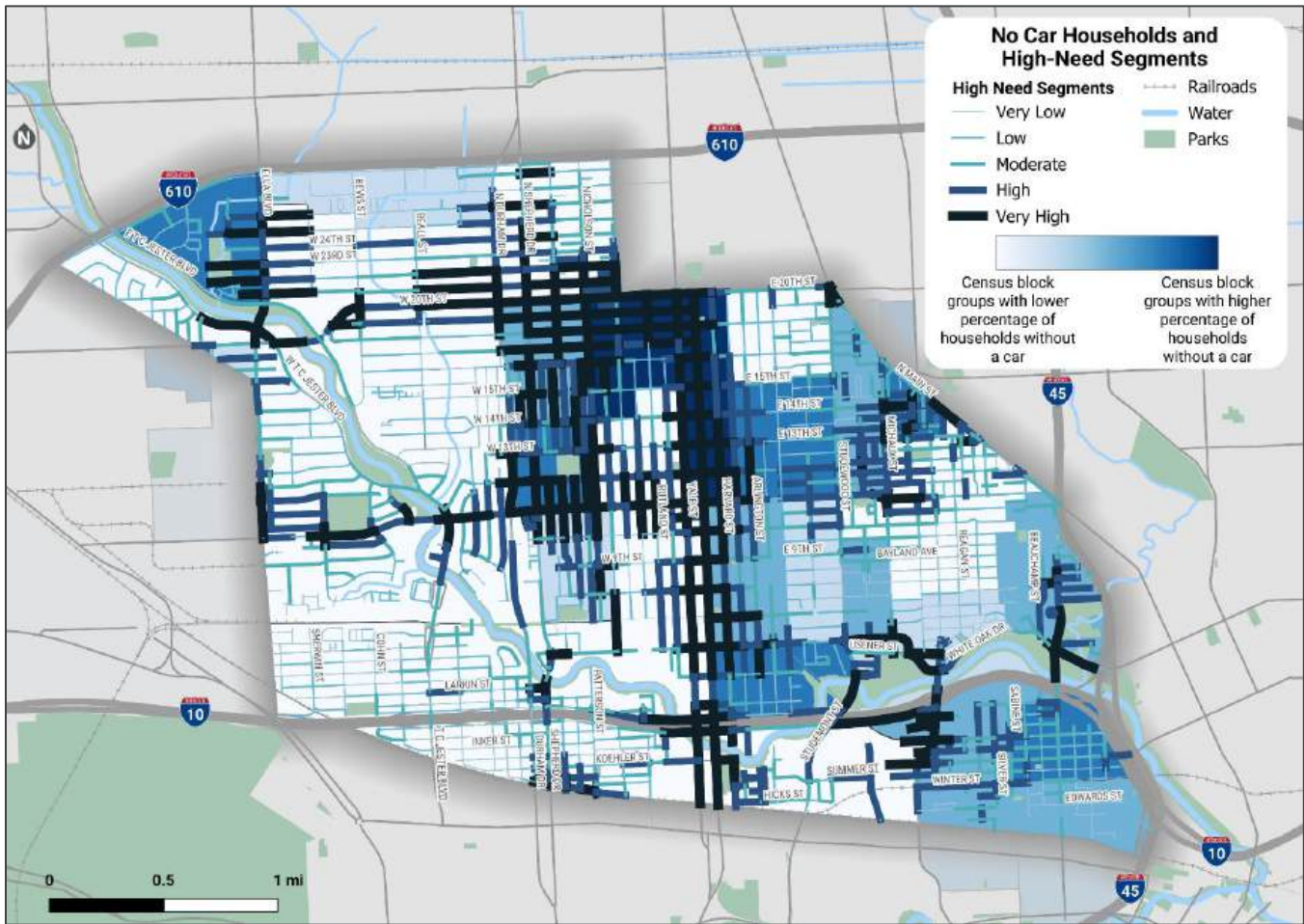
Map 10: Older Adult Population Density and High-Need Corridor Segments



Households without Access to a Vehicle

Map 11 displays the percentage of households within the Study Area without access to a vehicle. People living in households without access to a vehicle often rely on active modes of transportation and therefore safe access to transit and active mobility infrastructure is important for their daily needs. Similar to the previous two age demographic layers, this data can be used to review priorities as a screen when reviewing the PFN and refining project recommendations.

Map 11: Percent of Households Without Access to a Vehicle and High-Need Corridors



Step 3: Which Streets Rise to the Top?

To develop a cohesive set of corridors for assessment, the cumulative scoring from the previous two steps was reviewed at the segment and corridor level. Corridors were ranked based on the frequency of high-need segments (blocks), the frequency of each corridor’s ranking across all need areas, and consideration for where a continuous selection of segments completed a contiguous connection for users potentially accessing the White Oak Bayou Trail.

Table 3 displays the highest ranked roadways that resulted from this cumulative attribute scoring. The project team reviewed these initial corridors through a series of working meetings with the client, resulting in a refined list of roadways identified for fieldwork assessment. The initial list of high-need corridors included many streets that have already received investment by MHRA, are programmed for future investment, or run parallel to an existing active street or other corridor on the *People-First Network*. As Table 4 shows, several roadways were removed from the final fieldwork assessment list due to one of these factors. The project team also added other roadways to the list of streets for fieldwork in order to achieve the overall plan goals by (1) extending the *People-First Network* to connect to important destinations and underserved areas, and (2) completing the full network of corridors by connecting key segments of the *People-First Network*. Additions include streets that provide important connections for residents in Shady Acres, Woodland Heights, Norhill, Sunset Heights, Frist Ward, and the Ella Boulevard to Cottage Grove connection.

After refining the roadway segments, the project team then reviewed how these corridors connect to the existing active streets network and programmed projects within the Study Area. This connectivity review helped ensure that high-need corridors not only address data-identified needs but also strengthen a continuous, safe, and comfortable network. In developing assignments for conducting fieldwork on these streets, particular focus was given to improving connections to and across the White Oak Bayou Greenway, a key east–west spine for walking and bicycling. This review was used to help finalize the *People-First Network* by prioritizing corridors that close critical gaps and enhance access to regional trails and planned improvements.

The final fieldwork assessment roadways identified in the table below begin to highlight a network of streets that, when considered in tandem with programmed projects, can be prioritized for future investments that will improve safe access across the Study Area and provide greater connectivity to White Oak Bayou and other key destinations. The list of roads that were removed from the list as needing further assessment are included in Table 4, along with a notation on why the selected roadway was not included for further analysis.

Table 3: High Need Roadways for Field Assessment

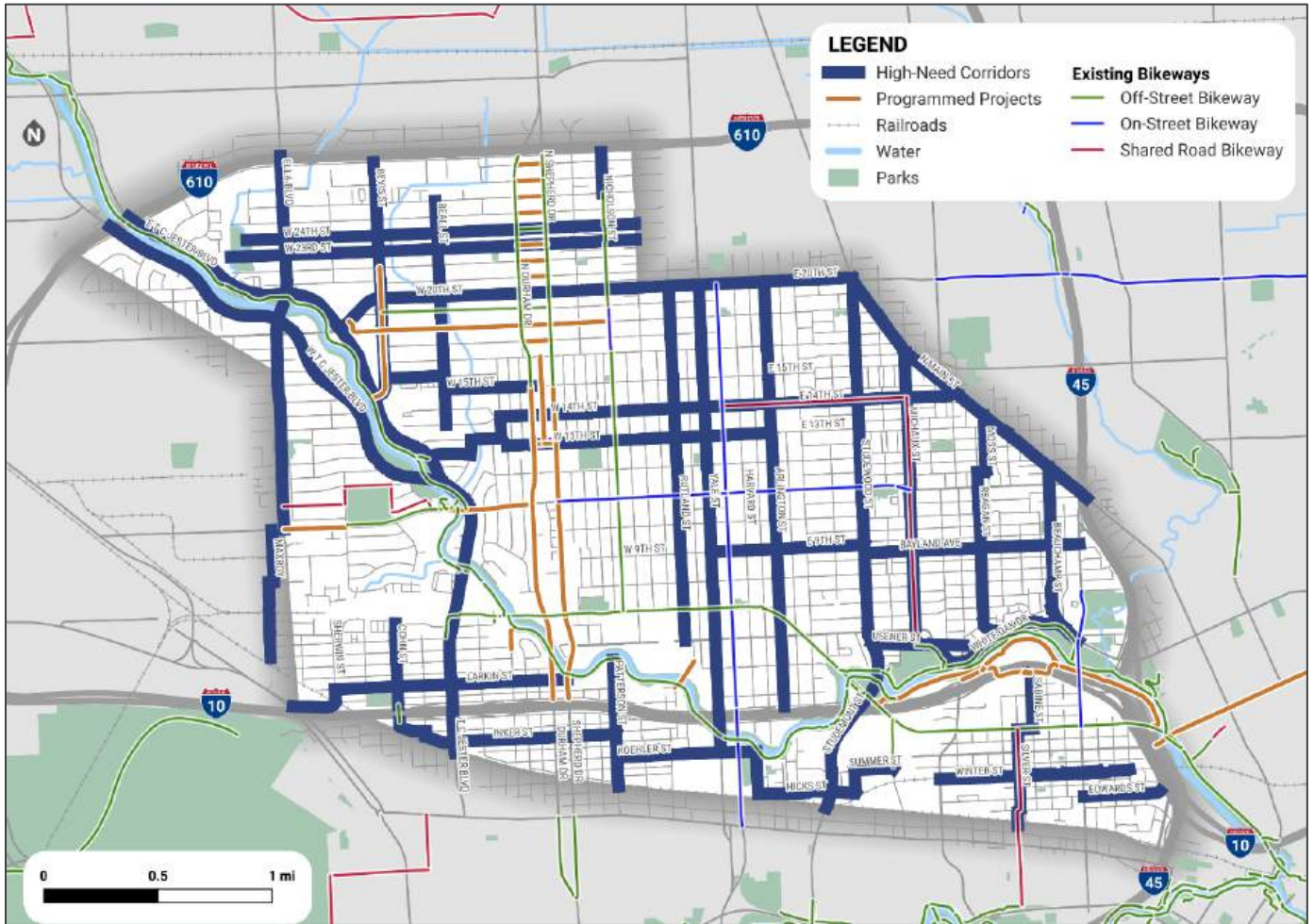
Street	From	To
Larkin St/Sherwin Street	Washington Ave	Shepherd Dr
9th Street	Yale St	Studewood St
Bayland Ave	Studewood St	Houston Ave
Beauchamp Street	N Main St	White Oak Dr
Patterson Street	WOB Greenway	Allen St/UPRR
W 15th Street/W 15 1/2 Street	N Durham Dr	Beall St
TC Jester Boulevard	IH-610	Allen St/UPRR
White Oak Drive	Usener St	Houston Ave
Usener Street	Studewood St	Taylor St

Street	From	To
Ella Boulevard	I-610	W 11th St
N Main Street	W Cavalcade St	I-45
13th Street	Prince St	Arlington St
Rutland Street	W 20th St	W 6th St
20th Street	E TC Jester Blvd	N Main St
Yale Street	W 20th St	Koehler St
W 23rd Street	Little Thicket Park	Ashland St
W 24th Street	Little Thicket Park	Ashland St
Inker Street	Nolda St	Patterson St
Koehler Street	Patterson St	Harvard St
Cohn Street	I-10	Egbert St
Silver Street/Sabine Street	I-10	UPRR
Moss Street/Reagan Street/Gladys Street	N Main St	White Oak Dr
Michaux Street	N Main St	Usener St
14th Street/Groveswood Lane/Wynnwood Lane	E TC Jester Blvd	N Main St
Beall Street	W 26th St	W 14th St
Hicks Street	Harvard St	Summer St
Harvard Street	Koehler St	Hicks St
Arlington Street	Katy FWY Service Road	20th St
Nicholson Street	W 26th St	I-610
Bevis Street	E TC Jester Blvd	I-610
Studewood Street/Studemont Street	E 20th St	Union Pacific Railroad
Winter Street	Sawyer St	I-10/I-45
Edwards Street	Houston Ave	Elder St
Maxroy Street	11th St	Kiam St
Nolda Street	Cohn St Bridge	Radcliffe St / Inker St

Table 4: Streets Removed from Field Assessment

Street	Reason Removed
Heights Boulevard	Existing bikeway and median trail; condition known
11th Street	Existing Active Street
Ashland Street	One block from Nicholson Street (an existing Active Street)
12th Street	One block from existing Active Street on 11th Street and one block from 13th Street on the PFN (being assessed)
Lawrence Street	One block from existing Active Street on Nicholson Street
W 21st Street	One block from 20th Street on the PFN (being assessed)
Shepherd Drive	Existing Active Street and programmed project
W 19th Street	Existing Active Street
Durham Drive	Existing Active Street and programmed project
W 22nd Street	Two blocks from two separate corridors on the PFN
Allen Street	Koehler Street and Inker Street selected as east-west alternatives to Allen Street south of I-10 due to their continuity through the Study Area
Parker Street	Patterson Street selected as a north-south alternative to Parker Street due to its existing crossing of I-10 and existing connectivity to the White Oak Bayou Greenway
Dorothy Street	One block from programmed project on Shepherd Drive
Northwood Street	Reagan Street selected as a north-south alternative to Northwood Street due to continuity through the neighborhood
Eli Street	Koehler Street and Inker Street selected as east-west alternatives to Allen Street south of I-10 due to their continuity through the Study Area
Alexander Street	Two blocks from programmed project on Shepherd Drive

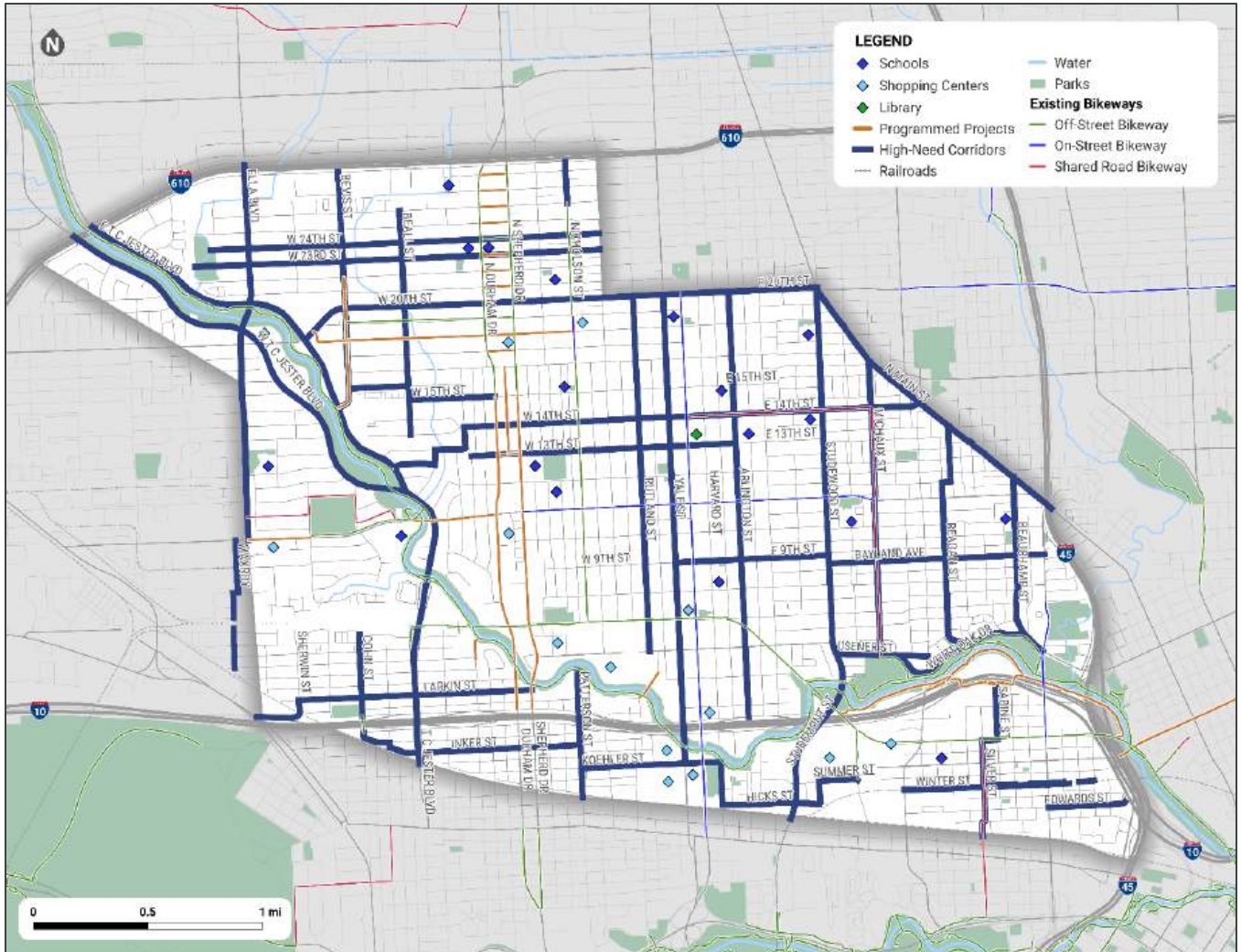
Map 12: The Greater Heights *People-First Network*



The identified high-need corridors, when combined with the programmed projects and the existing active streets and trails in the Study Area, will create a safe and accessible *People-First Network*, as illustrated in Map 12.

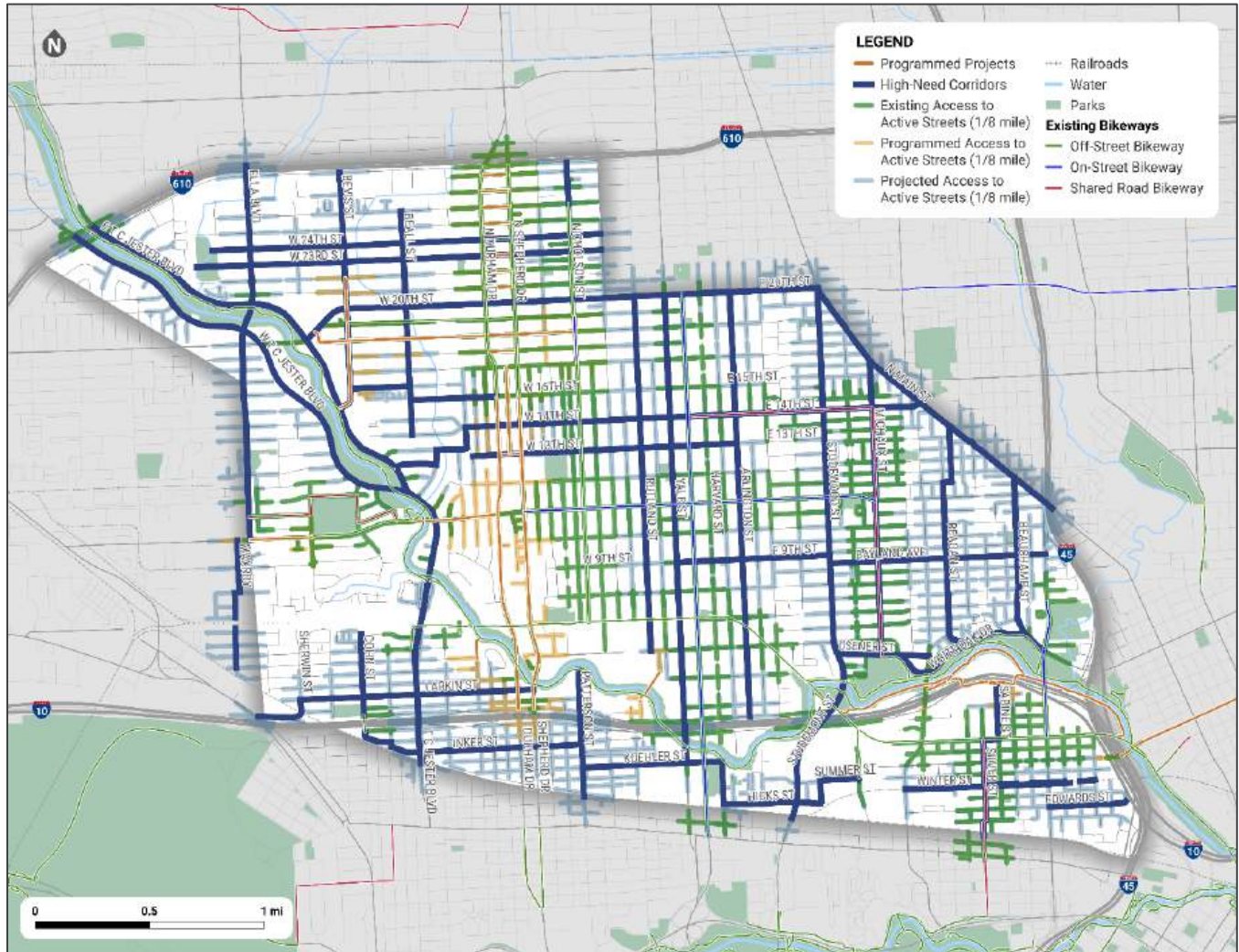
High-need corridors, existing active streets, and programmed projects were then viewed in comparison to mapped destinations to assess visually if the proposed *People-First Network* will provide adequate access (within 1/8 mile) to the mapped destinations. This is shown in Map 13.

Map 13: Existing and Programmed Projects, High-Need Corridors, and Destinations



Finally, the existing, programmed, and identified high-need corridors were viewed in reference to destinations and accessibility to active streets. As an illustrative step to gauge the potential impact of investing in improvements on the high-need corridors, a 1/8 mile walkshed was applied to the high-need corridors to illustrate the projected, improved community access to active streets if the investments were made in these corridors to realize the complete *People-First Network* (Map 14).

Map 14: Existing and Projected Access to Active Streets



Step 4: What Did We Confirm in the Field?

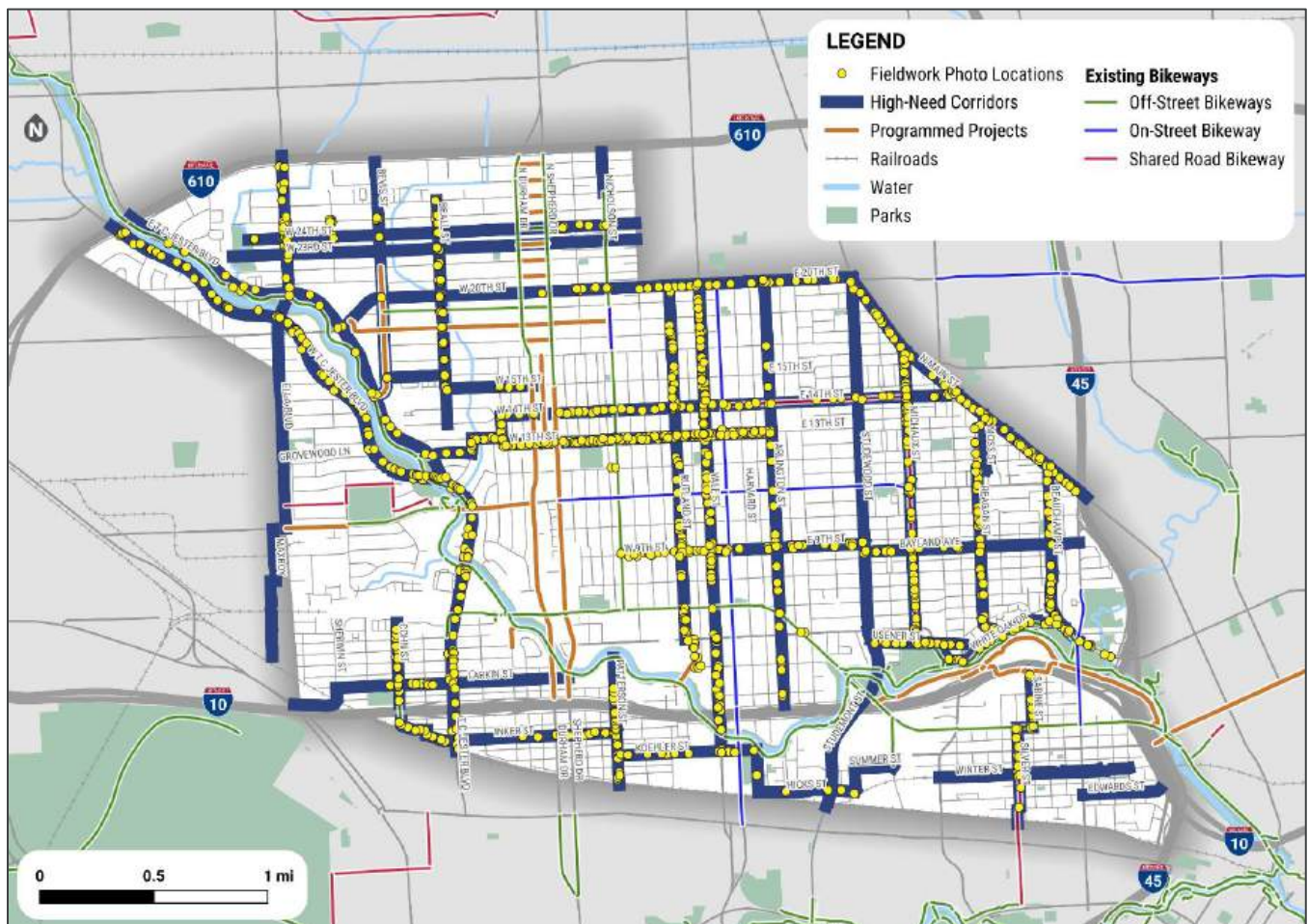
Results of the Needs Analysis established a clear foundation for **Task 3.2 – Corridor Inventory**. The priority segments identified herein were used to guide the field data collection phase. Using the Fulcrum digital survey tool, the project team spent two days in the field from Nov 18th through Nov 19th conducting an existing conditions inventory of the selected roadways, including details such as presence and conditions of sidewalks, curb ramps, crossings, lighting, shade, and traffic operations. Findings from fieldwork were used to verify and refine the mapped priorities before developing project recommendations.

A total of 736 data points were gathered during fieldwork and focused on selected high-need corridors and intersections to provide a conditions assessment of the selected corridors and support future project recommendations intended to improve conditions of sidewalks, curb ramps, crossings, lighting, and shade. Maps 15 – 21 highlight locations of data collected during the project team’s fieldwork, including specific existing conditions data pertaining to sidewalk and curb ramp conditions, intersection and roadway features, and presence of pedestrian-scale lighting.

Map 15: Fieldwork Data Points



Map 16: Locations of Fieldwork Photos



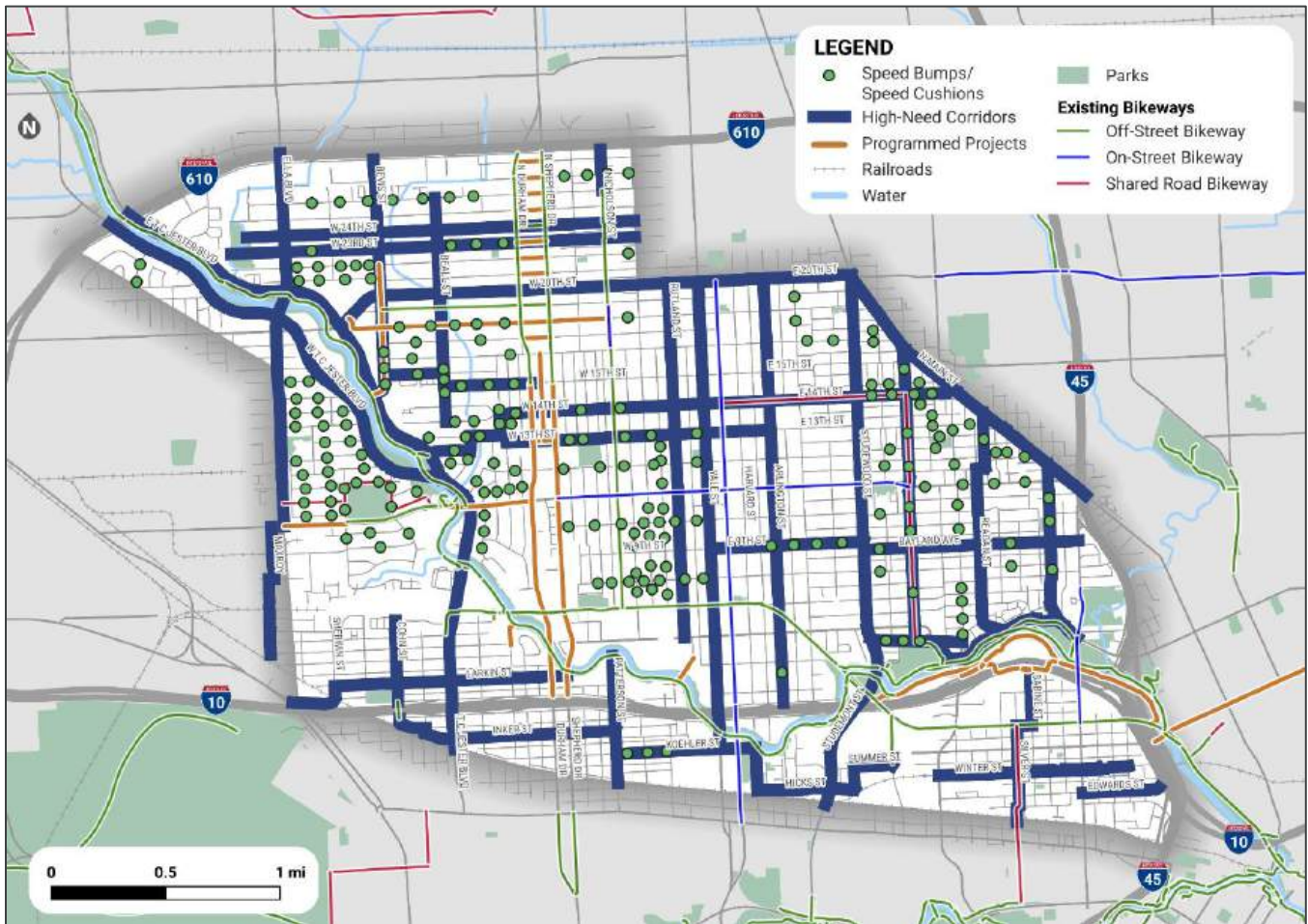
Map 16 shows where fieldwork photos were collected along the study area's high-need corridors, providing on-the-ground documentation of existing walking and biking conditions. These photos help the project team document and understand infrastructure gaps and identify areas for improvements, ultimately helping to guide recommendations for future improvements.

Map 17 highlights intersections along priority corridors in the Study Area where there are no curb ramps, where curb ramps are present but in poor condition, and where intersections do not have crosswalks creating barriers for pedestrians, especially those with mobility impairments. Identifying these locations supports targeted accessibility upgrades that can significantly improve safety, comfort, and accessibility for people at these intersections and improve compliance with the American's with Disabilities Act (ADA).

Map 17: Intersections with Poor Curb Ramps

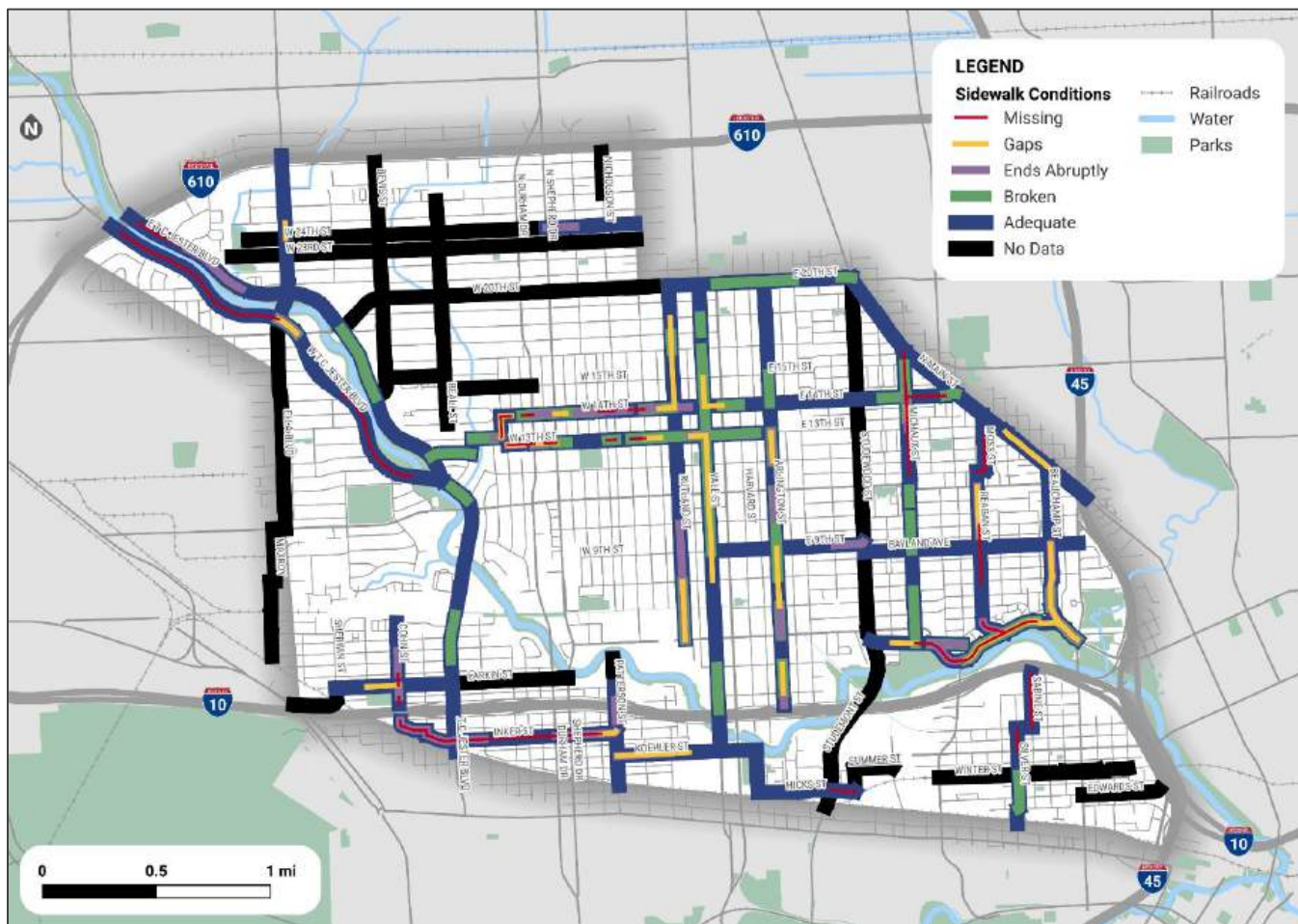


Map 18: Presence of Speed Bumps or Speed Ramps



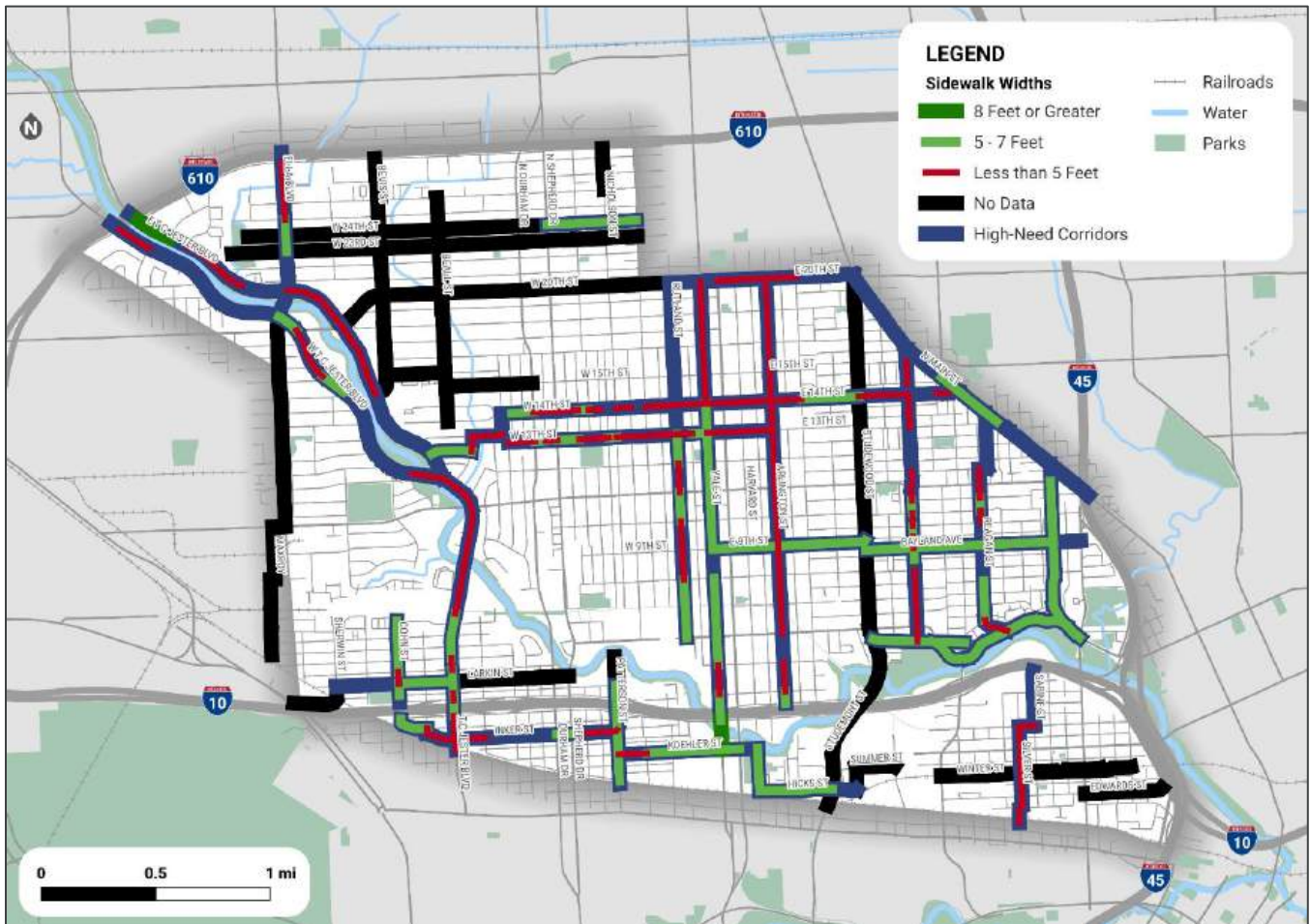
Map 18 identifies locations in the Study Area where speed ramps or speed cushions are present, which help reduce the speed of motorized vehicles and boost safety for pedestrians along corridors.

Map 19: Sidewalk Conditions



The map above identifies locations along high-need corridors where sidewalk segments have gaps, are broken, end abruptly, or are entirely missing, creating significant barriers for people walking or rolling. The sidewalk conditions reflected in Map 19 were collected by the project team during fieldwork; sidewalk conditions were assessed and recorded every block. These conditions reduce safety and comfort throughout the network as they force pedestrians into the roadway or onto narrow, unpaved shoulders, and create obstacles for people trying to reach transit, parks, schools, and other destinations. Filling these gaps would strengthen the overall walking network, increase accessibility, and support safer, more connected mobility options.

Map 20: Sidewalk Widths



Map 20 highlights sidewalk widths along priority corridors. Wider sidewalks increase accessibility and allow for a higher capacity of active mobility users along these corridors. Conversely, narrow sidewalks may be difficult for users in a wheelchair to use, decreasing accessibility. Wider sidewalks also allow pedestrians to pass each other without having to walk into the street or grassy areas. Widening sidewalks in the Study Area would strengthen the overall active mobility network and support safer, more accessible mobility options.

Map 21: Corridors with Pedestrian Streetlights



Map 21 identifies corridors that were found to have pedestrian streetlights during fieldwork. Pedestrian streetlights benefit active mobility users that are navigating trails, sidewalks, and intersections. Adding more streetlights for pedestrians throughout the Study Area provides an increased sense of safety and can encourage more residents to use active mobility options even in darker hours.

Fieldwork Results

Field data collected across the Greater Heights Study Area indicates a concentration of high-need intersections and corridors related to missing pedestrian infrastructure, poor curb-ramp conditions, and incomplete sidewalk networks. The purpose of ranking roadways and intersections from field data was to identify locations with the greatest need for pedestrian and bicycle improvements based on the field inventory. This process translated detailed observations into a consistent prioritization framework that can guide project development in the next stages of work.

During fieldwork, the project team walked each priority corridor and intersection to document conditions such as sidewalk gaps, broken pavement, missing curb ramps, poor lighting, and unsafe crossings. Each issue was noted and tallied so that locations with multiple overlapping problems would rise to the top.

Intersections were ranked based on the number of poor or missing curb ramps, the presence or absence of marked crosswalks, the length and complexity of crossings, and any visibility concerns. Roadway segments were ranked according to sidewalk quality, the presence of continuous pedestrian routes, lighting, accessibility features, and other conditions that affect comfort and safety.

Streets with the greatest number of problems received higher scores. These scores were combined by street names to understand where issues occur repeatedly along a corridor rather than at only one location. Intersections were ranked separately based on their own set of needs.

This process helped identify the locations where repairs or upgrades will make the greatest difference in improving walking and biking conditions across the Greater Heights neighborhood. These ranked corridors and intersections will guide the next phase of the project, when specific improvement concepts are developed.

The highest-need intersections, such as Yale at 4th, 10th, and 15th; Arlington at 14th; and Ella at 24th and 25th, exhibit multiple ADA deficiencies, incomplete crossings, and wide vehicle-dominated geometries that significantly impede safe walking and rolling. Corridor evaluations highlight extensive sidewalk gaps and accessibility issues along Reagan, Arlington, Rutland, 13th, and 14th Streets, as well as in Cottage Grove and the White Oak/Usener/Watson area. These findings provide a clear basis for prioritizing sidewalk and crossing improvements in Task 3.2, guiding field verification, design direction, and the development of the full *People-First Network*. Table 5 highlights observed issues at high-need intersections, specifically noting frequency of poor or missing curb ramps, the severity of conditions as noted in the field comments collected, as well as a summary of improvement needs.

Table 5: Highest-Need Assessed Intersections

Intersection	Observed Issues	Summary Need
Yale St & 4th St	"All in bad condition," no marked crossings, unsafe wide crossing	Full ADA ramp reconstruction + marked crosswalks + traffic calming
Yale St & 10th St	All ramps failing; missing crosswalks across Yale; poor visibility	Replace all ramps, add high-vis crossings, shorten crossing distance
Yale St & 15th St	All four corners "bad"; high-speed vehicular traffic	Complete ADA retrofit on all corners + refuge island evaluation
Arlington St & 14th St	Up to 6 poor ramps; school-access bottleneck; visibility issues	Rebuild ramps, adjust geometry, add school-area crossing treatments

Intersection	Observed Issues	Summary Need
Ella Blvd & 24th St	All ramps non-ADA or missing E-W; very wide arterial	New ramps on all corners + pedestrian signalization/refuge
Ella Blvd & 25th St	Failing ramps, missing E-W crossings; uncomfortable conditions	Full ADA upgrade + E-W crosswalk + median refuge
Bevis St & 25th St	Multiple poor ramps; narrow corners; missing warnings	Corner-by-corner ADA compliance + detectable warnings
Beall St & 16th–19th	Repeated missing/broken ramps; narrow/steep grades	Programmatic ramp buildout for 16th–19th intersections
N Main St & 14th St	No ramps present; extremely unsafe to cross; fast traffic	Add ramps on all corners; protected crossing or signal needed
Usener / Watson / White Oak	Complex geometry, missing ramps, unsafe approaches	Consolidate/realign crossings + add ramps + shorten approaches
Watson & Usener (secondary record)	Broken/misaligned ramps	Ramp reconstruction; evaluate stop-control
Arlington St & 13th	Poor ramps + school traffic	ADA upgrade + curb radius tightening
Beall St & 20th	Missing ramps + fast traffic	Full ADA with refuge island
White Oak Dr & Michaux	Missing ramps + high trail crossing demand	Raised crossing + ADA ramps
White Oak Dr & Taylor	Failing ramps + trail interface	Sidewalk tie-in + ramps + signage
TC Jester & Egbert	No refuge; blind corner; unsafe turning	Add stop control, ramp fixes, remove obstructions
TC Jester & 11th	Blind corners; multiple safety comments	Raised crosswalk + ped signal + traffic calming
Nicholson Trail & 23rd	Crossing gap + poor visibility	Realign trail, improve ramps and crossing
Heights Blvd & 10th	Ramp misalignment	ADA fix + alignment to desire lines
Durham & Center	Unsafe turning, high-speed conflicts	Signal upgrade + ramp work

Table 6 highlights specific conditions recorded during field assessments along high-need corridors, including missing sidewalks, broken segments, ADA issues, abrupt sidewalk endings, drainage problems, and a lack of perceived safe crossings.

Table 6: Conditions Along High-Need Assessed Corridors

Corridor	Observed Issues	Summary Need
Reagan St	Long missing segments, non-existent ramps, broken walks, unsafe school access	Full corridor sidewalk installation + ADA-compliant corners
Arlington St (6th–14th)	Broken/patchy sidewalks; abrupt ends; major ramp failures; school loading areas	Continuous sidewalks + ADA program across every cross street
Rutland St (6th–20th)	Multiple missing segments, abrupt ends, broken boardwalk-style sections	End-to-end sidewalk connection + consistent ADA upgrades
13th St (Heights–Durham)	Many missing or broken areas; driveway conflicts; poor ramps; school adjacency	Reconstruct sidewalks, widen to standard, improve crossings
14th St (Heights–TC Jester)	Narrow walks, missing gaps, high-speed arterials at crossings	Continuous walks both sides + protected crossings
Cottage Grove Area (Cohn/Larkin/Inker/Nolda)	Systemic sidewalk absence; open ditches; steep grades; high pedestrian demand	Area-wide gap-filling strategy + drainage/sidewalk redesign
White Oak Dr / Usener / Watson	Missing sidewalks, unsafe ramps, inconsistent trail access, fast traffic	Unified ped-to-trail access corridor with crossings
N Main St	Cracked, narrow sidewalks; close to fast traffic; wide crossings	Widen sidewalks, add buffers, install refuge islands
TC Jester Blvd	Narrow, broken sidewalks; missing connections to parks/trails; unsafe crosswalks	Spot reconstruction + major crossing enhancements
Beall St (16th–19th)	Repeated missing sidewalks; broken or steep ramps	Continuous sidewalks + consistent curb-ramp standards
Yale St corridor	Unsafe crossings, missing ramps at multiple intersections	Ramp upgrades + enhanced pedestrian crossing strategy
Ella Blvd corridor	Repeatedly failing ramps; missing crosswalks; poor ADA geometry	Comprehensive ADA and crossing retrofit
White Oak Bayou frontage	Missing links from neighborhoods to trail; misaligned ramps	Sidewalk-to-trail connector program

Corridor	Observed Issues	Summary Need
Nicholson area (north of trail)	Missing segments leading to community destinations	Sidewalk infill + smoother transitions to trail
15th St corridor	Tree-root heave; narrow sidewalks; multiple school routes	Widen walks + ramp alignment
20th St	Multiple unsafe crossings + missing refuge islands	Crossing upgrades + potential road diet
Bevis St	Missing sidewalks + poor ramp conditions	Full ADA retrofit + sidewalk construction
Allston St	Several “no sidewalk connecting to trail” gaps	Build walkway + improve trail access

Key Findings: Corridors

- Missing sidewalks are the most widespread infrastructure gap.
- Arlington, Rutland, Reagan, and 13th have multiple concurrent issues.
- Cottage Grove has systemic sidewalk absences and drainage conflicts.
- Trail access points are major pain points (White Oak, MKT).

Key Findings: Intersections

- Yale crossings are consistently unsafe (4th, 10th, 15th).
- Major arterials (Ella, Main) require enhanced crossings.
- Multiple intersections lack ADA ramps entirely.

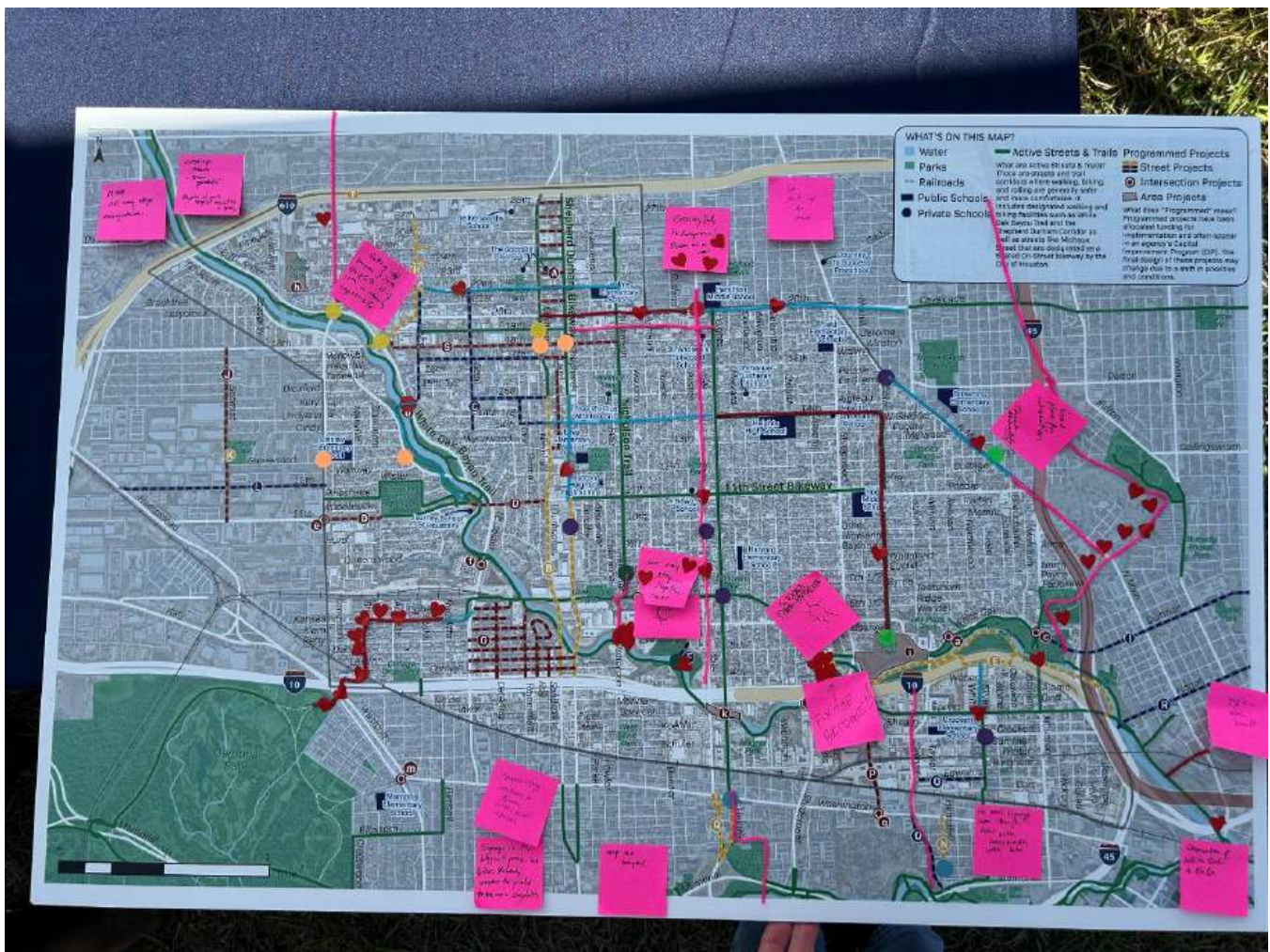
Step 5: How do We Complete a People-First Network?

The final stage or question in this process was assessing what a complete network would look like that could provide access to good active streets throughout the Greater Heights Study Area and fulfil the goals stated at the beginning of this memo. This involved two primary steps,

1. Reviewing community feedback,
2. Assessing if there were any gaps in coverage and proposing additional roadways to fill the gaps.

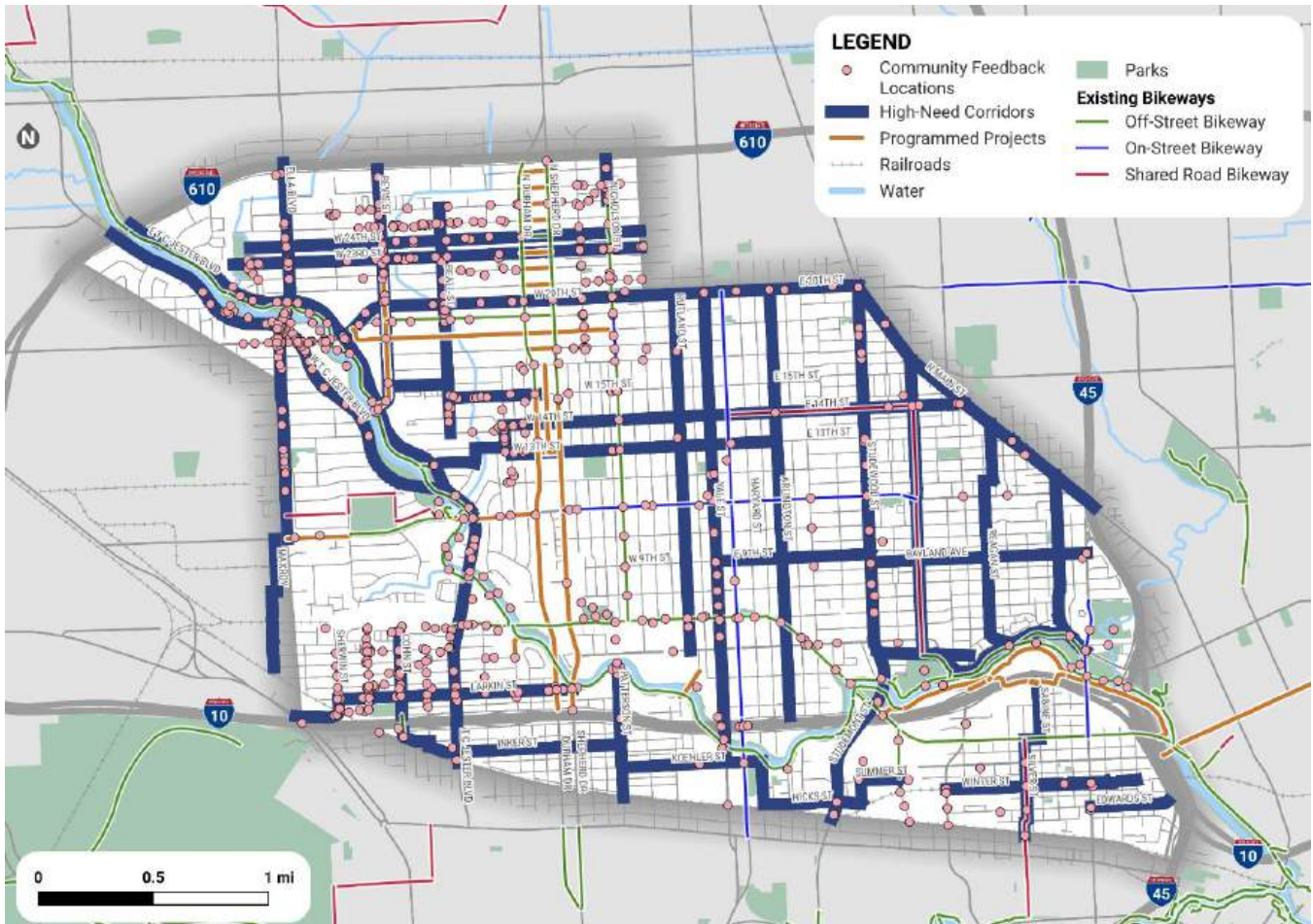
TEI provided Toole Design with community feedback gathered through the Greater Heights Strategic Connections outreach process (Task 6.3), which reinforced many of the physical gaps identified in this analysis. Participants frequently cited concerns about unsafe crossings, missing sidewalks, and a desire for shaded, comfortable walking routes and protected bikeways. An example of the feedback received can be seen in Figure 1.

Figure 1: Community Feedback Map Example



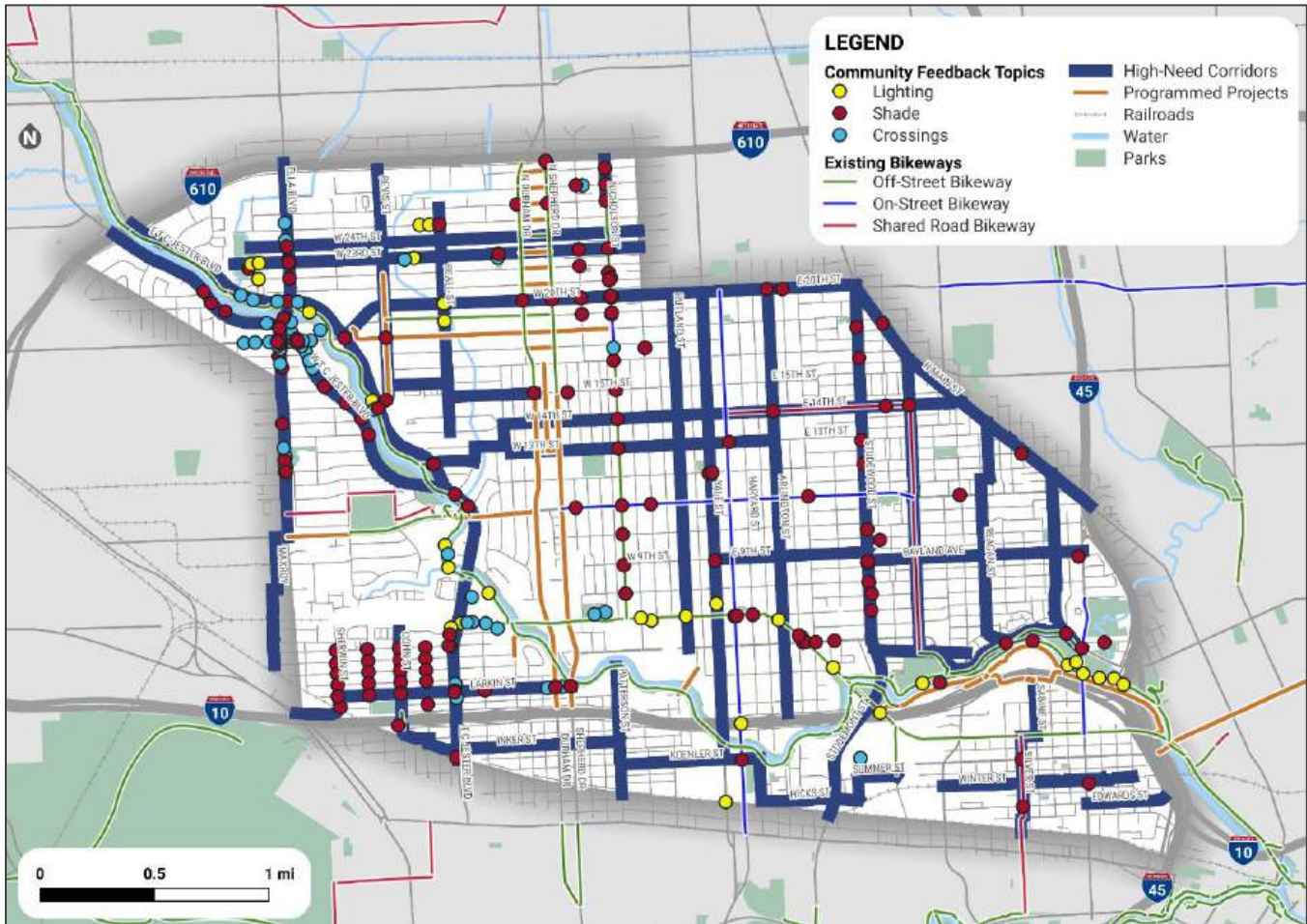
Map 22 shows all the community feedback points geocoded, combining feedback points from the various engagement events exemplified by Figure 1 above as well as feedback points from TEI's Social Pinpoint Online Feedback Map. These points along with the specific feedback associated with each point was then used to identify what types of improvements the community is in support of, and to provide additional scoring for the assessment of the PFN.

Map 22: Community Feedback Points



Map 23 further highlights the specific locations where the community provided feedback on lighting, shade, and crossing needs specifically. These points will be used to inform the purpose and need statements and project recommendations for proposed projects that are developed through this process. In total, there were 728 comments received, 673 of which were in the study area. Of the 673 comments in the study area, 45% were within 50 feet of a high-need corridor, and 5% were within 50 feet of a programmed project. Overall, 50.5% of comments were within 50 feet of a programmed project or a high-need corridor.

Map 23: Lighting and Shade Community Feedback Points



Summary - Community Comment Analysis; Key Themes and Percentages

Across the submitted comments, community priorities clearly cluster around a set of recurring issues. The following table summarizes the major categories and their approximate frequency based on feedback provided by the community and includes examples of the most prevalent comments under each category.

1. Sidewalk Gaps, Repairs, ADA Access ≈ 32% of comments

Examples include:

- Missing sidewalks
- Sidewalk gaps between newer builds
- Cracked, uneven, flooded, or obstructed sidewalks
- ADA ramps missing, misaligned, or in disrepair
- Sidewalks narrowed by vegetation, parking, or drainage issues

- “Goat trails” indicate where sidewalks should be
- Feeling of unsafe access to schools, parks, and trails

Key takeaway: There are *systemic sidewalk and ADA access challenges* across Heights neighborhoods, especially around schools (Helms Elementary, Sinclair Elementary), along Lawrence, W 27th, Dian, Beall, Yale, and around the MKT Trail/White Oak Bayou Greenway trail interfaces.

2. Crossing Safety & Need for Signals / Stop Signs / HAWK Beacons ≈ 28% of comments

Residents repeatedly noted unsafe crossings due to speeding vehicles, long distances between protected crossing points, and lack of signals. Most common requests included:

- More stop signs / 4-way stops
- HAWK signals near schools
- Mid-block crossings on Washington, 20th, 11th, Yale
- Raised crosswalks
- Better markings or pedestrian refuges
- Aligning crosswalks with actual pedestrian desire lines
- Safer crossings to reach parks and trails (White Oak Bayou Greenway, MKT Trail, Stude Park)

Key takeaway: Unsafe crossings, especially on wide or fast corridors (Main, Ella, Yale, Shepherd, Durham, Washington), represent a clear barrier to walking, biking, and accessing schools and parks.

3. Bike Network Connectivity, New Trails, and Protection ≈ 14% of comments

Comments included:

- Requests to extend existing trails (MKT → Memorial Park, White Oak Bayou, Ella, 12th, 11th)
- Add protected bike lanes (Houston Ave, Heights Blvd, N Main)
- Restore or improve bike lanes removed by resurfacing (E 20th)
- Address misaligned or unsafe trail/street crossings
- Calls to flip parking to protect bike lanes

Key takeaway: People want a *connected, comfortable, and protected* bike network that integrates trails with neighborhood streets.

4. Lighting & Personal Security ≈ 8% of comments

Common concerns included:

- Trails that are too dark (MKT Trail, White Oak Bayou Greenway, Heights Trail)
- Underpasses with no lighting
- Lighting gaps around parks, trailheads, and at W 23rd, Ella, Yale, and TC Jester
- Safety concerns linked to recent assaults on trails
- Desire for emergency call boxes similar to Buffalo Bayou Park

Key takeaway: Lighting deficits affect perceived and actual safety on trails and access routes and are a notable barrier to evening activity.

5. Traffic Speeding, Calming, and Driver Behavior ≈ 10% of comments

These include:

- Speed bumps
- Road diets
- No-turn-on-red signage
- Traffic calming around schools, daycares
- Speeding along Durham/Shepherd, 20th, 11th, Ella, TC Jester
- Requests for enforcement

Key takeaway: Streets engineered for speed — or lacking traffic control — consistently endanger pedestrians and cyclists.

6. Drainage, Flooding, and Surface Conditions ≈ 3% of comments

- Flooded ramps
- Puddling sidewalks
- Streets with potholes

These are less frequent but notable in areas near bayous and older neighborhoods.

7. Shade, Trees, Heat Mitigation ≈ 3% of comments

Requests for:

- Shade structures at parks and trails
- Street trees along walking routes
- Mitigating “heat island” sidewalk segments

8. Transit Access Improvements ≈ 2% of comments

- Connect to bus stops
- Extend METRO Rapid service to Heights
- Add shade/shelters at stops

9. Parking/Visibility Conflicts ≈ 2% of comments

- Cars blocking sidewalks
- Visibility issues due to parked vehicles
- No-parking zones needed on narrow streets

Feedback received by the public on these themes and specific locations is used directly to inform which potential projects meet the most expressed community needs and would be likely to have the most community support.

Key takeaways for this plan include but are not limited to the following.

- 1. Sidewalk and ADA access are the #1 concern**, especially near schools and trail access points.
- 2. Crossing safety is the #2 concern**, with a strong geographic pattern: Washington Ave, Yale, Main, 11th, 20th, Ella, and TC Jester.
- 3. Lighting deficits on trails and access routes are constraining evening and wintertime mobility.**
- 4. There is strong public demand for completing trail connections** between MKT Trail, White Oak Bayou Greenway, Memorial Park, and the Greater Heights street grid.
- 5. Traffic calming around schools should be a high priority**, based on both comment frequency and severity.
- 6. Residents strongly value shade and heat mitigation**, which should inform the comfort index of the *People-First Network*.

Finalize Conditions Assessments of High-Need Corridors for Prioritization and Project Development

After reviewing community feedback and analyzing field work data, the *People-First Network* table was updated to reflect the new data. Table 7 shows the total score for each corridor based on attributes including:

- Schools within ¼ mile
- Libraries within ¼ mile
- Parks within ¼ mile
- Hospitals/Health Care Centers within ¼ mile
- Crashes along the corridor
- Missing sidewalks along the corridor
- Sidewalk gaps along the corridor
- Intersections along the corridor without crosswalks
- Corridors that received comments during community feedback

The total score helps identify which corridors are the highest priority and help guide future suggestions for improvements. For corridors that have equal total scores, the number of community feedback points and the number of VRU crashes (data can be found in the appendix) along the corridor will be used to determine preference.

Table 7: High-Need Corridor Attributes and Final Scores

Street	From	To	Schools	Libraries	Parks	Crashes	Hospitals / HCC	Missing Sidewalks	Sidewalk Gaps	Intersections W/O Crosswalks	Connection to Programmed Project	Community Feedback	Total Score
14th St / Grovewood Ln / Wynnwood Ln	E TC Jester Blvd	N Main St	1	1	1	1	1	1	1	1	1	1	10
13th St	Prince St	Arlington St	1	1	1	1	1	1	1	1	1	1	10
Ella Blvd	I-610	W 11th St	1	0	1	1	1	0	1	1	1	1	8
Michaux St	N Main St	Usener St	1	0	1	1	1	1	1	1	0	1	8
Rutland St	W 20th St	W 6th St	1	1	1	1	1		1	1	0	1	8
White Oak Dr	Usener St	Houston Ave	0	0	1	1	1	1	1	1	1	1	8
Yale St	W 20th St	Koehler St	1	1	1	1	1	0	1	1	0	1	8
Arlington St	E 20th St	I-10	1	1	1	1	0	0	1	1	0	1	7
N Main St	W Cavalcade St	I-45	1	0	1	1	1	0	1	1	0	1	7
Silver St / Sabine St	I-10	UPRR	1	0	1	1	0	1	0	1	1	1	7
TC Jester Blvd	IH-610	Allen St/UPRR	0	0	1	1	0	1	1	1	1	1	7
20th St	E TC Jester Blvd	N Main St	1	0	1	1	1	0	0	1	1	1	7
W 23rd St	Little Thicket Park	Ashland St	1	0	1	1	1	0	0	1	1	1	7
W 24th St	Little Thicket Park	Ashland St	1	0	1	1	1	0	0	1	1	1	7
Larkin St / Sherwin St	Washington Ave	Shepherd Dr	0	0	1	1	0	0	1	1	1	1	6
Beall St	W 26th St	W 14th St	0	0	1	1	0	0	0	1	1	1	5
E 9th St	Yale St	Studewood St	1	0	1	1	0	0	0	1	0	1	5
Hicks St / Summer St	Harvard St	Summer St	0	0	1	0	1	1	0	1	0	1	5
Inker St / Nolda St	I-10	Patterson St	0	0	1	0	0	1	1	1	0	1	5
Koehler St	Patterson St	Harvard St	0	0	1	1	1	0	0	1	0	1	5

Moss St / Reagan St / Gladys St	N Main St	White Oak Dr	0	0	1	0	1	1	1	1	0	0	5
Bevis St	E TC Jester Blvd	IH-610	0	0	1	1	0	0	0	1	1	1	5
Studewood St / Studemont St	E 20th St	UPRR	1	0	1	1	0	0	0	1	0	1	5
Bayland Ave	Studewood St	Houston Ave	1	0	1	0	0	0	0	1	0	1	4
Beauchamp	N Main St	White Oak Dr	1	0	1	0	0	0	1	0	0	1	4
Cohn St	I-10	Egbert St	0	0	1	0	0	1	0	1	0	1	4
Patterson St	WOB Greenway	Allen St/UPRR	0	0	1	1	0	0	0	1	0	1	4
Usener St	Studewood St	Taylor St	0	0	1	0	1	0	1	1	0	0	4
W 15th St / W 15 1/2 St	N Durham Dr	Bevis St	0	0	1	0	0	0	0	1	1	1	4
Winter St	Sawyer St	1-10/1-45	1	0	1	1	0	0	0	0	0	1	4
Harvard St	Koehler St	Hicks St	0	0	1	0	1	0	0	0	0	0	2
Edwards St	Houston Ave	Elder St	0	0	1	0	0	0	0	0	0	1	2
Nicholson St	26th St	IH-610	0	0	1	0	0	0	0	0	0	1	2
Maxroy St	11th St	Kiam St	0	0	0	0	0	0	0	0	1	0	1

Next Steps

With the needs analysis and field inventory complete, the next phase of the project will translate these findings into specific project concepts. Corridors with the highest need scores, such as Rutland, 13th, Arlington, Reagan, and the White Oak/Usener/Watson area will move into project definition, where the team will identify crossing improvements, sidewalk connections, bikeway opportunities, and lighting or shade enhancements. Community feedback themes and demographic screens will help shape design priorities and determine which projects best serve people who rely on walking, bicycling, or transit today.

In the next phase, corridors will be prioritized based on three criteria: (1) need score, (2) community support, and (3) feasibility. Corridors with both high need and strong public support will advance first, while feasibility will inform implementation phasing.

Fieldwork confirmed that missing sidewalks, failing curb ramps, and unsafe crossings are widespread across the highest-ranked corridors. These specific conditions will directly inform project concept development for example, identifying where continuous sidewalks are needed along Reagan, where curb ramps must be rebuilt along Arlington and Rutland, or where trail access points need safer crossings at White Oak, TC Jester, and Michaux.

The *People-First Network* is not just a technical outcome it is the backbone for creating a safer, more connected Greater Heights. By identifying where several needs overlap, the PFN ensures that investments are targeted where they will make the biggest difference: giving kids safer routes to school, making it easier for older adults to reach parks and libraries, improving trail access, and ensuring short, everyday trips are accessible by a variety of mode options.

Future project prioritization will also consider how improvements support residents with the greatest mobility needs particularly youth, older adults, people with disabilities, and households without access to a car. These demographic overlays help ensure investment decisions advance equitable mobility.